



MEETING AGENDA

Southeast Area Plan Advisory Committee Meeting #9

MEETING DATE: Thursday, July 30, 2020

MEETING TIME: 5:30 PM – 8:00 PM

LOCATION: Online using Webex. Participation details can be found at:

<https://www.bendoregon.gov/government/citizen-committees/southeast-area-plan-advisory-committee>

Objectives

- Update on SEAP Funding Plan
- Code updates: field work and code work for natural features; multi-use path and trails
- Information on High Desert Park site

Agenda

1. Welcome, Introductory Items (Chair Sharon Smith) – 15 min

- a. Introductions/conflict of interest disclosures
- b. Approval of minutes from previous meeting
- c. Public Comment - Note: only written comments are being accepted at this time. Comments may be submitted by email to dsyrnyk@bendoregon.gov by 2:30 p.m. on Thursday, July 30. All comments received by that deadline will be provided to the Committee and summarized by Chair Smith at the meeting. Comments received after that time will be provided to the committee at the next meeting.

2. Agenda Overview and Where We Are in the Process (Joe Dills) – 10 min

- a. Schedule and process review

3. Funding Plan Update (Project team) – 70 min

This is an informational and discussion item regarding on-going work for funding of infrastructure for the SE Area Plan.

- a. Briefing and discussion on the following:
 - i. Framing
 - ii. DSL and transportation coordination

Accessible Meeting Information

This meeting/event location is accessible. Sign language interpreter service, assistive listening devices, materials in alternate format such as Braille, large print, electronic formats and CD Formats, or any other accommodations are available upon advance request. Please contact Damian Syrnyk meeting at dsyrnyk@bendoregon.gov, 541-312,4919. Providing, at least, 3 days' notice prior to the event will help ensure availability.



- iii. Sewer update
- iv. Water update
- v. ODOT fee update
- b. Round Robin discussion
- c. Next steps

4. Code Updates (Damian Syrnyk and BreAnne Gale) – 45 min

As follow-up to the June discussion, this is a discussion and direction item for SEAPAC to provide input regarding the topics listed below.

- a. Staff briefing and Committee discussion:
 - i. Rock outcrops and trees
 - ii. Multi-use path and trails
 - iii. Alternative collector standard
 - iv. Follow up items from last meeting
- b. Closure – “on the right track” check-in and next steps for code

5. High Desert Park Site (Sarah Bodo) – 15 min

This is an information item about future planning for the park, and how Bend Park & Recreation District plans for compatibility with neighboring properties.

6. Next steps

- a. Announcements
- b. Next meeting date

Agenda Item No. 1:
Minutes from SEAPAC #8,
June 4, 2020

Minutes

Southeast Area Plan Advisory Committee

Meeting #8

Southeast Area Expansion Plan

June 4, 2020

Remote via WebEx



CITY OF BEND

Committee Members

Ken Atwell, *Member*

Don Myll for Kip Barrett, *Member*

Casey Bergh, *Member*

Sarah Bodo, *Member*

Butch Hansen, *Member*

William Hubbert, *Member*

Anthony Oddo, *Member*

Jeff Reed, *Member (absent)*

Jacob Schumacher, *Member*

Sharon Smith, *Chair*

Rachel Strickland, *Member*

Dixon Ward, *Member*

Rick Williams, *Member*

Steve Wilson, *Member*

Rachel Zakem, *Member*

City Staff

BreAnne Gale, *Senior Planner*

Russ Grayson, *Community Development Director*

Ian Leitheiser, *Assistant City Attorney*

Allison Platt, *Senior Planner*

Brian Rankin, *Long-range Planning Manager*

Karen Swirsky, *Senior Planner*

Damian Syrnyk, *Senior Planner*

Jenny Umbarger, *Administrative Support*

Sharon Wojda, *Chief Financial Officer*

Consultants

Joe Dills, *Angelo Planning Group*

Kyra Haggart, *Angelo Planning Group*

Matt Kittelson, *Kittelson & Associates*

Ken Pirie, *Walker Macy*

1. Welcome, Introductory Items

Joe Dills called the meeting to order. Chair Smith took a roll call of attendance, and asked members to disclose conflicts of interest:

- Bruce Hubbert, homeowner in the Elbow
- Tony Oddo, homeowner in the Elbow
- Jacob Schumacher, property owner in the Elbow
- Dixon Ward, property owner in the Elbow
- Steve Wilson, property owner in the Elbow
- Ken Atwell, no conflicts
- Rachel Strickland, no conflicts

- Casey Bergh, no conflicts
- Sarah Bodo, representative of Bend Park and Recreation District which owns property in the Elbow
- Butch Hansen, no conflicts
- Sharon Smith, representative of Bend-La Pine School District which owns property in the Elbow
- Rick Williams, no conflicts
- Rachel Zakem, no conflicts

Chair Smith reviewed the two public comments received prior to the meeting: Christen Brown submitted comments on sewer plans for the area; Ken Atwell submitted a two-page memo summarizing information about the Easton project and infrastructure costs.

Member Williams made a motion to approve the previous meeting's minutes as submitted; Member Wilson seconded. Approval was unanimous.

2. Agenda Overview and Where We Are in the Process

Mr. Dills reviewed the status of the project, as outlined in the presentation.

3. Funding Plan (Part 2)

Mr. Dills reviewed the goals for this meeting's funding discussion, and Mr. Cragie reviewed funding costs, challenges, tools, and options for the Southeast area, as outlined in the presentation.

Members shared general thoughts on infrastructure funding including supplemental system development charges (SDCs), sharing costs with other local developments such as the Department of State Lands (DSL) site, deferring transportation infrastructure projects with timing based on traffic impact studies, the City's contribution to investments, and options for sewer infrastructure funding. Mr. Bergh had specific questions about the costs of Knott Road frontage improvements. Members also expressed whether they had interest in participating in smaller working groups to address funding topics.

4. Draft Code Amendments

Mr. Syrnyk and Ms. Gale reviewed concepts for the development of code amendments including Comprehensive Plan map and policy amendments, proposed changes in uses allowed in different zones, proposed development standards, an alternative collector street standard, and alignments for bicycle facilities, multi-use paths, and trails, as outlined in the presentation.

Members shared general thoughts on the information presented. Comments regarded building, travel lane, and setback sizing, transition zones, alley loading, allowances for drive-throughs in

commercial areas, and allowed uses for High Desert Park. An item will be added to the next meeting's agenda to further discuss park planning. Additional comments regarded allowances for residences in Mixed Employment (ME) zones, providing individual development flexibility to encourage housing affordability, and conditional uses for marijuana- and auto-related businesses in Commercial General (CG) zones. Staff to report back on currently allowed auto-related uses in CG.

Mr. Syrnyk provided an overview of code regulations and staff work related to preserving rocks, trees, and other significant features, as outlined in the presentation. Members shared general thoughts on the information provided. Comments regarded the suggested retention percentage for trees and rock outcrops, and potential flexibility. Staff will contact committee members via email to solicit interest in participating in smaller work groups to discuss further.

Mr. Dills adjourned the meeting at 8:11pm.

Respectfully submitted,

Jenny Umbarger
Damian Syrnyk
Growth Management Department

Accessible Meeting/Alternate Format Notification



The City of Bend is devoted to creating a public meeting event, materials and communications that are accessible. If additional formatting or a physical location are requested, please contact Jenny Umbarger at jeumbarger@bendoregon.gov, or TTY (541) 389-2245. Providing at least 3 days' notice prior to the event will help ensure availability of services requested.

Agenda Item No. 2:
Southeast Area Plan
Code Summary Memo



Southeast Area Plan Code Summary

PREPARED FOR: Southeast Area Plan Advisory Committee (SEAPAC)
 COPY TO: File
 PREPARED BY: Project Team
 DATE: July 23, 2020

Overview

This memorandum provides a summary of the latest draft Bend Development Code (BDC) amendments that will implement the Southeast Area Plan (SEAP) and vision. At SEAPAC Meeting #8 (June 4, 2019), the project team presented SEAPAC, for their review and consideration, a memo outlining high-level development code concepts. This previous memo outlined proposed use changes for the different zoning districts within the SEAP, and additional development and use standards.

Since that time, staff has been working to draft regulations that reflect the ideas and concepts discussed by SEAPAC during the June 4th SEAPAC #8 meeting. Below is a summary of the draft BDC amendments which incorporate these ideas and concepts.

The purpose for considering changes to the BDC for the SEAP is to provide more detail about the type of development intended for the area than is typically identified by the City's zoning code or Comprehensive Plan. Once adopted, it will create standards for the development of land within the plan boundaries that will supersede (where applicable) those of the underlying zone.

For SEAPAC Meeting #9 (July 30, 2020), the project team requests feedback from SEAPAC on the proposed BDC amendments presented in this memorandum. Following SEAPAC's feedback and discussion, the project team will incorporate the committee's comments and prepare these for public review and comments during an online open house scheduled for August 2020.

Southeast Area Plan Code – Topics, Discussion, and Recommendations

The following is a summary of the code topics discussed at SEAPAC Meeting #8 (June 4, 2020) along with project team's recommendations on how to address them in the BDC.

Residential Zones

1. Alley access/street orientation. At SEAPAC Meeting #8, staff presented the concept of alley access and street orientation standards that would require residential lots located in the RS (standard residential) zone to have alley access and be oriented to the arterial or collector, when applicable. The committee discussed this potential requirement and a committee member expressed concern with marketability and livability of RS residential units that front arterials and collectors.

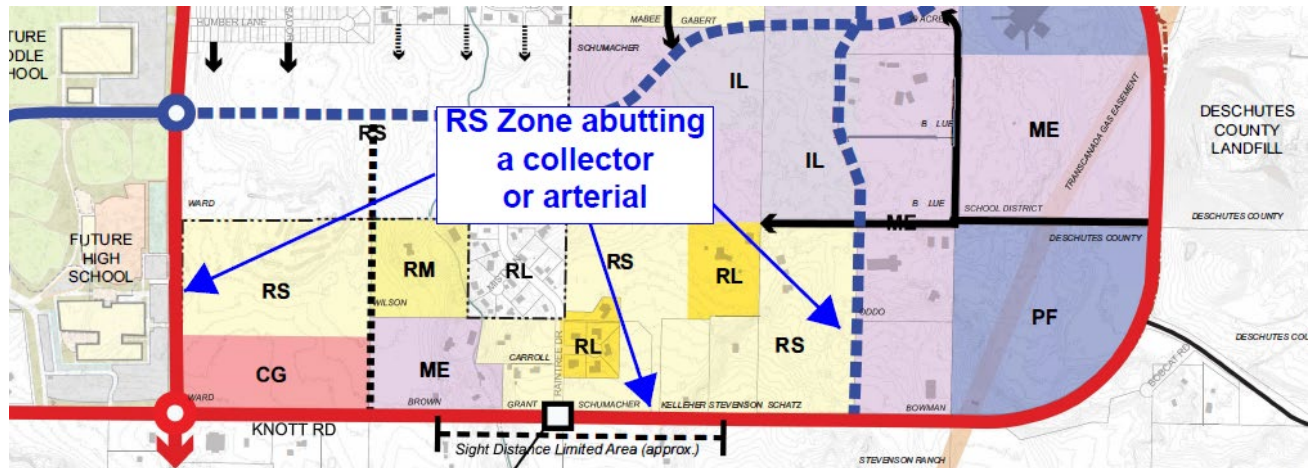
The existing BDC Section 3.1.200(E)(1) requires:

“New lots and parcels created through land division that have frontage onto an arterial or collector street shall provide alternative options for access as indicated below:

1. *Residential lots or parcels not intended for multifamily housing shall provide alley access to the individual lots fronting onto the arterial or collector.”*

Based on feedback from SEAPAC, as well as the existing BDC requirement for alley access and limited area of applicability for this requirement in the SEAP (as shown in Figure 1 below), staff is recommending that no additional requirements for alley access or building orientation standards be added.

Figure 1. Locations where RS zone abuts a collector or arterial



Commercial Zones

1. Automobile-Oriented and Automobile-Dependent Uses and Facilities. In the BDC, retail sales and service that are classified as “auto-oriented” include those that allow drive up or drive-through facilities. Drive-through facilities are those that are designed to allow drivers to remain in their vehicles before and during an activity on the site (e.g. coffee, banking). BDC Section 3.6.300(A)(1) regulates the development of auto-oriented uses that include uses such as banks, coffee huts, and retail with drive-through pharmacies. BDC Section 3.6.300(A)(2) addresses gas stations. The standards in Sections 3.6.300(A) are intended to provide a vibrant, storefront character, slow traffic, and encourage walking and transit use

The project team recommends the following changes to support the development of auto-oriented uses in a manner consistent with the vision for the SEAP as a walkable community. These changes include:

- Prohibit drive-up, drive-in, or drive-through drives or lanes to be located between a building and a public or private street.
- Prohibit drive-in and drive-through facilities from being oriented towards a public street or private street.
- Prohibit standalone commercial and public parking as a permitted use.
- Automobile Services and Gas Station:
 - Require a minimum 10’ landscape buffer with a mature vegetation height of at least three feet within three years of planting between all onsite vehicular circulation areas and the street.
 - Require the primary building (i.e. gas station convenience store) to be located adjacent to the street (at the 10’ minimum/maximum building setback requirement) and to include a street-oriented pedestrian entrance.

2. Marijuana Businesses in the CG Zone. There are several marijuana related uses currently allowed in the CG Zone under BDC 2.3.300, including:

- a. Medical marijuana dispensary and marijuana retailer
- b. Marijuana wholesale (More than 75% of sales are wholesale)
- c. Marijuana Testing, Research and Development Facilities

With respect to these uses in the CG zone, the project team recommends the following:

- No additional regulations for dispensaries and retailers. These uses are already regulated under BDC 3.6.300(P)(4)(d).
- Prohibit marijuana wholesale. This would be consistent with the direction proposed for this use in the IL and ME Zones.
- Allow testing, research, and development facilities. This is also consistent with other similar uses in the SSEAP.

Regarding dispensaries, these uses are regulated under BDC 3.6.300(P)(4)(d):

d. Medical Marijuana Dispensaries. No medical marijuana dispensary may operate or conduct business within:

i. One thousand feet of a public or secondary school for which attendance is compulsory under ORS [339.020](#) (2013); or a private or parochial elementary or secondary school, teaching children as described in ORS [339.030\(1\)\(a\)](#) (2013);

ii. One thousand feet of another medical marijuana dispensary;

iii. One hundred fifty feet of a licensed child care facility, as defined in [BDC Chapter 1.2](#); or

iv. One hundred fifty feet of a Bend Park and Recreation District developed park facility including neighborhood parks, community parks, regional parks, natural areas, urban plazas and community river parks, or an Oregon State Park.

The project team does not recommend any changes to how dispensaries are allowed in the CG Zone because each CG zone in the SEAP abuts or will be across a public street from a school. The CG designated area in the north abuts High Desert Middle School; the CG designated at the corner of 15th St and Knott Road is across the street from the new high school.

3. Standalone Residential Uses in the CG Zone. Staff is recommending amendments to allow for standalone residential uses in the CG zone, with the following standards and limitations:

- Standalone residential uses in the CG zone that are not part of a vertical or horizontal mixed-use development are limited to the following uses: Multifamily Residential, Attached Single-Family Townhomes, and Accessory Dwelling Units (ADUs)
- Must not be located on a development site that fronts a “Shopping Street” or arterial.
- Must meet the minimum density standards for the RH zone (21.7 units/gross acre)
- Must meet specific development and design standards for standalone residential uses in commercial zones to ensure compatibility with the surrounding commercial development, consistent with similar requirements for residential uses located in commercial areas in other parts of the City. These standards include:
 - 10’ minimum/maximum setback
 - street orientation standards

- minimum transparency requirements (25% ground floor, 20% upper levels)
- simple building articulation standards
- parking standards (1 space per dwelling unit, 0.5 parking space per affordable dwelling unit)
- open space requirement exception when located within a ¼ mile of a public park

4. “Shopping Streets”. The existing BDC section 2.2.500.C.2, requires a “shopping street” with specific pedestrian-oriented design standards, when multiple-building development is contemplated on parcels or lots that are 10 acres or greater. In addition to the existing requirements in section 2.2.500(C)(2), the project team recommends a requirement that a shopping street be identified at the time of master plan, site plan, annexation, and/or subdivision for properties that are 20 acres or greater and contain 10 acres or more of area with a comprehensive plan map designation of Commercial General. Staff is recommending additional design elements be required, including:

- Shopping street length requirements (a minimum of 400’ long) with ground floor commercial development required on both sides.
- Prohibiting vehicular access from a shopping street.
- Requiring parking to be located behind primary structures.
- Prohibiting some uses otherwise allowed in the CG zone, including: Automobile Dependent and Automobile Oriented Retail Sales and Services, Drive-In and Drive-Through Facilities, standalone Commercial and Public Parking, standalone residential, and Commercial Storage

Mixed Employment Zone

1. Automobile-Oriented and Automobile-Dependent Uses and Facilities in the ME Zone.

Like the CG Zone, retail sales and service uses that are auto-oriented are permitted subject to the standards under BDC 3.6.300(A). However, the ME Zone addresses restaurant/food and beverage services separately. Those uses without a drive-through are permitted; those with a drive-through are allowed as conditional use. This means that in addition to the standards in BDC 3.6.300(A), such uses are also subject to the processes, standards, criteria, and potential conditions for conditional uses under BDC Chapter 4.4.

For drive-throughs in the ME Zone, the project team recommends eliminating the conditional use permit and applying the same changes proposed above for such uses in the CG Zone.

2. Marijuana Businesses in the ME Zone. There are several Marijuana businesses allowed in the Mixed Employment (ME) Zone under BDC 2.3.200. These uses include the same uses already allowed in the ME Zone:

- Medical marijuana dispensary and marijuana retailer
- Marijuana wholesale (More than 75% of sales are wholesale)
- Marijuana Testing, Research and Development Facilities

Dispensaries and retailers are regulated in the ME Zone according to BDC 2.3.200(C)(2)(b):

b. In the ME Zone, retail sales and service uses and medical marijuana dispensary and marijuana recreational retailer uses shall not exceed 50,000 square feet ground floor, except that on property five acres or greater retail sales and service uses shall not exceed 75,000 square feet.

In addition, dispensaries and marijuana retailers are regulated under BDC 3.6.300(P)(2)(d), like they are in the CG Zone.

With respect to Marijuana businesses in the ME Zone, the project team recommends the following:

- Make no SEAP specific changes for dispensaries and retailers in SEAP. Such uses are already regulated under the base zone (here, ME) and the standards in the Bend Development Code.
- Like the CG and IL Zones, prohibit marijuana wholesale in the ME Zone
- Allow testing, research, and development facilities, in the ME Zone, as these are consistent with similar uses in the ME Zone.

3. Housing in the ME Zone. Several types of housing, with the exception of single-family detached housing, are allowed in the ME Zone under BDC 2.3.200. These types include:

- Attached single family townhomes
- Two- and Three-Family Housing
- Multi-family Residential
- Temporary Housing
- Accessory Dwelling Units

These residential uses/types of housing are regulated under BDC 2.3.200(C)(1):

1. New Residential Uses. In order to ensure that the ME and PO Zones retain a focus on employment uses, new residential uses in the ME and PO Zones are limited as follows:

a. Residential uses that are part of a mixed-use development in which nonresidential uses occupy at least the floor area equivalent to the entire ground-floor area of the development are permitted.

b. Residential uses that are part of a mixed-use development in which nonresidential uses occupy less than the floor area equivalent to the entire ground-floor area of the development are conditional.

c. Residential uses that are not part of a mixed-use development are prohibited.

With respect to the types of housing allowed as part of a mixed-use development, mixed-use may include either vertical or horizontal development, and may also be attached or detached buildings.

In addition to the types of housing listed above, the project team has recommended adding live/work dwelling units to the list of allowed types of housing in the ME Zone. The term “live/work dwelling unit” refers to a structure or portion of a structure that combines a nonresidential use that is allowed in the zone with a residential livable space. A live/work dwelling unit is allowed instead of, or in addition to, a home business as defined by the BDC

With respect to housing in the ME zone, the project team recommends allowing live/work dwelling units as a permitted use and type of housing. The team does not recommend any further changes to the code for housing in the ME Zone.

Light Industrial Zone

1. Design Standards. In addition to the existing standards in BDC 2.4.700, the proposed changes include:

- a. A requirement for primary building entries to be emphasized with architectural features such as awnings, projections, recesses, changes in materials or similar features and to also include pedestrian scaled elements such as lighting and signage.
- b. Encouragement of the use of durable, quality materials (including brick, stone, concrete, concrete masonry units, and wood) and exterior design features (including change in materials, texture, color and trim detailing) in order to break up large building facades and create a human scale.

2. Parking. Prohibiting parking from being located between the primary structure and front property line.

3. Setbacks. Staff reviewed the feedback from SEAPAC and the existing buffering and setback requirements in the IL zone and is recommending no changes be made. The existing setbacks are as follows:

Front Yard Setbacks: The minimum front yard setback is 10 feet.

Rear and Side Yard Setbacks: None, except when abutting a Residential Zone the setbacks are as follows: 20 feet; portions of buildings or structures that exceed 35 feet in height shall be set back an additional one-half foot for each foot by which the building height exceeds 35 feet.

Public Facility Zone

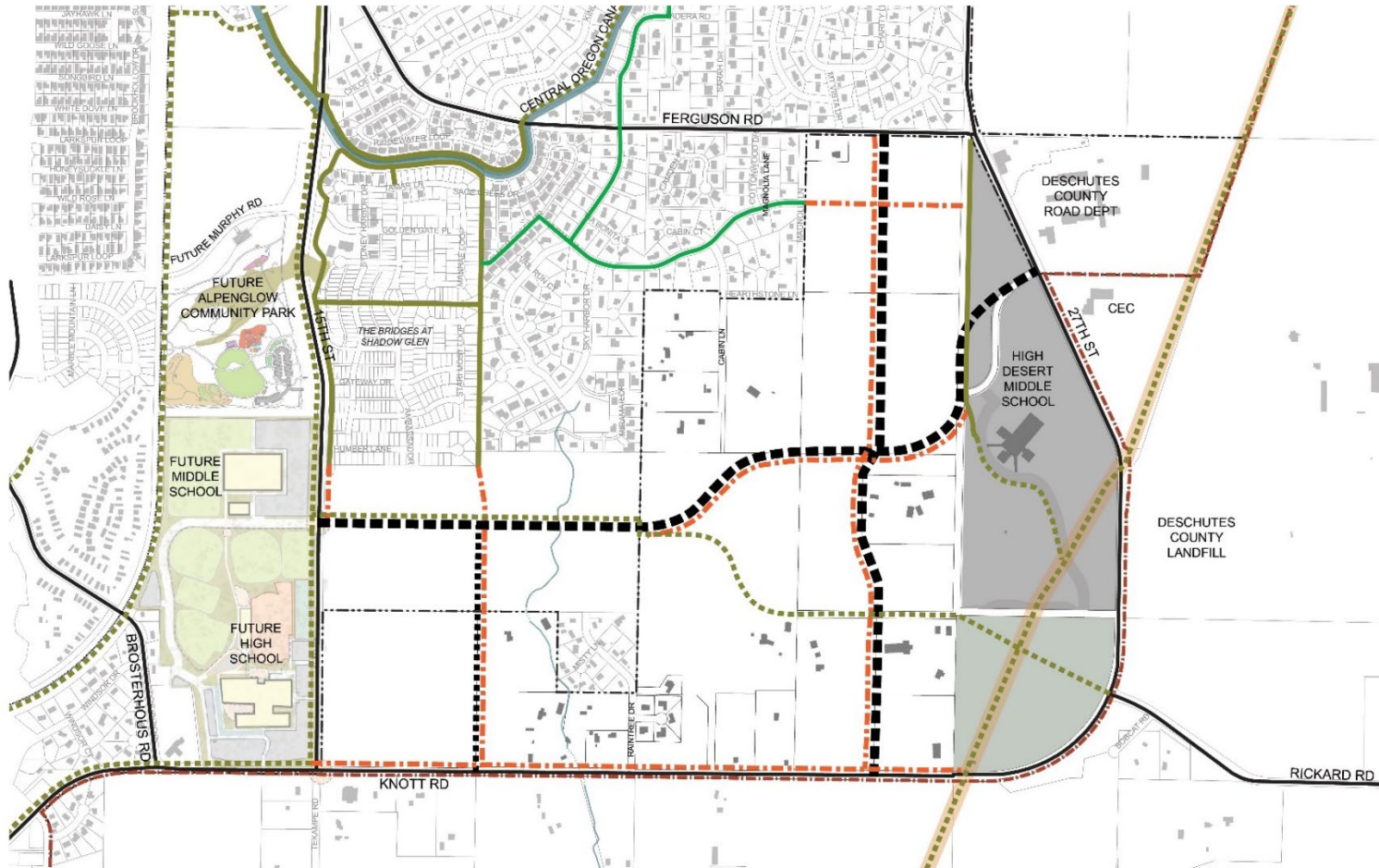
Based on the feedback from SEAPAC #8, the project team proposes no changes to the existing permitted and conditional uses currently allowed in the PF Zone. Sarah Bodo will give a presentation on the Bend Park and Recreation District's process for developing a plan for an individual park, such as High Desert Park.

Multi-use Paths and Trails

The presentation for this code will include one change to the map shared with SEAPAC at your June 4th meeting. The attached map shows the location of the multi-use path that parallels the east-west collector through the Ward/Pahlisch property on the north side to match the plan for the development of Easton. Paths and trails must be constructed according to their alignment shown on the map, and in conjunction with roadway construction, a land division, or site development.

Multi-use paths and trails constructed outside of a road right of way must be at least 10 feet wide and located within an easement that will be dedicated to the public. Trails constructed outside of the public right of way may be used to satisfy open space requirement for master plans and/or landscaping requirement for site plans.

Figure 2. Southeast Area Plan Multi-Use Paths & BPRD Trails



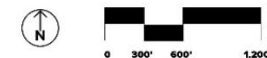
SOUTHEAST AREA PLAN
MULTI-USE PATHS &
BPRD TRAILS

- Existing Facility
- BPRD Multi-Use Path*
- Multi-Use Path
- 2- or 3-Lane Collector
- 2-Lane Local or Collector
- Low Stress Network Route
- Existing City Boundary
- Urban Growth Boundary
- Tax Lots
- Railroad
- Piped Canal
- Canal
- TransCanada
- Natural Gas Pipeline

Note: Multi-use paths can be incorporated into roadway design

SUBJECT TO CHANGE
7/10/2020

*Trails identified in the BPRD Comprehensive Plan are referred to as multi-use paths in the City's development code



Significant Trees and Rock Outcrops

Based on feedback from SEAPAC #8, the project team conducted additional field work and site study analysis to develop the following draft tree and rock outcrop regulations. Field visit results and analysis will be presented at the July 30th SEAPAC meeting.

Significant Trees

In SEAP, BDC Section 3.2.200 Land Conservation and Section 16.10.100 Tree Preservation Standards would apply. In addition to BDC Section 3.2.200, a minimum requirement of 50% of trees 24" or greater would be required to be preserved in SEAP. These could be incorporated into required open space and landscaping. Additional incentives would be available for preserving trees greater than 24" DBH or tree groves that go beyond those in the existing tree ordinance, See BDC 3.2.200(D). The following are proposed incentives for preserving significant trees in SEAP:

- Reductions of setbacks up to **30** percent.
- Reduction of required on-site parking up to **15** percent.
- Increased lot coverage up to **20** percent.
- Reduced landscape coverage up to **ten** percent.
- Allow reduced lot sizes to preserve individual significant trees or tree groves.

Note: the proposed incentives shown above will be reviewed with SEAPAC at the July 30, 2020 meeting.

Rock Outcrops

Recommendations to incorporate natural rock outcrops into SEAP development include two approaches:

1. **Reduced landscape requirements** when **existing rock outcrops** are incorporated into the development's landscaping.
2. The **required preservation of significant rock outcrops**, with specified exceptions.

Reduced landscape requirements

Developments in SEAP that incorporate existing (on-site) rock outcrops, defined as rocks or clusters of naturally occurring lava rock, into their landscaping can qualify for reduced ground cover requirements in BDC 3.2.300(D) from 80% plant ground cover required to 50%.

Preservation of significant rock outcrops

The location of significant rock outcrops would need to be provided with a survey through the development review process for master plans, site plans, and subdivisions. Significant rock outcrops, defined as clusters of naturally occurring lava rocks, that meet the below criteria, would be required to be preserved:

- Have a height at one point above the surrounding terrain of **8 feet** or more
- Are over 3,000 square feet in area

The total area of preserved rock outcrops areas would not need to exceed more than 3% of total parcel size. Certain exceptions would be made available such as:

- Where public or shared infrastructure could not be sited elsewhere
- If minimum density is not achievable.

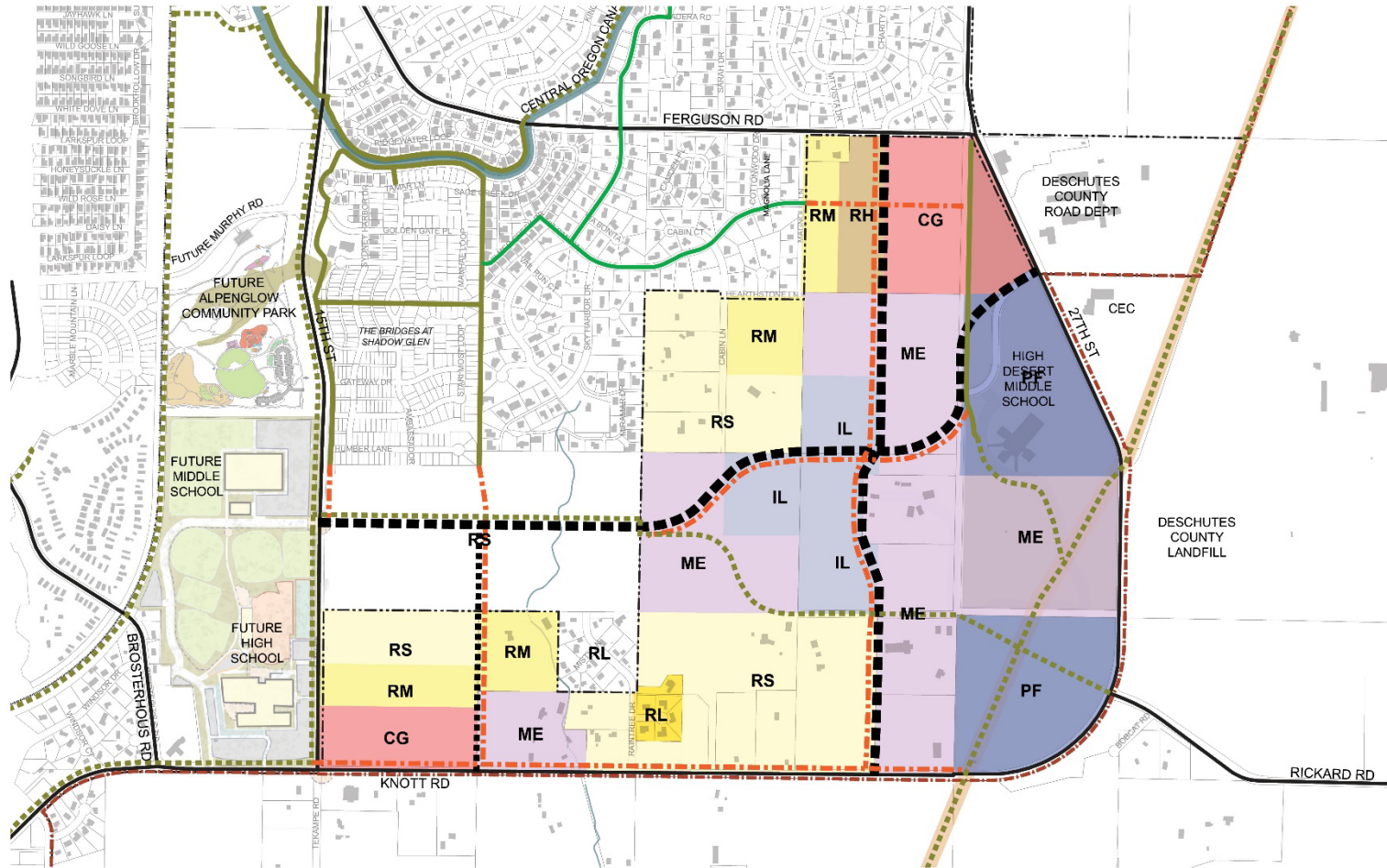
- Soft surface trails would be able to be developed within significant rock outcrop areas, as allowed in the ASI code, BDC Section 2.7.700(E).

Incentives would be made available for the area preserved for rock outcrops including:

- Reductions of setbacks up to **30** percent.
- Reduction of required on-site parking up to **15** percent.
- For public facilities, an increase in maximum parking allowed by **15** percent.
- Increased lot coverage up to **20** percent.
- Reduced landscape coverage up to **ten** percent
- Reduced landscaping requirements for plant ground cover.

Note: the proposed incentives shown above this text will be reviewed with SEAPAC at the July 30, 2020 meeting.

Figure 3. Southeast Area Plan Land Use, Transportation, and Trail Concepts



LAND USE,
TRANSPORTATION,
AND TRAIL CONCEPTS

- Existing Facility
- BPRD Multi-Use Path*
- Multi-Use Path
- 2- or 3-Lane Collector
- 2-Lane Local or Collector
- Low Stress Network Route

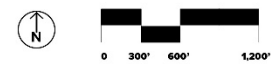
COMPREHENSIVE PLAN DESIGNATIONS

- CG - Commercial General
- IL - Industrial Light
- PF - Public Facility
- ME - Mixed Employment
- RH - Residential Urban High Density
- RM - Residential Urban Medium Density
- RS - Residential Urban Standard Density
- RL - Residential Urban Low Density

SUBJECT TO CHANGE
7/10/2020
Note: Multi-use paths
can be incorporated into
roadway design



*Trails identified in the BPRD Comprehensive Plan are referred to as multi-use paths in the City's development code



Agenda Item No. 3:
Southeast Area Plan
Collector Street Design
Elements Memo



Collector Street Design Elements for Southeast Area Plan V2.0

PREPARED FOR: Southeast Area Plan Advisory Committee
 COPY TO: Southeast Area Plan Project Team, Southeast Area Plan Project File
 PREPARED BY: Damian Syrnyk, Senior Planner
 DATE: July 17, 2019

Overview

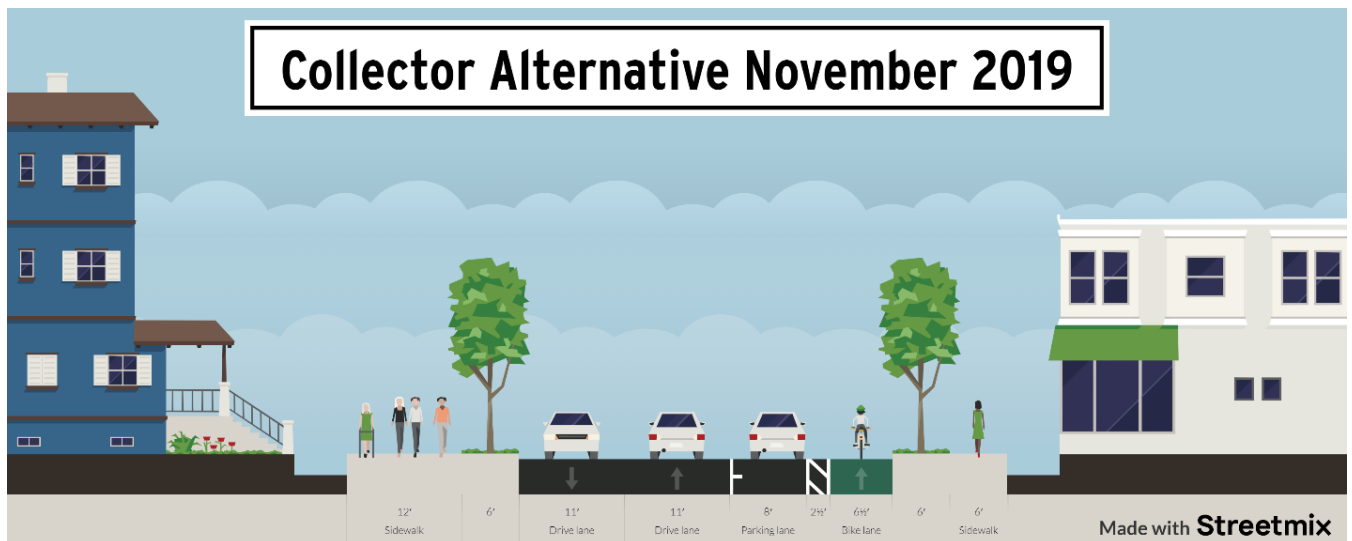
This memorandum presents the final draft of a proposed alternative standard for collector streets. The project team outlined a proposed alternative collector standard for SEAPAC's consideration and direction at your September 26, 2019 meeting. SEAPAC provided feedback the project team used to make several changes to the proposed alternative standard. This memorandum also includes several examples of how the alternative collector standard can be modified based on the land use context, for example, whether it's primarily residential, mixed use, or more commercial and employment focused.

The project team requests SEAPAC's review and feedback on the enclosed proposed alternative collector standard. For reference, you will find a link to the [September 26, 2019](#) meeting packet that includes the September 12th memorandum to SEAPAC (see Agenda Item No. 7).

Proposed Design Elements for an Alternative Collector Standard

Figure 1 presents an example of a cross section for a collector street using the alternative standard.

Figure 1. Cross section of Collector Street using Alternative Standard



The following presents the elements of the alternative collector standard, with highlighted text indicating changes made since the September 2019 SEAPAC meeting. If SEAPAC approves of the alternative standard as proposed in this memorandum, the project team will prepare draft code amendments incorporating the standard in Chapter 3.4, Public Improvements, of the Bend Development Code (BDC). BDC 3.4.200, Transportation Improvement standards, is the appropriate section in which to incorporate the alternative standard.

Required Elements – These standards are minimums that must be met with development of a collector street under this standard.

- **Right of Way (ROW)** –80 feet
- **Travel Lane Width** – 11 feet (when separated by left-turn lane, pedestrian safety island, etc.); 11.5 feet when opposing lanes are immediately adjacent (head-on).
- **Bike Lane** – 6 feet plus a buffer of 2 ½ for car door opening buffer (or buffer to moving traffic)
- **Driveway Access** – Access will be allowed per BDC 3.1400 Vehicular Access Management. There is no change proposed for this alternative standard.
- **On Street Parking** – If on-street parking is provided, it shall be designed and constructed according to BDC 3.3.500, On-Street Parking Design Standards. On-street parking may require an additional easement to ensure the other elements of the collector road right of way satisfy those above. Curb extensions are required around on-street parking at intersections to ensure everyone can see each other at crosswalks.

Flexible Elements - The following elements may flex depending within the proposed ranges

- **Sidewalk** – from 6 feet to up to 8 feet
- **Multi-Use Path** – 10 to 12 feet if located on the side of a street, separated from travel lanes and for two-way pedestrian and bicycle travel
- **Planting Strip** – 4 to 6 feet on each side to include street trees, bioswales, and/or landscaping; street trees could be planted in tree wells in areas with narrower sidewalk widths or in areas where sidewalk space wants to be maximized
- **Median/Turn Lane** – Consider including for intersections that warrant a left-turn lane (e.g. the intersection of the east-west collector with north-south collector) or when additional enhancement is warranted for a pedestrian crosswalk (safety island).

For reference, this memorandum includes a proposed new Table F that would be added to BDC 3.4.200 to reflect the new alternative collector standard, including both fixed and flexible elements.

Proposed Table F: Alternative Improvement Standards for Dedicated Public Collector Roads

Minimum Right of way	Minimum travel lane width	Planter strips	Minimum Left-Turn Lane/Median Island Width (2)	Maximum Grade	Direct Site Access (3)	Sidewalks Both Sides (4)	Curbs
80'	11' / 11.5'	4' to 6'	13'/10'	6%	Yes	6' to 8"	Yes

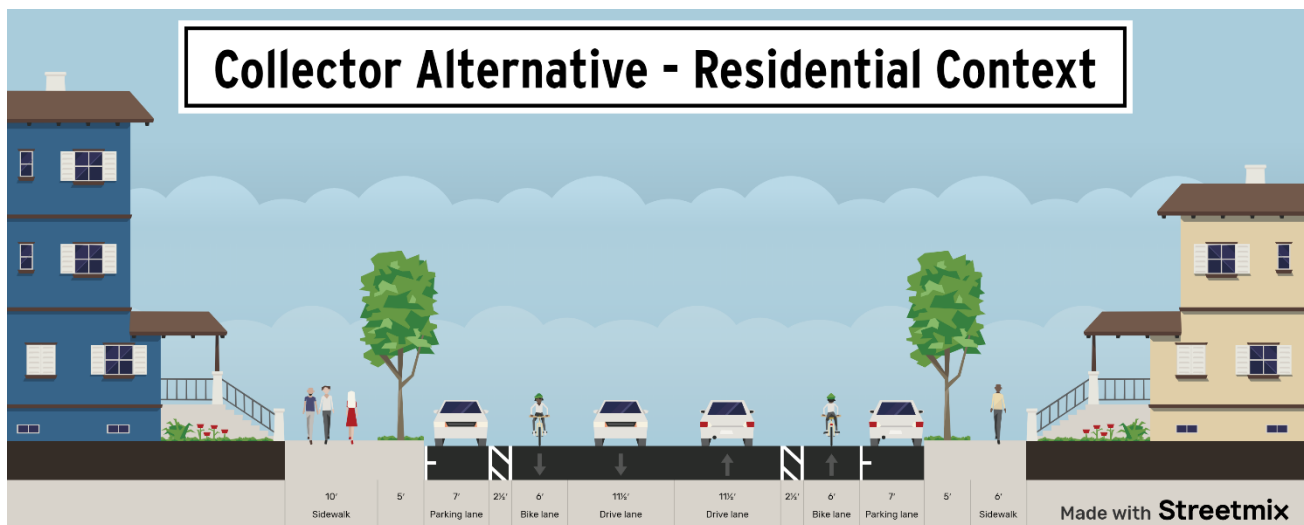
Requirements

1. Travel lanes must be at least 11 feet in width (11.5 feet when opposing flow is immediately adjacent (head-on)).
2. The first dimension is the minimum required width of the left-turn lane while the second dimension applies to the raised median width constructed between intersections, or at intersections.
3. Site access allowed per BDC 3.1.400, Vehicular Access Management
4. A sidewalk is not required on the side of a collector developed under this standard if a multi-use path is provided and is at least 10 feet wide for two-way travel. This type of path is dedicated for pedestrian and bicycle trips only.

Examples of Collector Alternative Standard in Different Land Use Contexts

The following cross sections provide three different examples of using the alternative collector standard, including examples with and without on-street parking, and one including a left-turn lane. These examples are intended to be illustrative.

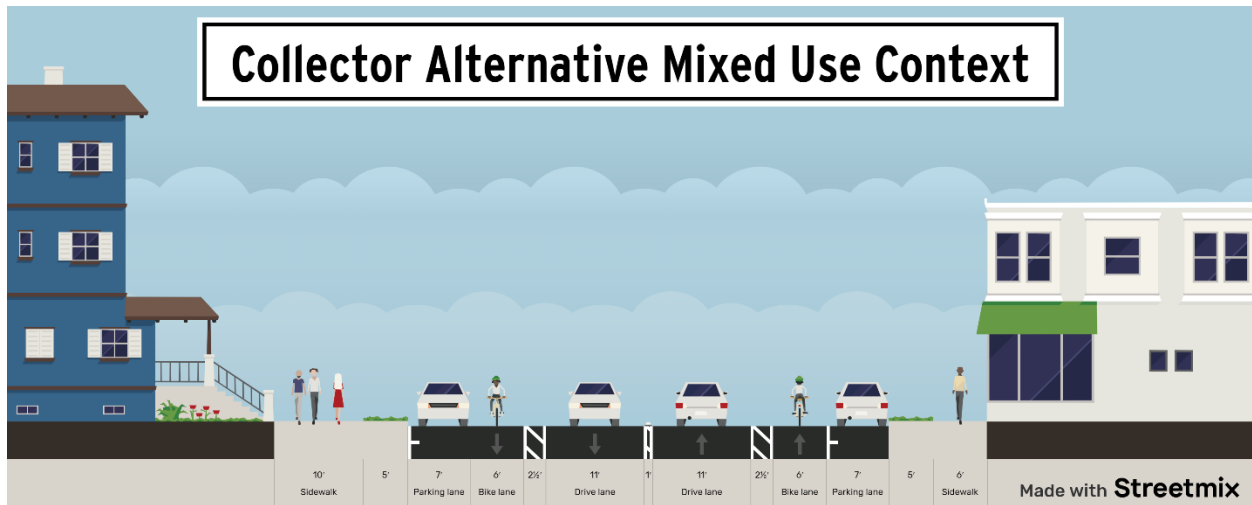
Figure 2. Residential Context



Notes:

- 10' sidewalk on the left (north side or east side) of street represents a multi-use path
- Park/Landscape strip of five (5) includes six inches on each side for concrete curbs abutting sidewalk and the parking lane. The strip for landscaping would be four (4) feet in width.
- Drive lanes are shown in this example at 11'-6" to reflect the 1' narrow double yellow centerline stripe between the lanes.
- Buffers of 2.5' provided between bicycle lanes and on-street parking for car door opening. Buffers can be placed between bike lane and moving traffic lane instead.

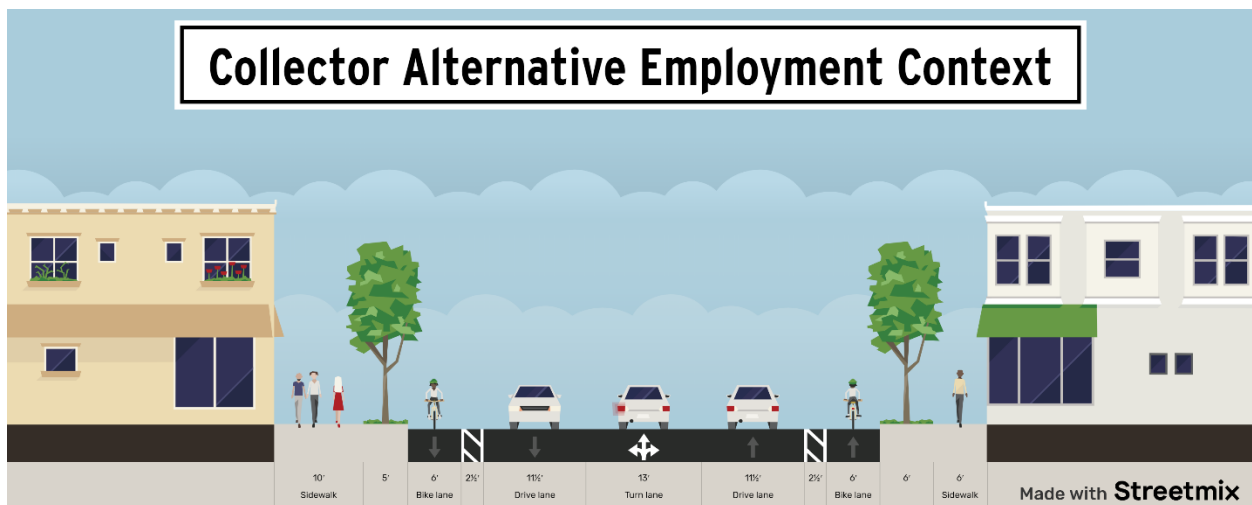
Figure 3. Mixed Employment/Mixed Use Context



Notes:

- 10' sidewalk on the left (north side or east side) represents a multi-use path
- On-street parking slightly wider
- Bike lanes both 6 feet with 2.5' buffers (buffers can be moved out to between travel lanes)
- Both cross sections for a more residential context and a mixed-use context are similar

Figure 4. Employment/Industrial Context



Notes:

- 10' sidewalk on left (north or east side) side represents multi-use path
- On-street parking replaced with a left-turn lane (assuming approach to intersection)
- Room left over to expand planting strips, sidewalk, and/or bike lanes