



BEND MPO
Metropolitan Planning Organization

Technical Advisory Committee Meeting

May 1, 2024

Call to Order & Introductions

Tyler Deke

Call to Order & Introductions

TAC Members

- Henry Stroud, Bend Park and Recreation District (BPRD)
- Sharon Smith, Bend La Pine Schools (BLS)
- Robin Lewis, City of Bend
- Eric Lint, Cascade East Transit (CET)
- Greg Bryant, Citizen Representative
- Josh Clawson, Central Oregon Community College (COCC)
- Brian Potwin, Commute Options
- Tarik Rawlings, Deschutes County
- Dave Thomson, Deschutes County BPAC
- Ken Shonkwiler, ODOT Region 4
- Casey Bergh, OSU Cascades
- Angie Brewer, Department of Land Conservation & Development (DLCD)*
- Jasmine Harris, Federal Highway Administration (FHWA)*
- Danielle Casey, Federal Transit Administration (FTA)*

BMPO Staff

- Tyler Deke, Manager*
- Andrea Napoli, Senior Planner
- Kelli Kennedy, Program Coordinator

Agency & Consultant Staff

- Don Morehouse, ODOT

Members of the public, visitors, and presenters will be asked to introduce with name and entity.

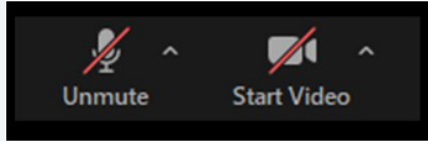
*Non-voting members

Hybrid Meeting Guidelines

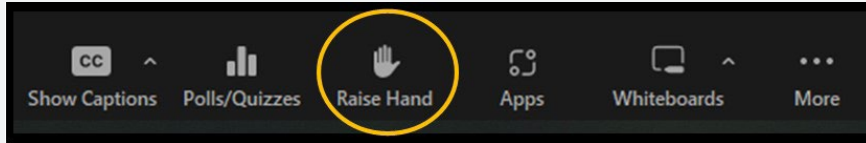
Tyler Deke

Hybrid Meeting Guidelines

- You will be on **mute**, and your **video** will be off when you first join the meeting.



- Please click the **raise hand** icon to speak next.



- If you join the webinar by phone, dial ***9** to raise or lower your hand.
- This meeting will be recorded and is available as a live streaming event on YouTube. The YouTube event can be reviewed on the City of Bend YouTube channel.

Public Comment

Tyler Deke

Public Comment

- Time for members of the public to provide comment.
- Additional time for public comment will be provided before the meeting adjourns.

Meeting Summary

Tyler Deke

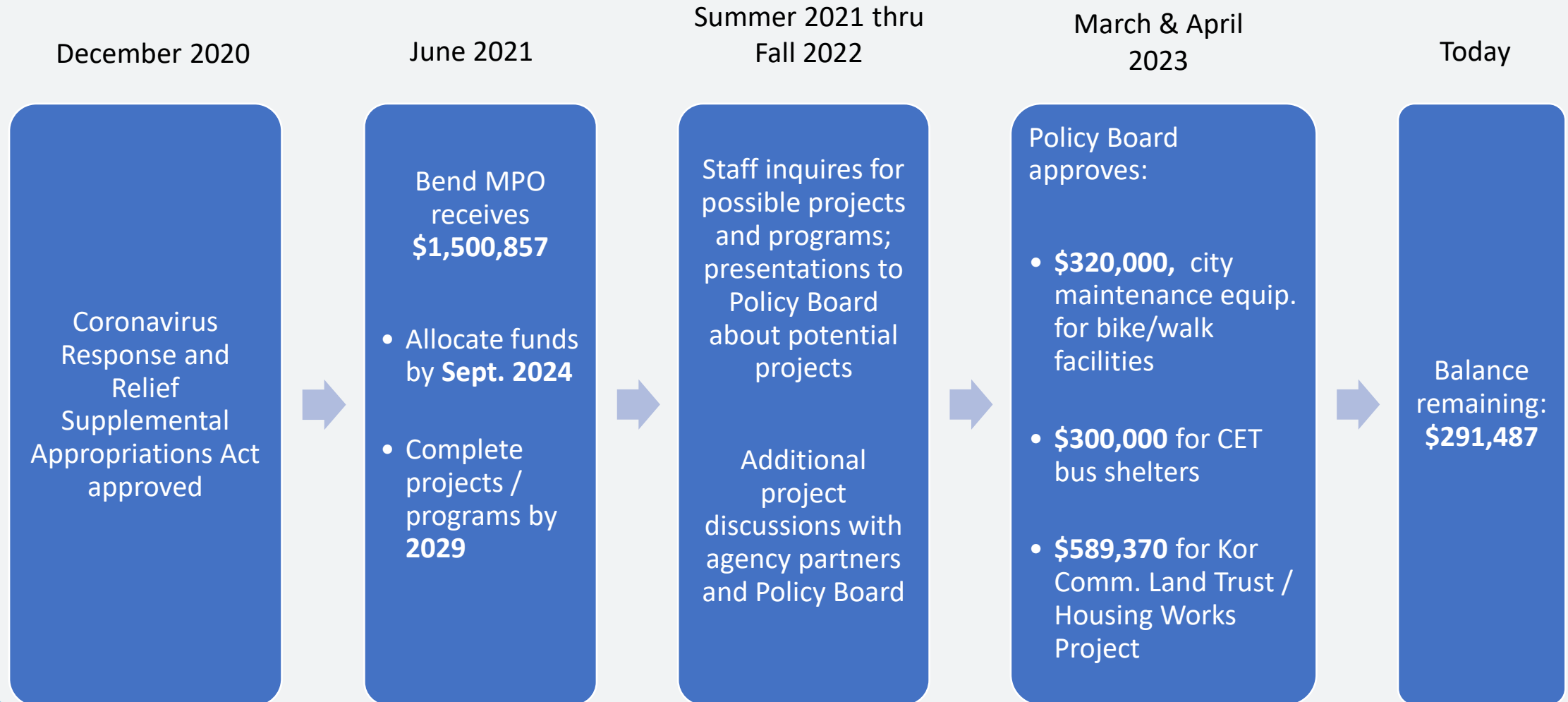
Meeting Summary

- Review and approve April 3, 2024, draft meeting summary (Attachment A).
- Recommended language for motion: *I move approval of the April 3, 2024, Technical Advisory Committee draft meeting summary as presented.*

Federal COVID Funding

MPO Staff

Federal COVID Funding - Timeline



Federal COVID Funding

- Board ideas for use of remaining funding mixed
 - Bike/ped, safety, accessibility, projects that don't have other funding source
 - Gap funding for programmed projects
 - Focus on projects with broader impacts/benefits, not localized impacts/benefits
- Seeking info on higher-priority projects in need of funding (\$291k)
 - Possibly one priority per agency/jurisdiction
 - Staff will take ideas to Board

Metropolitan Transportation Improvement Program (MTIP) Amendments Review

MPO Staff

MTIP Amendments Review

- 2024-2027 MTIP background
 - Short-term (4-year) listing of projects and programs
 - Covers federal fiscal years 2024-2027 (October 1, 2023 – September 30, 2027)
 - Adopted in May 2023
 - Included 25 projects at time of adoption

- Projects currently listed = 47 (soon to be 45)
 - 8 added with Policy Board approval
 - 14 slipped forward from 2021-2024 MTIP into current MTIP
 - 2 pending removal, due to change in funding source

2024-2027 MTIP Approved Project Totals (May 19, 2023)				
AGENCY	TOTALS			
	Federal	State	Local	Grand TOTAL
Bend MPO	\$ 4,039,812	\$ 94,463	\$ 367,912	\$ 4,502,187
City of Bend	\$ 7,505,805	\$ -	\$ 859,073	\$ 8,364,878
ODOT	\$ 6,611,290	\$ 1,485,202	\$ 57,880	\$ 8,154,371
Transit	\$ 11,570,685	\$ -	\$ 6,570,114	\$ 18,140,798
ALL AGENCY TOTAL	\$ 29,727,591	\$ 1,579,665	\$ 7,854,978	\$ 39,162,235

2024-2027 MTIP Current Project List Totals (as of April 23, 2024)				
AGENCY	TOTALS			
	Federal	State	Local	Grand TOTAL
Bend MPO	\$ 4,039,812	\$ 94,463	\$ 367,912	\$ 4,502,187
City of Bend	\$ 29,640,096	\$ 1,867,643	\$ 6,392,860	\$ 37,900,599
ODOT	\$ 33,420,602	\$ 4,876,530	\$ 2,176,158	\$ 40,473,291
Transit	\$ 17,954,059	\$ -	\$ 8,726,532	\$ 26,680,590
ALL AGENCY TOTAL	\$ 85,054,569	\$ 6,838,637	\$ 17,663,462	\$ 109,556,668

NOTE: totals may not reflect very recent increases/decreases. Typically, there is a lag between approval and updated amounts being reflected.

MTIP Amendments Review

- Amendments are required when certain adjustments are made (e.g., change in project cost, scope, or location).
 - **Full amendments** – require Policy Board adoption and 21-day public notification process.
 - **Administrative amendments** – may be forwarded to Policy Board and TAC for informational purposes in agenda packets or at meetings.
- Amendments processed since adoption = 36 (soon to be 38)
 - 14 full amendments, including 2 currently open for public comment through May 13
 - 24 administrative amendments
 - Processed in ODOT system (ongoing, as needed), tracked internally
 - Coordinated with ODOT staff

MTIP Amendments Review

- Public involvement and comment process:
 - 21-day public notification process/comment period prior to Policy Board review
 - Comments provided to Policy Board for consideration
 - Opportunity for additional public comment at Policy Board meeting
 - 5 public comment periods for amendments since adoption, including one currently open
- Related documents available on the [BMPO MTIP webpage](#):
 - 2024-2027 MTIP
 - 2024-2027 MTIP Project List – updated quarterly (Attachment C)
 - 2024-2027 MTIP Amendment Log – updated quarterly (Attachment D)

2023-2025 Unified Planning Work Program (UPWP) Mid-Cycle Amendments

MPO Staff

2023-2025 UPWP Mid-Cycle Amendments

Three Programs

- MPO Work Program
- COVID Relief Program
- State Highway Fund (SHF) Program (new, formerly “STBG”)

Multiple Year MPO Work Program

- UPWP includes tasks and deliverables for FY24 and FY25 (covering July 1, 2023 to June 30, 2025)

The funding levels in the budget tables show FY24 and FY25

- Amend mid-cycle to reflect updated budget information & work priorities



FY25 Priorities

Highest Priorities

- **Metropolitan Transportation Plan - update due September 2024**
 - Updated chapters, funding analysis and project lists
 - Travel demand model update
- Climate change planning and rulemaking
 - Oregon Climate Friendly and Equitable Communities rules
 - Federal GHG Performance Measure
- Maintain 2024-2027 MTIP, including State Highway Funds management
- Update TSAP & TSAP implementation
- COVID Relief Funding

FY25 Priorities

Other Priorities

- US20 Refinement Plan (and other ODOT projects)
- Household Travel Survey
- Travel model updates & future strategy
- Data Development
- Annual Obligated Funding Report
- Oregon Modeling Statewide Collaborative
- Performance monitoring & reporting

2023-2025 UPWP Mid-Cycle Amendments

Significant Plan & Budget Changes

- State Highway Fund (formerly STBG) revenues
 - MPO now direct recipient of SHF
 - MPO will need to manage and transfer funds to recipients
- Federal *Safe Streets and Roads for All* grant
 - \$200,000 federal grant to update Bend Transportation Safety Action Plan
 - May work directly with Deschutes County on plan updates
- Transportation Safety Action Plan (TSAP) Implementation Grant
 - ODOT grant funding for public safety messaging throughout Central Oregon
- Reserves
 - Reserves are a new line-item in the FY25 budget. The reserves are unspent SHF revenues
- COVID revenues
 - About \$290,000 balance
 - Needs to be awarded by September 2024

US97/Baker Road Interchange Area Management Plan (IAMP)

Don Morehouse, ODOT

US 97 BAKER RD INTERCHANGE AREA MANAGEMENT PLAN (IAMP)

BEND MPO TAC MEETING
MAY 1, 2024

SHAPING A SMARTER
TRANSPORTATION EXPERIENCE™
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AGENDA

1 / PROJECT STATUS

2 / REVIEW AND DISCUSS DRAFT IAMP

PROJECT STATUS

WHAT WE DID

- Project definitions and background
- Goals and Objectives
- Evaluate baseline conditions
- Developed and evaluated alternatives
- Selected a preferred alternative
- Developed access management plan and implementing actions
- 2 Online Open Houses
- 5 meetings with our TAC and CAC
- 3 meetings with the Bend MPO Policy Board

WHAT REMAINS

- Revise Draft IAMP as needed
- Present IAMP to:
 - > Bend Planning Commission and Transportation Commission
 - > County Planning Commission
 - > County Board of Commissioners

REPORT ORGANIZATION

- Executive Summary
- Chapter 1. Introduction
- Chapter 2. Project Process
- Chapter 3. Existing and Future Needs
- Chapter 4. Recommended Interchange Improvements
- Chapter 5. Access Management Plan and Management Actions
- Chapter 6. IAMP Implementation and Adoption

BRIEF REVIEW OF ALTERNATIVES (CH 4)

- Alternative 1: Enhance Existing Ramp Terminals - Preferred Option
- Alternative 2: Tight Urban Diamond Interchange (TUDI)
- Alternative 3: Southbound On- And Off-Ramp Flyovers with Roundabout (Flyover Interchange) - Long Term Option

Alt. 1: Enhanced Existing Ramp Terminals

- Adds signals and/or a roundabout at existing ramp terminals
- Lengthens southbound off-ramp
- Eliminates turning conflicts between closely spaced intersections (realignment)
- Lowest cost option (original est. cost: **\$14.1 Million**)



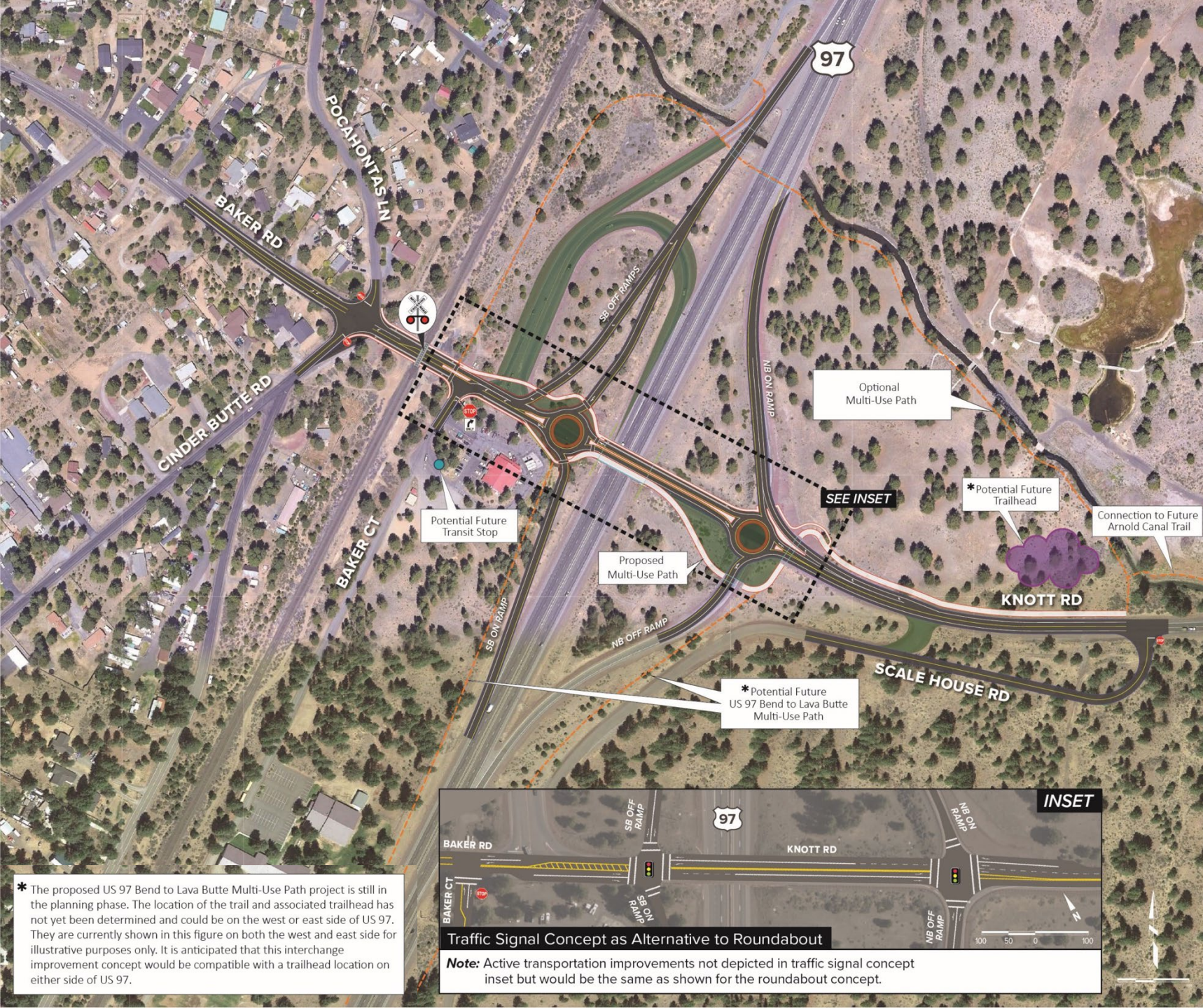
Traffic Signal Concept as Alternative to Roundabout

Note: Active transportation improvements not depicted in traffic signal concept inset but would be the same as shown for the roundabout concept.

* The proposed US 97 Bend to Lava Butte Multi-Use Path project is still in the planning phase. The location of the trail and associated trailhead has not yet been determined and could be on the west or east side of US 97. They are currently shown in this figure on both the west and east side for illustrative purposes only. It is anticipated that this interchange improvement concept would be compatible with a trailhead location on either side of US 97.

Alt. 2: Tight Urban Diamond Interchange (TUDI)

- Reconstructs the interchange to use a more traditional “diamond” configuration
- Adds signals and/or roundabouts at ramp terminals
- Includes lengthening of the southbound off- and on-ramps
- No direct left out of Baker Court – must U-turn at roundabout.
- Includes an at-grade crossing with the multi-use path on the south side of Baker Road
- Middle cost option (original est. cost: **\$18.3 Million**)



* The proposed US 97 Bend to Lava Butte Multi-Use Path project is still in the planning phase. The location of the trail and associated trailhead has not yet been determined and could be on the west or east side of US 97. They are currently shown in this figure on both the west and east side for illustrative purposes only. It is anticipated that this interchange improvement concept would be compatible with a trailhead location on either side of US 97.

INSET

97

BAKER RD

KNOTT RD

SB OFF RAMP

NB OFF RAMP

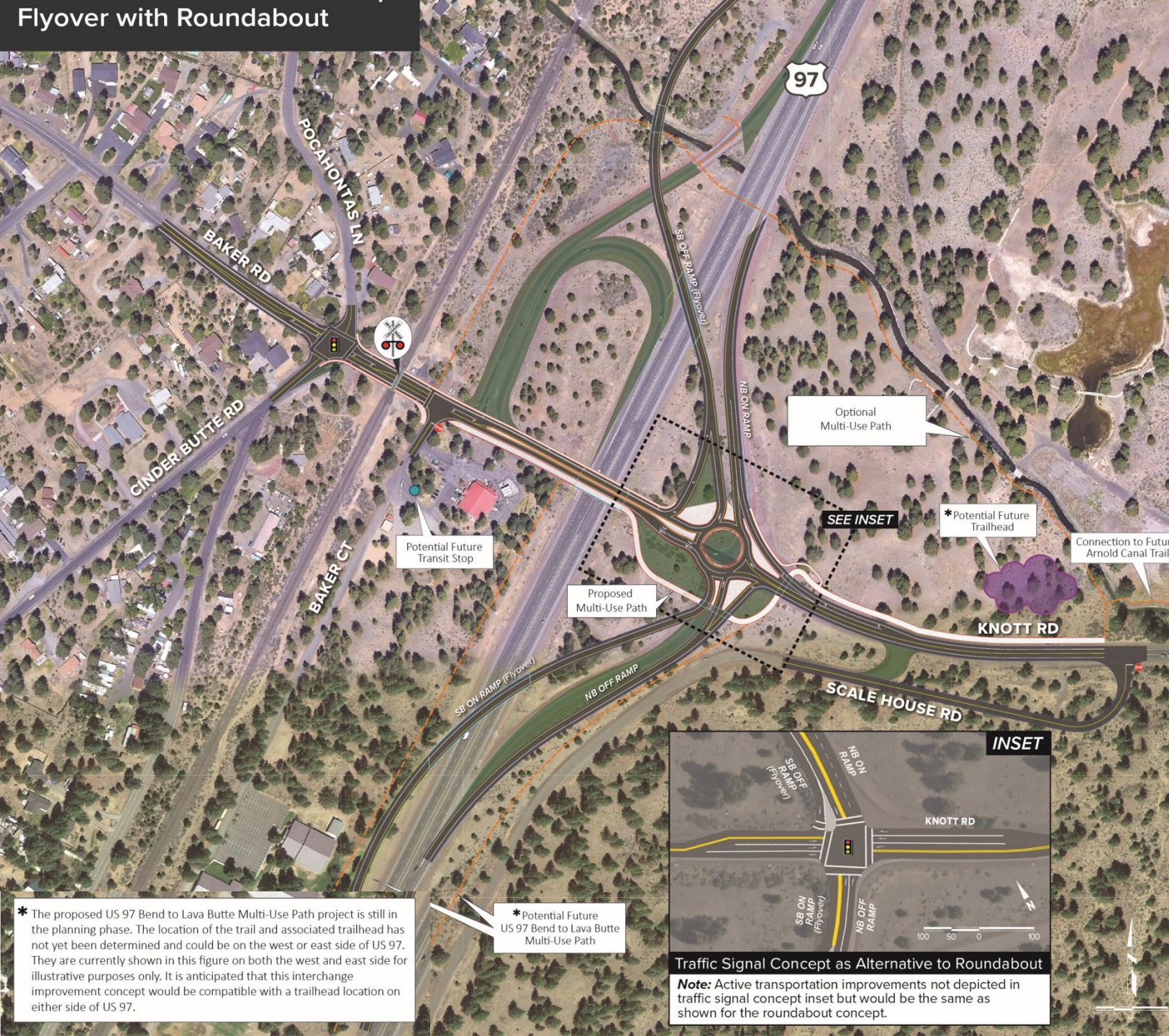
SB ON RAMP

NB ON RAMP

100 50 0 100

Traffic Signal Concept as Alternative to Roundabout

Note: Active transportation improvements not depicted in traffic signal concept inset but would be the same as shown for the roundabout concept.



Alt. 3: Southbound On- And Off-Ramp Flyovers with Roundabout (Flyover Interchange)

- Reconstructs the US 97 southbound on- and off-ramps by realigning them to a shared intersection with the northbound ramps on the east side of US 97
- This would require new bridges over US 97 for the southbound on- and off-ramps and a new bridge over the Arnold Canal
- All of the on- and off- ramps would connect at one partial multilane roundabout intersection
- Highest cost option (original est. Cost: **\$34.5 Million**)

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*Potential Future US 97 Bend to Lava Butte Multi-Use Path

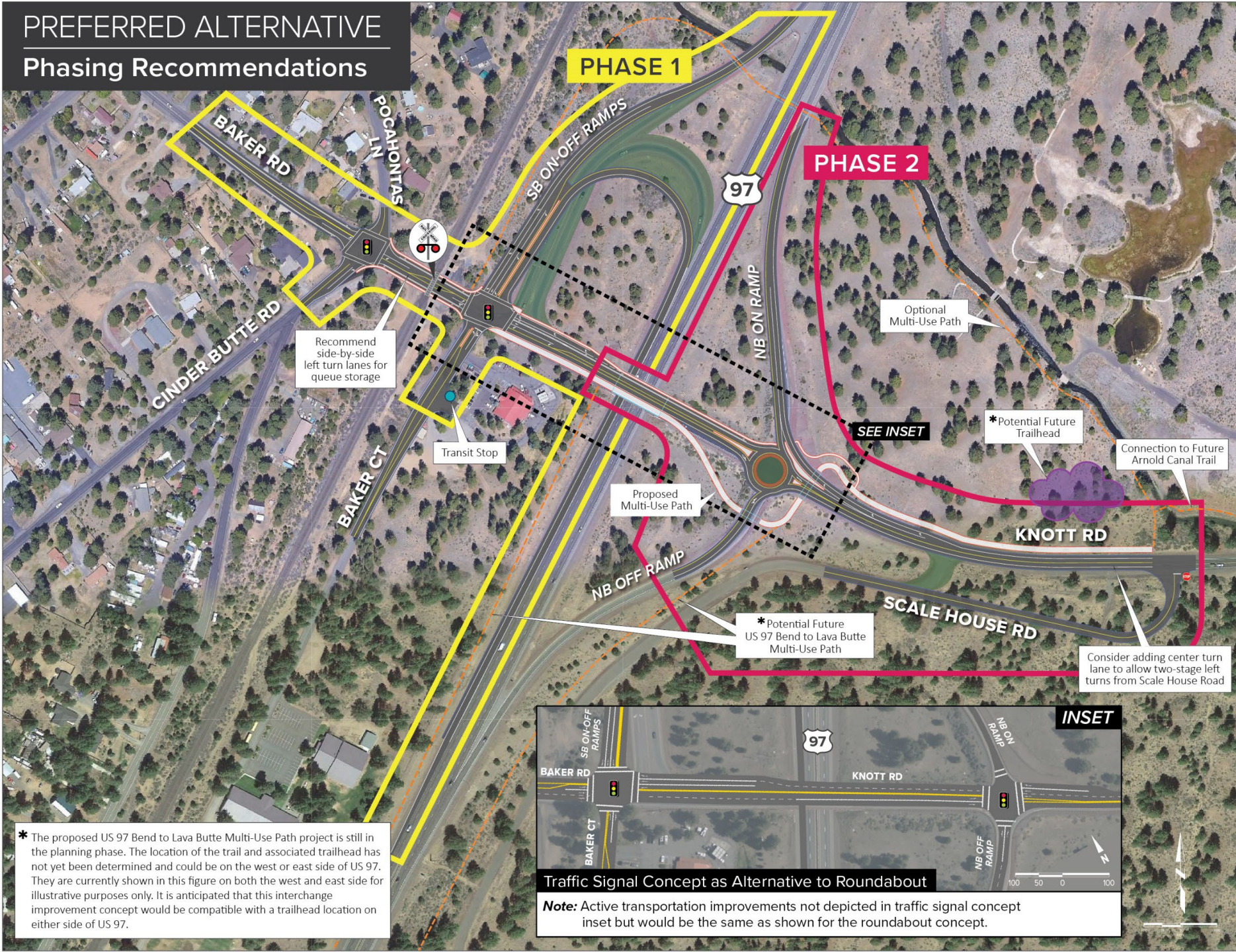
Traffic Signal Concept as Alternative to Roundabout

Note: Active transportation improvements not depicted in traffic signal concept inset but would be the same as shown for the roundabout concept.

RECOMMENDATION

- Alternative 3 is widely supported, but also recognized to be significantly more costly and harder to build and phase.
 - > Many have expressed a desire to get improvements soon.
- Therefore, Alternative 1 was selected as the preferred alternative.
 - > Alternative 1 does a better job than Alternative 2 at resolving access conflicts and improving Baker Ct. access and reducing conflicts for people walking and biking.
 - > Alternative 1 is lower cost and can be more easily phased.
- Alternative 3 can be considered in the future.

PREFERRED ALTERNATIVE
Phasing Recommendations



Preferred Alternative –
 Alt 1 with
 consideration for
 future Alt 3
 improvements

Alternative 1 Phasing:

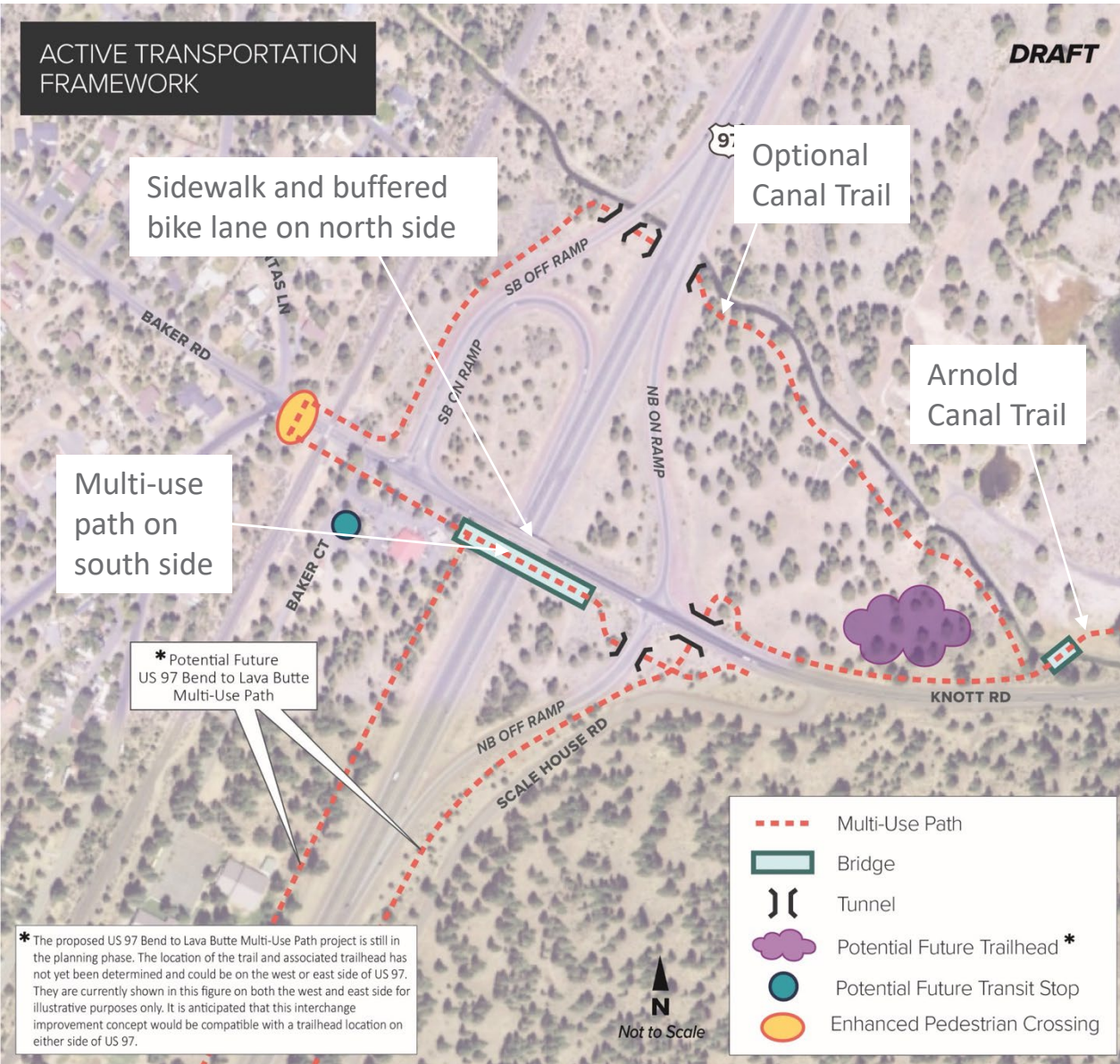
- Phase 1 - \$14.8 million
- Phase 2 - \$23.2 million
- **Total: \$38 million**

* The proposed US 97 Bend to Lava Butte Multi-Use Path project is still in the planning phase. The location of the trail and associated trailhead has not yet been determined and could be on the west or east side of US 97. They are currently shown in this figure on both the west and east side for illustrative purposes only. It is anticipated that this interchange improvement concept would be compatible with a trailhead location on either side of US 97.

Traffic Signal Concept as Alternative to Roundabout

Note: Active transportation improvements not depicted in traffic signal concept inset but would be the same as shown for the roundabout concept.

Active Transportation Improvements



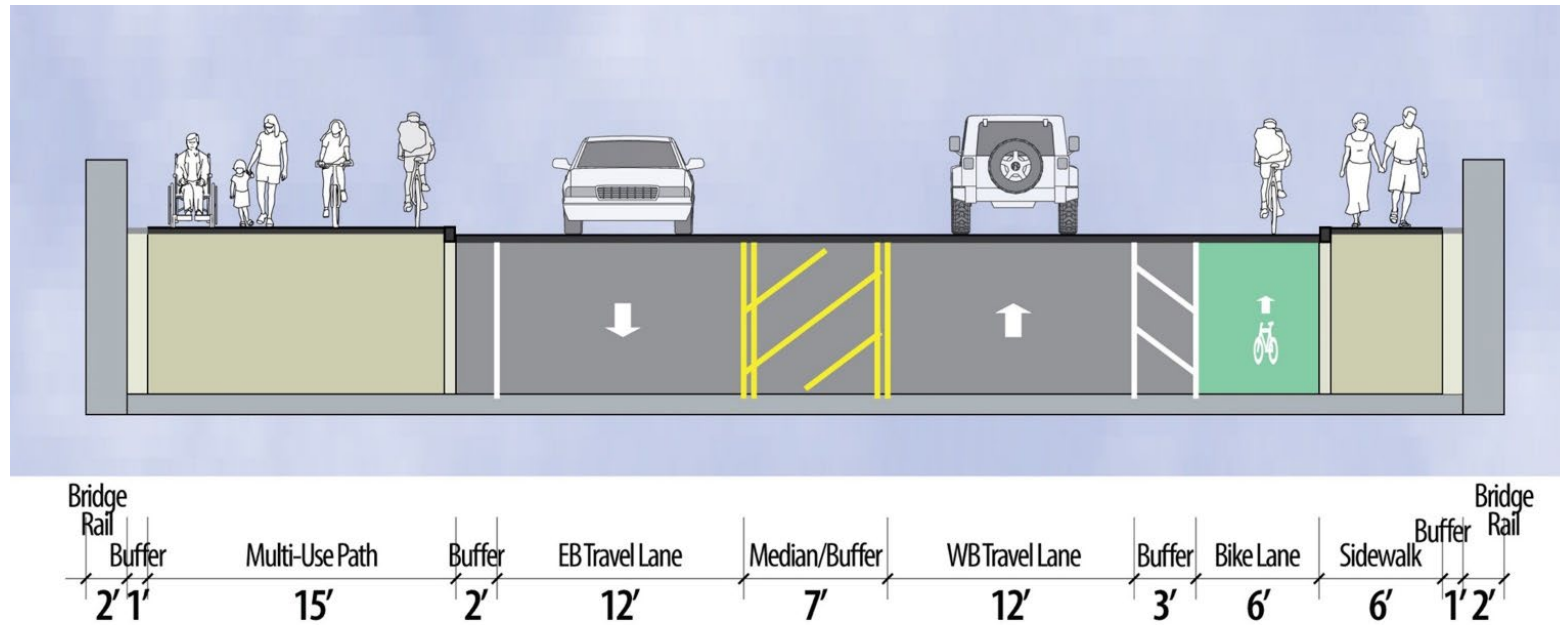
West end of Interchange

- Multi-use path, enhanced crossings (signals), transit stop

East end of Interchange

- Multi-use path with tunnels under the US 97 northbound off-ramp and Knott Road
- Connection to the future Arnold Canal Trail
- Potential for a future trailhead to be located in the northeast quadrant, connecting to the paved multi-use path that is planned between this interchange and the Lava Lands Visitor Center (the path could be either on the west or east side of US 97)

Baker Road Cross Section with Active Transportation Improvements



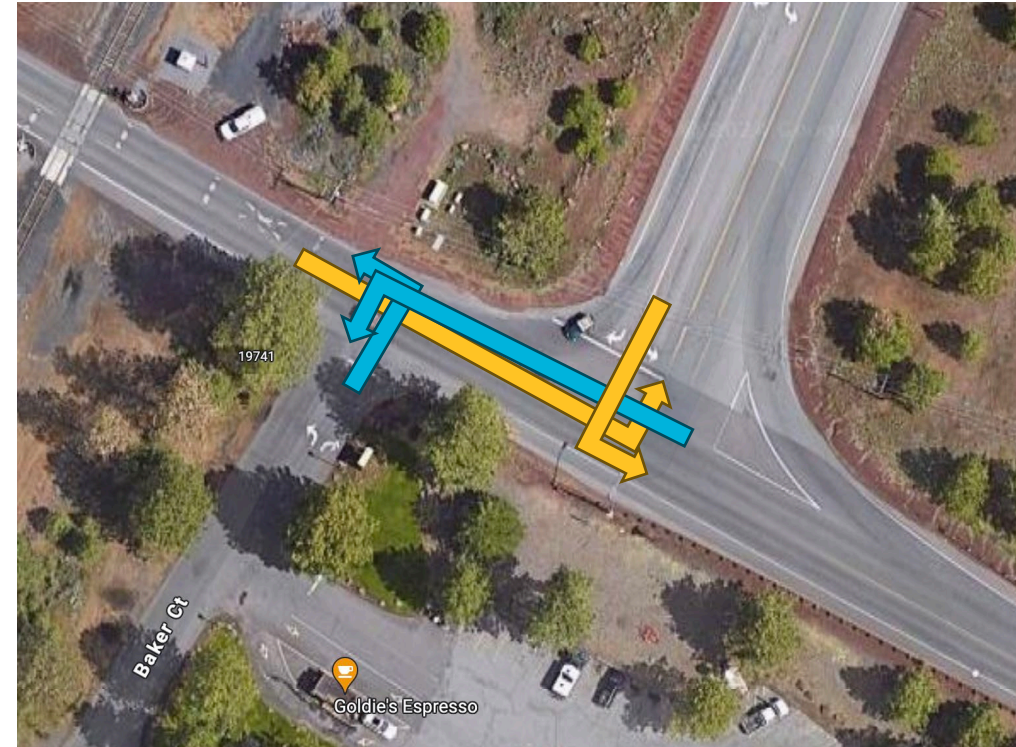
Additional Refinements:

- Vertical barriers.
- Narrow median for wider multi-use path on north side.
- Delineate space between people walking and biking on the multi-use path.
- Visual or physical gateway elements on new bridge.

ACCESS MANAGEMENT PLAN (CH 5)

What is an Access Management Plan and Why does the IAMP have one?

- Protect the investment being made in transportation improvements.
- Protect public safety.
- IAMPs are required to have access management plans.
- It provides direction and decision-making criteria, but actual changes will occur through future land use applications and project design phases.



ACCESS MANAGEMENT PLAN (CH 5)

Key Principles and Methodology

- Provide direction and decision-making criteria for access changes
- Modeled after the project goals and objectives (Key Principles = Goals)
- Mailed to property owners for review and comment (Nov 6)
- The Methodology will again be subject to property owner comment with each design phase. This would also include specific actions and more public outreach.

ACCESS MANAGEMENT PLAN (CH 5)

Access Management Actions

- Would be implemented by ODOT, Deschutes County, or others as applicable
 - > Likely as part of future land use actions or project phases
- Actions are described for each Area and are somewhat general in nature to allow for appropriate discretion for each decision in the future

TABLE 14: ACCESS MANAGEMENT ACTIONS

AREA 1	
EXISTING CONDITIONS: Area 1 includes properties along Baker Road between the BNSF railroad and a point that is approximately 1,320 feet west of the future intersection of the US 97 southbound ramps/Baker Court. Properties in this area are generally developed with single-family homes, consistent with Rural Residential (RR10) zoning. Many properties only have access to Baker Road and must rely on it for access, though some also abut Apache Road, Cinder Butte Road, or Pocahontas Lane.	ACTIONS: This area is fully developed and there are no known plans to rezone the land or construct new public streets in the vicinity. Properties that are unable to access alternative public streets are anticipated to continue to have access directly to Baker Road. The public street intersection on Baker Road with Cinder Butte Road and Pocahontas Lane is proposed to be improved with turn lanes and a traffic signal. No changes are proposed to the intersection with Apache Road. As opportunities arise, such as through development and redevelopment of properties, access directly to Baker Road should be removed to properties that abut other public streets with lower functional classifications (i.e., Apache Road, Cinder Butte Road, or Pocahontas Lane) where access to those other streets is feasible and safe to construct. For properties without access to an alternative public street, no more than one direct access to Baker Road should be provided where feasible.

OTHER MANAGEMENT STRATEGIES (CH 5)

- Consider **reducing the posted speed** along Baker Road and Knott Road after the improvements are constructed (from 35 mph).
- Maintain and enhance **evacuation routes** for southwest and southeast communities – particularly during construction.
- Add **wayfinding signage** when regional trails and trailheads are constructed.
- Manage the new **interconnected traffic signals** on Baker Road at the US 97 southbound ramps and Cinder Butte Road to prioritize safety.
 - > Assumes ODOT operates and maintains the Cinder Butte Rd signal too.
- Maintain the ability to safely and conveniently **accommodate transit** access.
 - > Future changes to the Baker Ct. cul-de-sac

PLAN IMPLEMENTATION (CH 6)

- Deschutes County will adopt the IAMP as a legislative amendment to the Comprehensive Plan
- ODOT will adopt the IAMP as a refinement to the Oregon Highway Plan
- Bend MTP to include an updated project description as part of fall 2024 update
- Plan and design phases also must be brought to the Mobility Advisory Committee

Table 6-4: Aspirational Project List for State Facilities

Project #	Project Name	Location	Project Description	Planning Level Cost (\$1,000s)
N-4	US 97 operational and safety management improvements and associated City street improvements	US 97 within Bend MPO boundary	Includes potential recommended Parkway Plan projects such as RI/RO Access Modifications/Closures, Ramp Meters, Butler Market Interchange Improvements, Revere Ave Lane Re-allocation, US 97 Auxiliary Lanes, Baker/Knott Interchange ramp terminal improvements, etc.	TBD
Total State Facility Aspirational Projects Cost: >380³⁹ million				

³⁹ The cost estimate for project C-19 will be informed by the on-going Deschutes County ITS plan, and the estimate for project N-4 will be informed by the on-going US 97 Bend Parkway Study

QUESTIONS?

Member & Guest Roundtable

Time for TAC member to provide updates on current projects and planning efforts.

Public Comment

Time for members of the public to provide comment.

Next TAC Meeting

The next meeting of the BMPO TAC is scheduled for
June 5, 2024, at 10:00 a.m.

Adjourn

Tyler Deke

Accommodation Information for People with Disabilities



To obtain this information in an alternate format such as Braille, large print, electronic formats, etc. please contact Kelli Kennedy at kkennedy@bendoregon.gov or 541-693-2122; Relay Users Dial 7-1-1.