

CUL-DE-SAC GENERAL NOTES:

1. SEE R-1 FOR GENERAL NOTES.
2. SEE STD DWG R-1 AND R-1D/E FOR PAVEMENT AND BASE AGGREGATE DEPTHS ON LOCAL ROADS. TURN AROUNDS ARE TO BE PAVED UNLESS OTHERWISE APPROVED WHEN LOCATED AT THE TERMINUS OF A PHASED SUBDIVISION WITH ANTICIPATE STREET CONTINUATION WITHIN A YEAR.
3. 10' EASEMENT REQUIRED ON THE T-COURT FOR PEDESTRIAN PATH. SEE DEVELOPMENT CODE 3.8.400C3
4. TURN AROUNDS TO BE IN COMPLIANCE WITH OREGON FIRE CODE.

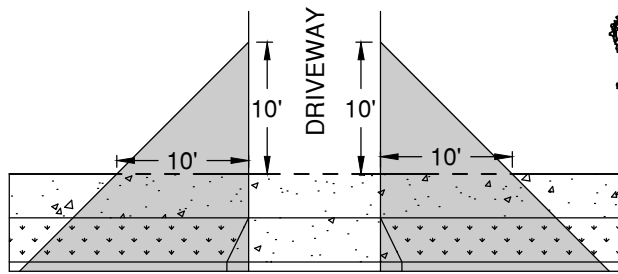
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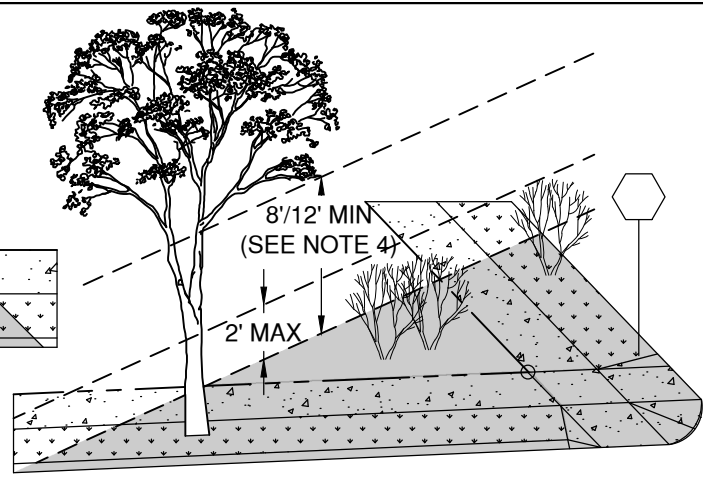
**CITY OF BEND**  
STANDARD DRAWING  
710 NW WALL ST., BEND, OREGON 97701

**TYPICAL STREET DEAD-END TURNAROUND**

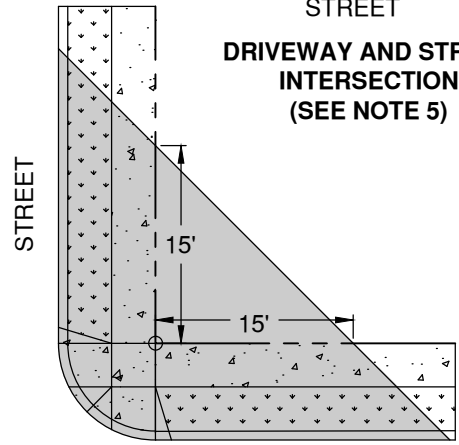
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DATE 11/01/2024
APPR
STD DWG R-1H



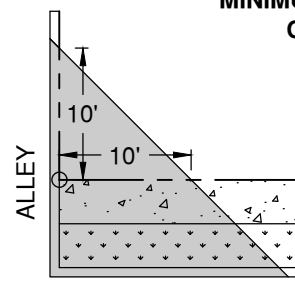
DRIVEWAY AND STREET INTERSECTION (SEE NOTE 5)



MAX HEIGHT OF SHRUBS AND MINIMUM LIMBING REQUIRED IN CLEAR VISION AREA



STREET/STREET INTERSECTION



STREET/ALLEY INTERSECTION

○	PROPERTY CORNER
---	PROPERTY LINE
▨	SIDEWALK
▨	LANDSCAPE BUFFER
▨	CLEAR VISION AREA

CLEAR VISION AREAS ARE ESTABLISHED AS FOLLOWS:

- CLEAR VISION TRIANGLES SHALL BE ESTABLISHED AT THE CORNER OF ANY PROPERTY ADJACENT TO INTERSECTIONS OF PUBLIC OR PRIVATE STREETS, ALLEYS, MID-BLOCK LANES, AND/OR RAILROAD RIGHTS-OF-WAY.
- THE TWO LEGS OF THE CLEAR VISION TRIANGLE ARE EACH MEASURED FROM THE POINT OF INTERSECTION OF THE TWO CORNER LOT LINES, SPECIAL SETBACK LINES, OR ACCESS EASEMENT LINES. WHERE LOT LINES HAVE ROUNDED CORNERS, THE LOT LINES ARE EXTENDED IN A STRAIGHT LINE TO A POINT OF INTERSECTION. THE CLEAR VISION AREA EXTENDS TO THE FACE OF CURB AT THE STREET OR ALLEY
- THE LENGTH OF BOTH LEGS OF THE CLEAR VISION AREA TRIANGLE IS AS FOLLOWS:  
 TYPICAL, ALL ZONES: 15 FEET  
 RAILROADS: 15 FEET  
 ALLEY INTERSECTION: 10 FEET  
 DRIVEWAYS: 10 FEET
- WITHIN THE CLEAR VISION AREA, OBSTRUCTIONS TO VISION OTHER THAN A STREET SIGN, HYDRANT, CROSSWALK, CLOSED SIGNS, SIGN POST, OR POLE LESS THAN 8 INCHES IN DIAMETER SHALL BE CLEARED FROM PROPERTY UNDER THE CONTROL OF THE CITY, HOMEOWNER, OR DEVELOPER. SHRUBS OR FOLIAGE MUST NOT EXCEED 2'-0" IN HEIGHT. PLANTING NEW TREES OR INSTALLATION OF COMMUNICATION TOWERS AND TRANSFORMERS, ARE NOT PERMITTED WITHIN THE CLEAR VISION AREA. EXISTING TREES MUST BE MAINTAINED/LIMBED TO A MINIMUM OF 8'-0" ABOVE THE TOP OF CURB OR 12'-0" ABOVE ADJACENT BIKE LANES.
- DRIVEWAY APPROACHES, ON-STREET PARKING AND DRIVEWAYS ARE NOT PERMITTED WITHIN THE CLEAR VISION AREA. ON-STREET PARKING DESIGN DOES NOT INCLUDE SPACES WITHIN 20 FEET OF AN ACCESSIBLE RAMP OR WITHIN 10 FEET OF A DRIVEWAY APPROACH.

NOTE: INTERSECTION SIGHT TRIANGLES ARE DISTINCT FROM, AND IN ADDITION TO, CLEAR VISION AREAS. INTERSECTION SIGHT TRIANGLE DIMENSIONS VARY WITH STREET WIDTH, GEOMETRY, TOPOGRAPHY, AND POSTED SPEED; ADDITIONAL CLEARING AS NECESSARY TO PROVIDE CLEAR INTERSECTION SIGHT DISTANCE IS ALSO REQUIRED; SEE CHAPTER 3.3 OF THE CITY OF BEND DESIGN STANDARDS.

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DIV	ROADWAY
REV	DATE



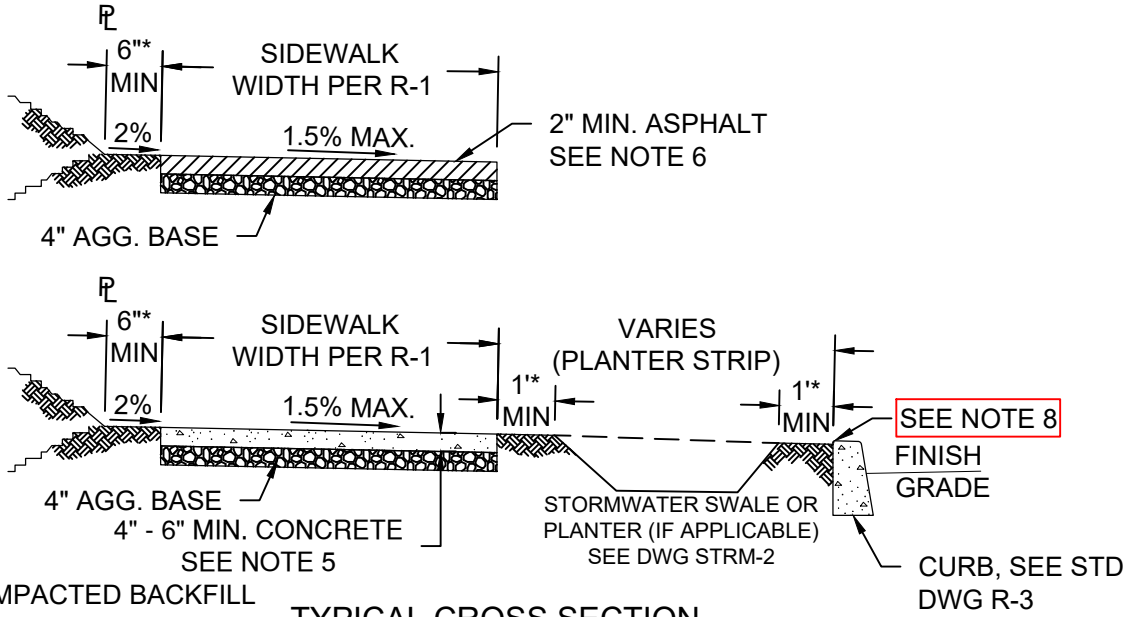
CITY OF BEND

CITY OF BEND  
STANDARD DRAWING

710 NW WALL ST., BEND, OREGON 97701

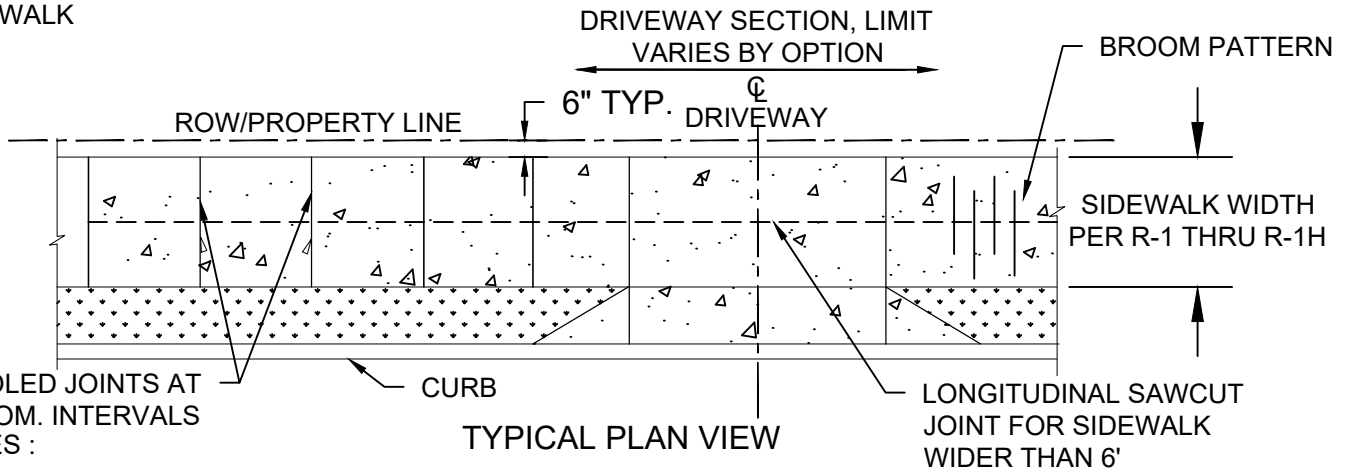
CLEAR VISION AREAS AT INTERSECTIONS

SCALE	NTS
DATE	11/01/2024
APPR	
STD DWG	R-2



\*PROVIDE COMPACTED BACKFILL ADJACENT TO CURB AND SIDEWALK

TYPICAL CROSS SECTION



- NOTES :
1. SIDEWALKS SET BACK ADJACENT TO PROPERTY LINE ARE STANDARD. USE CURB-TIGHT SIDEWALKS ONLY WHERE PERMITTED. SIDEWALK SHALL BE PROPERTY-TIGHT EXCEPT TO MEANDER AROUND TREES OR BARRIERS (UTILITIES, SIGNS, ETC.) OR PER DESIGN STANDARD SECTION 3.4.7 - HILLSIDE.
  2. CONST. EXPANSION JOINTS AT 25' MAXIMUM SPACING, AND AT POINTS OF TANGENCY, AND ON EACH SIDE OF DRIVEWAY APRONS. EXPANSION JOINTS MUST BE FULL DEPTH OF PAVING SECTION.
  3. CONST. CONTRACTION JOINTS AT 5' MAXIMUM SPACING, AND AT ENDS OF EACH RAMP.
  4. FOR DRIVEWAY DETAILS, SEE STD. DRGS. R-5A THROUGH R-5E.
  5. SIDEWALK THICKNESS MINIMUM 4" THICK , TYPICAL. MINIMUM 6" THICK IF SIDEWALK IS INTENDED AS PORTION OF DRIVEWAY, CURB RAMP, OR ADJACENT TO MOUNTABLE CURB.
  6. ASPHALT SHARED-USE PATH WHERE APPROVED BY THE ENGINEER.
  7. SIDEWALK BASE WILL BE VISUALLY INSPECTED FOR COMPACTION. CITY INSPECTORS CAN REQUIRE COMPACTION TESTING WHERE BASE ROCK COMPACTION IS VISUALLY OUT OF COMPLIANCE WITH SPECIFICATIONS 0040 / 00641.
  8. LANDSCAPING MUST BE 1/4 TO 1/2 INCH BELOW THE TOP OF ADJACENT SIDEWALK AND CURB.

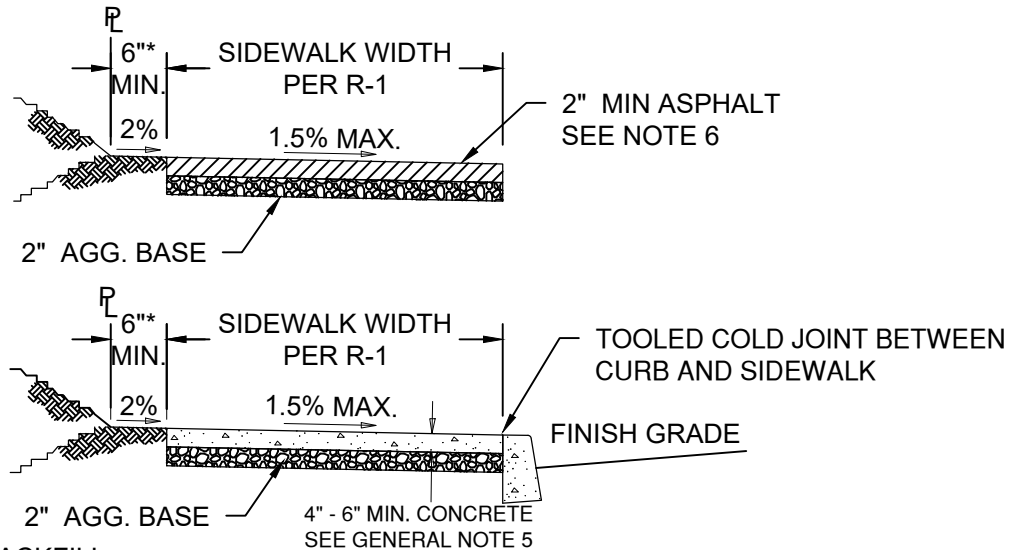
DRAWN CJH	
DIV ROADWAY	
REV	DATE



**CITY OF BEND**  
STANDARD DRAWING  
710 NW WALL ST., BEND, OREGON 97701

**SHARED-USE PATH/SIDEWALK, SETBACK**

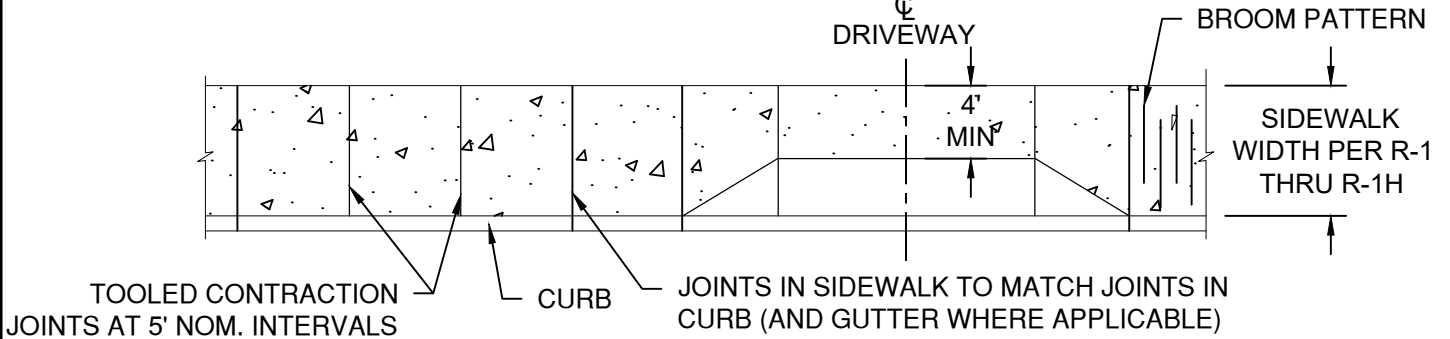
SCALE NTS
DATE 11/01/2024
APPR
STD DWG R-4A



\*PROVIDE COMPACTED BACKFILL ADJACENT TO CURB AND SIDEWALK

TYPICAL CROSS SECTION

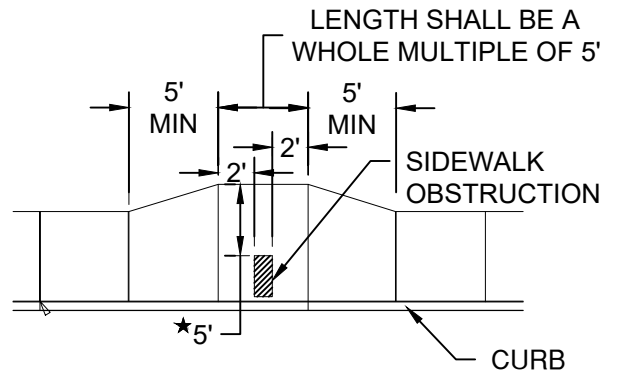
DRIVEWAY SECTION, LIMIT VARIES BY OPTION



TYPICAL PLAN VIEW

NOTES:

1. SIDEWALKS SET BACK ADJACENT TO PROPERTY LINE ARE STANDARD. USE CURB-TIGHT SIDEWALKS ONLY WHERE PERMITTED.
2. CONST. EXPANSION JOINTS AT 25' MAXIMUM SPACING, AND AT POINTS OF TANGENCY, AND ON EACH SIDE OF DRIVEWAY APRONS. EXPANSION JOINTS MUST BE FULL DEPTH OF PAVING SECTION.
3. CONST. CONTRACTION JOINTS AT 5' MAXIMUM SPACING, AND AT ENDS OF EACH RAMP.
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6. ASPHALT SHARED-USE PATH WHERE APPROVED BY THE ENGINEER.
7. SIDEWALK BASE WILL BE VISUALLY INSPECTED FOR COMPACTION. CITY INSPECTOR CAN REQUIRE COMPACTION TESTING WHERE BASE ROCK COMPACTION IS VISUALLY OUT OF CONFORMANCE WITH SPECIFICATIONS 0040 / 00641.



★ WHEN SITE CONSTRAINTS PROHIBIT A 5' PASSAGE, THE ENGINEER MAY DIRECT THIS TO BE REDUCED, BUT NO LESS THAN 4'.  
**REQUIRED SIDEWALK WIDENING AROUND OBSTRUCTIONS**

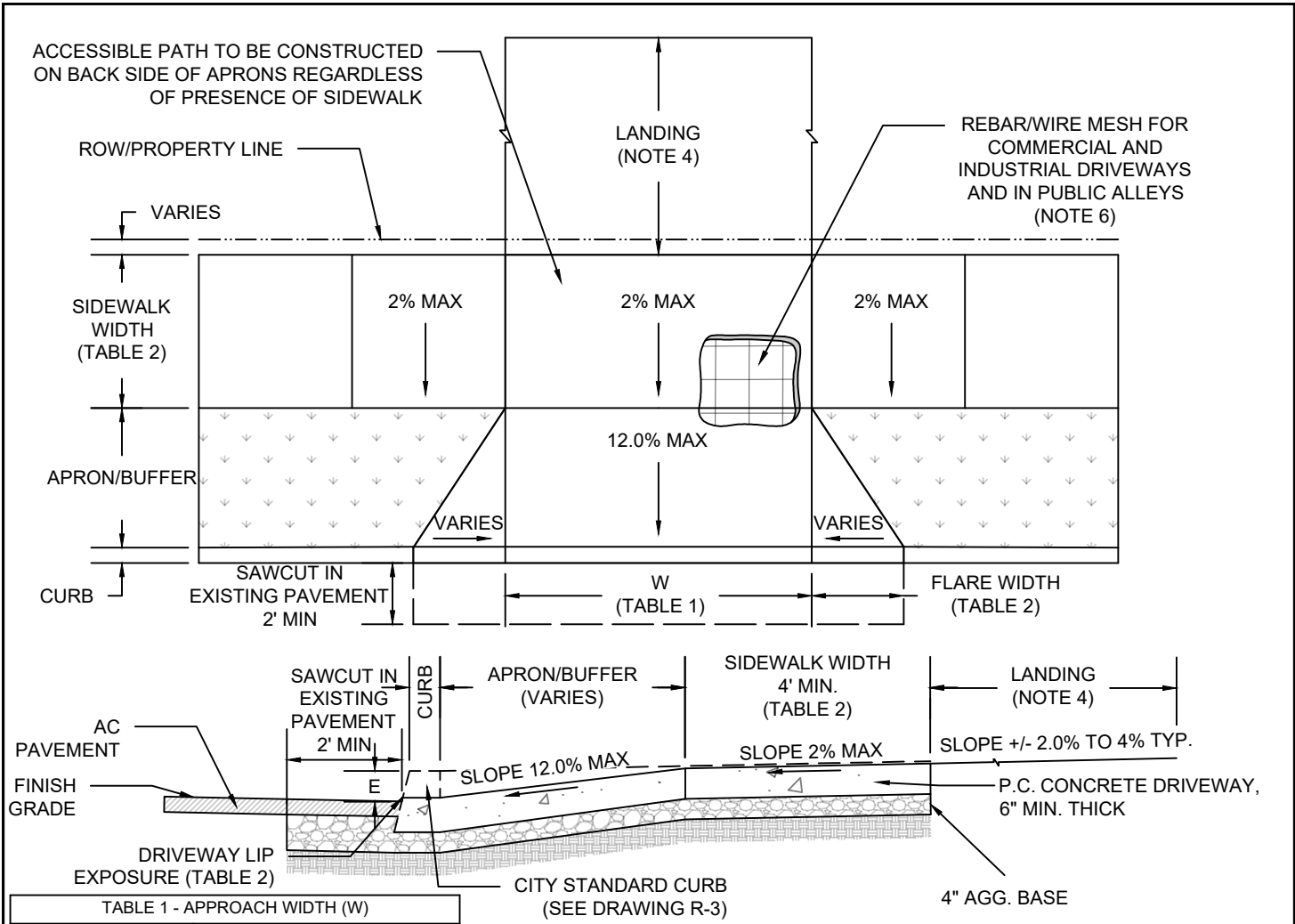
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REV	DATE



**CITY OF BEND**  
 STANDARD DRAWING  
 710 NW WALL ST., BEND, OREGON 97701

**SHARED-USED PATH/SIDEWALK, CURB-TIGHT**

SCALE NTS
DATE 11/01/2024
APPR
STD DWG R-4B



**TABLE 1 - APPROACH WIDTH (W)**

TYPE	WIDTH
RESIDENTIAL	10' - 24'
SINGLE FAMILY	10'-24'
TOWNHOME	32' MAX SHARED; 16' MAX LOCAL STREETS
DUPLEX	SUM 32' MAX (2 APRON MAX)
TRIPLEX	SUM 32' MAX (3 APRON MAX)
QUADPLEX	SUM 32' MAX (4 APRON MAX)
MULTIFAMILY	20' - 30'
COMMERCIAL	10' - 35'

**TYPICAL PLAN AND PROFILE VIEW  
DRIVEWAY APPROACH, SETBACK SIDEWALK (STANDARD)**

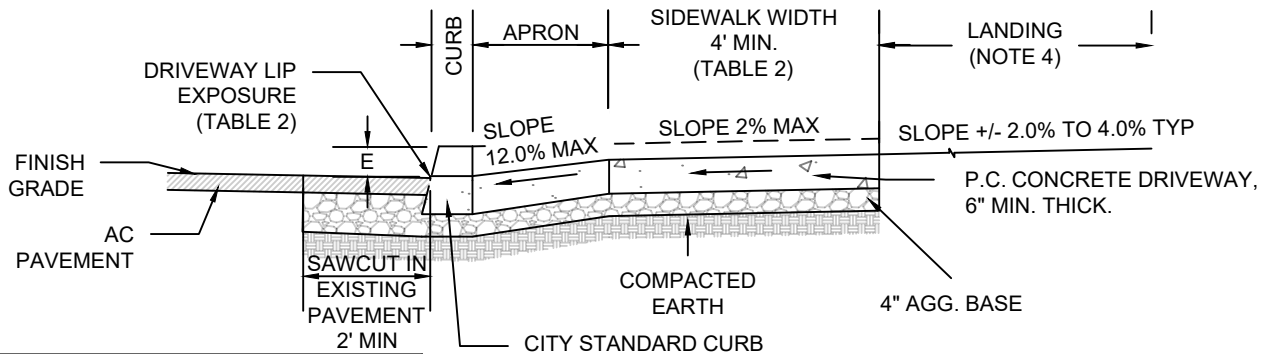
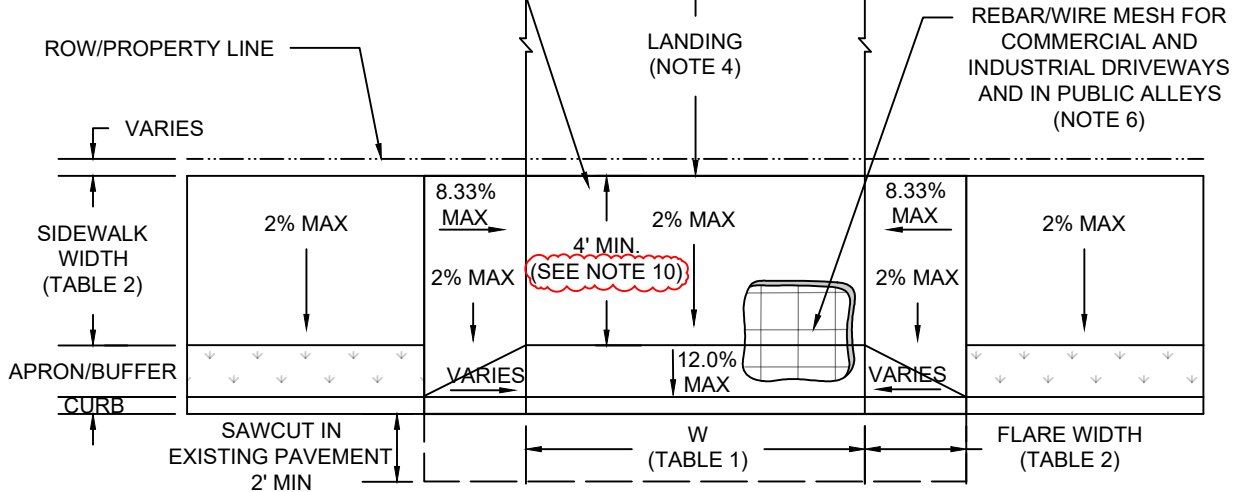
**TABLE 2 - DRIVEWAY APPROACH WITH SETBACK SIDEWALK SPECIFICATIONS**

TYPE OF STREET	SIDEWALK WIDTH	LIP EXPOSURE	APRON GRADE, POSITIVE GRADE TO ROW	FLARE WIDTH
LOCAL	PER R-1D THRU R-1F	3/4"	12.0% MAX	3'
COLLECTOR	PER R-1B & R-1-C	1"	12.0% MAX	6'
ARTERIAL	PER R-1A	1"	12.0% MAX	6'

- GENERAL NOTES:**
- SIDEWALKS SHALL MEET ALL STANDARDS OF CURRENT PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG).
  - CURB AND SIDEWALK TYPES VARY, SEE PLANS. SEE STD. DRG. R-3 FOR CURB DETAILS. SEE STD. DRGS. R-4A & R-4B FOR SIDEWALK DETAILS.
  - TOOLED JOINTS ARE REQUIRED AT ALL DRIVEWAY SLOPE BREAK LINES.
  - THE LANDING SHALL BE PAVED WITH CONCRETE OR ASPHALT SURFACE FOR A MINIMUM OF 20 FEET ON ALLEYS. LANDINGS ON PRIVATE LOTS SHALL BE IN ACCORDANCE WITH THE BEND DEVELOPMENT CODE. CONSTRUCT AS DIRECTED OR AS SHOWN ON PLANS. DO NOT ENTER PRIVATE PROPERTY WITHOUT APPROPRIATE PERMIT OR EASEMENT. MATERIAL WITHIN THE ROW SHALL BE CONCRETE.
  - CHECK THE GUTTER FLOW DEPTH AT DRIVEWAY LOCATIONS TO ASSURE THAT THE DESIGN FLOOD DOES NOT OVERTOP THE BACK OF SIDEWALK AT DRIVEWAY. IF OVERTOPPING OCCURS PLACE AN INLET AT UPSTREAM SIDE OF DRIVEWAY OR PERFORM OTHER APPROVED DESIGN MITIGATION.
  - #4 REBAR (20" ON CENTER, TO BE SUSPENDED TO CENTER OF CONCRETE DEPTH) REQUIRED IN COMMERCIAL AND INDUSTRIAL DRIVEWAYS AND IN PUBLIC ALLEYS. 6"x6" 10 GAUGE MINIMUM WELDED WIRE MAY BE USED IN LIEU OF REBAR.
  - CONCRETE DRIVEWAY APRON REQUIRED WHERE SIDEWALK AND/OR CURB IS EXISTING/PROPOSED, OTHERWISE AN ASPHALT APPROACH CAN BE INSTALLED TO EDGE OF PAVEMENT TO SIMILAR WIDTHS OF THE DRIVEWAY APRON AS APPROVED BY THE CITY ENGINEER.
  - REFER TO THE BEND DEVELOPMENT CODE 3.1.400 AND 3.6.200 FOR NUMBER AND WIDTH OF APRONS PERMITTED PER LOT.
  - THIS SAME STANDARD APPLIES TO ALLEYS
  - NO PAVERS OR HEATED SIDEWALKS/APRONS ARE PERMITTED WITHIN THE RIGHT OF WAY.

DRAWN AJD		<p align="center"><b>CITY OF BEND</b> STANDARD DRAWING 710 NW WALL ST., BEND, OREGON 97701</p>	SCALE NTS
DIV ROADWAY			DATE 11/01/2024
REV DATE			APPR
<p align="center"><b>CITY OF BEND</b></p>		<p align="center"><b>DRIVEWAY APPROACH, SETBACK (STANDARD)</b></p>	<p>STD DWG R-5A</p>

ACCESSIBLE PATH TO BE CONSTRUCTED ON BACK SIDE OF APRONS REGARDLESS OF PRESENCE OF SIDEWALK



TYPE	WIDTH
RESIDENTIAL	10' - 24'
SINGLE FAMILY	10'-24'
TOWNHOME	32' MAX SHARED; 16' MAX LOCAL STREETS
DUPLEX	SUM 32' MAX (2 APRON MAX)
TRIPLEX	SUM 32' MAX (3 APRON MAX)
QUADPLEX	SUM 32' MAX (4 APRON MAX)
MULTIFAMILY	20' - 30'
COMMERCIAL	10' - 35'

**TYPICAL PLAN AND PROFILE VIEW  
DRIVEWAY APPROACH, SETBACK, PARTIALLY LOWERED  
(ALTERNATE B)**

TYPE OF STREET	SIDEWALK WIDTH	LIP EXPOSURE	APRON GRADE, POSITIVE GRADE TO ROW	FLARE WIDTH
LOCAL	PER R-1D THRU R-1F	3/4"	12.0% MAX	3'
COLLECTOR	PER R-1B & R1-C	1"	12.0% MAX	6'
ARTERIAL	PER R-1A	1"	12.0% MAX	6'

**GENERAL NOTES:**

- SIDEWALKS SHALL MEET ALL STANDARDS OF CURRENT PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG).
- CURB AND SIDEWALK TYPES VARY, SEE PLANS. SEE STD. DRG. R-3 FOR CURB DETAILS. SEE STD. DRGS. R-4A & R-4B FOR SIDEWALK DETAILS.
- TOOLED JOINTS ARE REQUIRED AT ALL DRIVEWAY SLOPE BREAK LINES.
- THE LANDING SHALL BE PAVED WITH CONCRETE OR ASPHALT SURFACE FOR A MINIMUM OF 20 FEET **ON ALLEYS. LANDINGS ON PRIVATE LOTS SHALL BE IN ACCORDANCE WITH THE BEND DEVELOPMENT CODE. CONSTRUCT AS DIRECTED OR AS SHOWN ON PLANS. DO NOT ENTER PRIVATE PROPERTY WITHOUT APPROPRIATE PERMIT OR EASEMENT. MATERIAL WITHIN THE ROW SHALL BE CONCRETE.**
- CHECK THE GUTTER FLOW DEPTH AT DRIVEWAY LOCATIONS TO ASSURE THAT THE DESIGN FLOOD DOES NOT OVERTOP THE BACK OF SIDEWALK AT DRIVEWAY. IF OVERTOPPING OCCURS PLACE AN INLET AT UPSTREAM SIDE OF DRIVEWAY OR PERFORM OTHER APPROVED DESIGN MITIGATION.
- #4 REBAR (2'0" ON CENTER, TO BE SUSPENDED TO CENTER OF CONCRETE DEPTH) REQUIRED IN COMMERCIAL AND INDUSTRIAL DRIVEWAYS AND IN PUBLIC ALLEYS. 6"x6" 10 GAUGE MINIMUM WELDED WIRE MAY BE USED IN LIEU OF REBAR.
- CONCRETE DRIVEWAY APRON REQUIRED WHERE SIDEWALK AND/OR CURB IS EXISTING/PROPOSED, OTHERWISE AN ASPHALT APPROACH CAN BE INSTALLED TO EDGE OF PAVEMENT TO SIMILAR WIDTHS OF THE DRIVEWAY APRON AS APPROVED BY THE CITY ENGINEER.
- REFER TO THE BEND DEVELOPMENT CODE 3.1.400 AND 3.6.200 FOR NUMBER AND WIDTH OF APRONS PERMITTED PER LOT.
- THIS SAME STANDARD APPLIES TO ALLEYS
- WHERE ON A LOW STRESS ROUTE, THE SIDEWALK/PATH WIDTH THROUGH THE DRIVEWAY APRON MUST BE MAINTAINED.
- NO PAVERS OR HEATED SIDEWALKS/APRONS ARE PERMITTED WITHIN THE RIGHT OF WAY.**

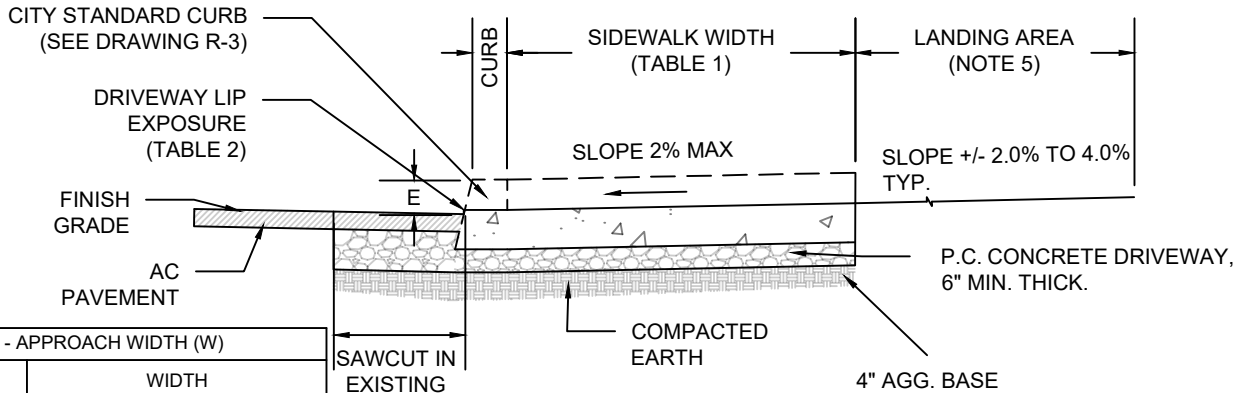
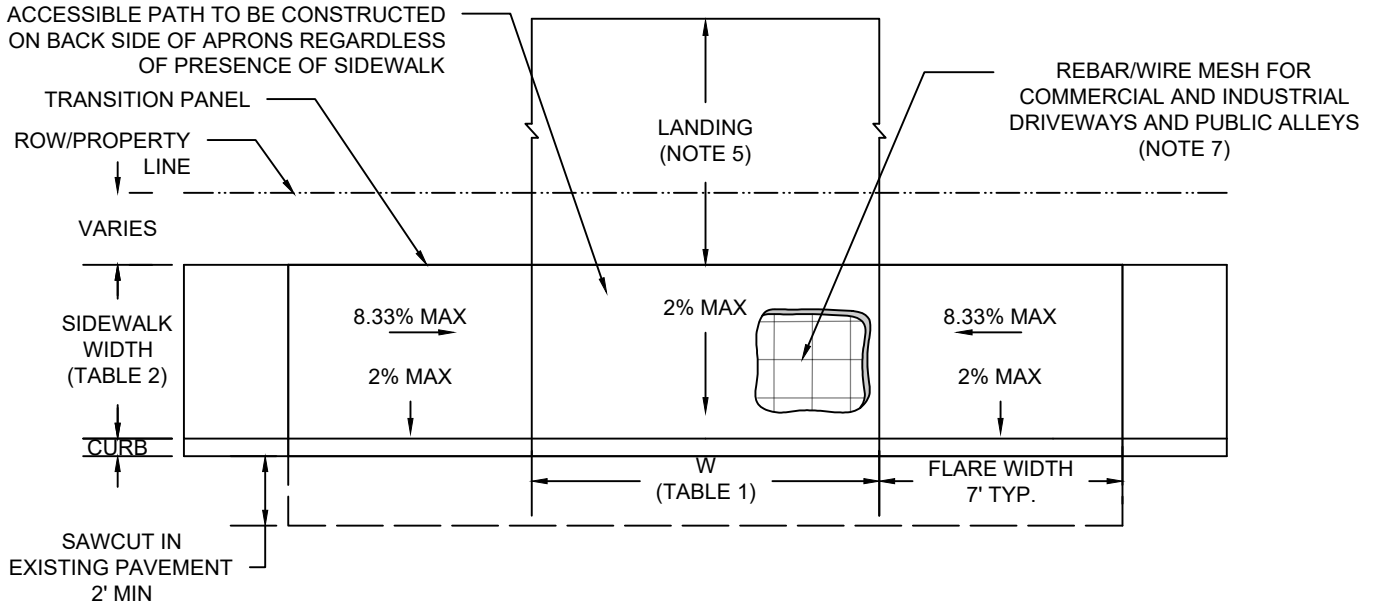
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DIV	ROADWAY
REV	DATE



**CITY OF BEND**  
STANDARD DRAWING  
710 NW WALL ST., BEND, OREGON 97701

DRIVEWAY APPROACH, SETBACK, PARTIALLY LOWERED (ALTERNATE B)

SCALE	NTS
DATE	11/01/2024
APPR	
STD DWG	R-5B



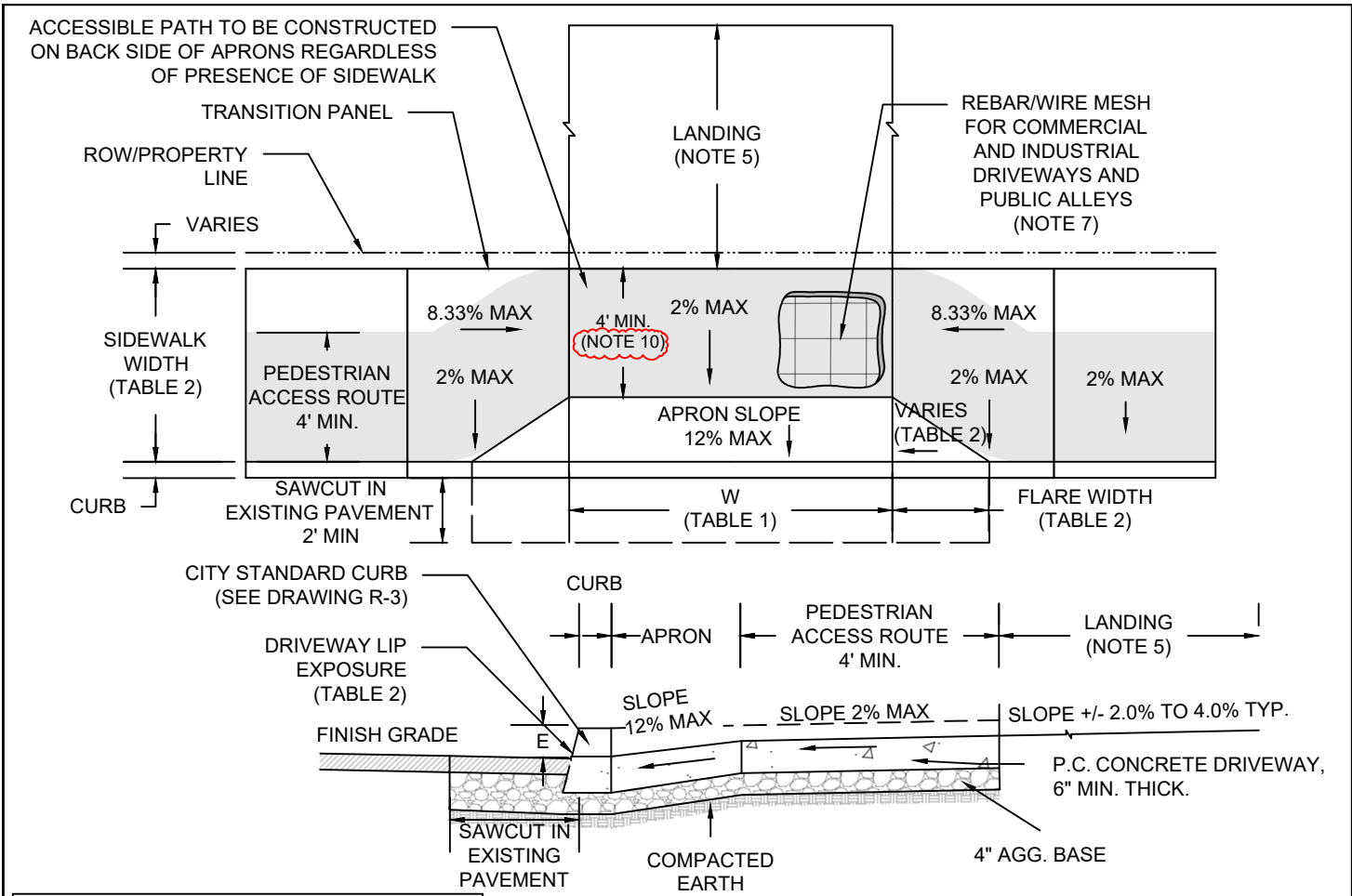
**TYPICAL PLAN AND PROFILE VIEW DRIVEWAY APPROACH, CURB-TIGHT, FULLY LOWERED (ALTERNATE C)**

TYPE	WIDTH
RESIDENTIAL	10' - 24'
SINGLE FAMILY	10'-24'
TOWNHOME	32' MAX SHARED; 16' MAX LOCAL STREETS
DUPLEX	SUM 32' MAX (2 APRON MAX)
TRIPLEX	SUM 32' MAX (3 APRON MAX)
QUADPLEX	SUM 32' MAX (4 APRON MAX)
MULTIFAMILY	20' - 30'
COMMERCIAL	10' - 35'

TYPE OF STREET	SIDEWALK WIDTH	LIP EXPOSURE	APRON GRADE, POSITIVE GRADE TO ROW	FLARE WIDTH
LOCAL	PER R-1D THRU R-1F	3/4"	12.0% MAX	3'
COLLECTOR	PER R-1B & R1-C	1"	12.0% MAX	6'
ARTERIAL	PER R-1A	1"	12.0% MAX	6'

- GENERAL NOTES:**
- SIDEWALKS SHALL MEET ALL STANDARDS OF CURRENT PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG).
  - CURB AND SIDEWALK TYPES VARY, SEE PLANS. SEE STD. DRG. R-3 FOR CURB DETAILS. SEE STD. DRGS. R-4A & R-4B FOR SIDEWALK DETAILS.
  - TOOLED JOINTS ARE REQUIRED AT ALL DRIVEWAY SLOPE BREAK LINES.
  - THE LANDING SHALL BE PAVED WITH CONCRETE OR ASPHALT SURFACE FOR A MINIMUM OF 20 FEET ON ALLEYS. LANDINGS ON PRIVATE LOTS SHALL BE IN ACCORDANCE WITH THE BEND DEVELOPMENT CODE. CONSTRUCT AS DIRECTED OR AS SHOWN ON PLANS. DO NOT ENTER PRIVATE PROPERTY WITHOUT APPROPRIATE PERMIT OR EASEMENT. MATERIAL WITHIN THE ROW SHALL BE CONCRETE.
  - CHECK THE GUTTER FLOW DEPTH AT DRIVEWAY LOCATIONS TO ASSURE THAT THE DESIGN FLOOD DOES NOT OVERTOP THE BACK OF SIDEWALK AT DRIVEWAY. IF OVERTOPPING OCCURS PLACE AN INLET AT UPSTREAM SIDE OF DRIVEWAY OR PERFORM OTHER APPROVED DESIGN MITIGATION.
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  - REFER TO THE BEND DEVELOPMENT CODE 3.1.400 AND 3.6.200 FOR NUMBER AND WIDTH OF APRONS PERMITTED PER LOT.
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  - NO PAVERS OR HEATED SIDEWALKS/APRONS ARE PERMITTED WITHIN THE RIGHT OF WAY.

DRAWN <b>AJD</b> DIV <b>ROADWAY</b> REV DATE		<b>CITY OF BEND</b> STANDARD DRAWING 710 NW WALL ST., BEND, OREGON 97701	SCALE NTS
<b>CITY OF BEND</b>			DATE 11/01/2024
<b>DRIVEWAY APPROACH, CURB-TIGHT, FULLY LOWERED (ALTERNATE C)</b>		APPR STD DWG R-5C	



TYPE	WIDTH
RESIDENTIAL	10' - 24'
SINGLE FAMILY	10'-24'
TOWNHOME	32' MAX SHARED; 16' MAX LOCAL STREETS
DUPLEX	SUM 32' MAX (2 APRON MAX)
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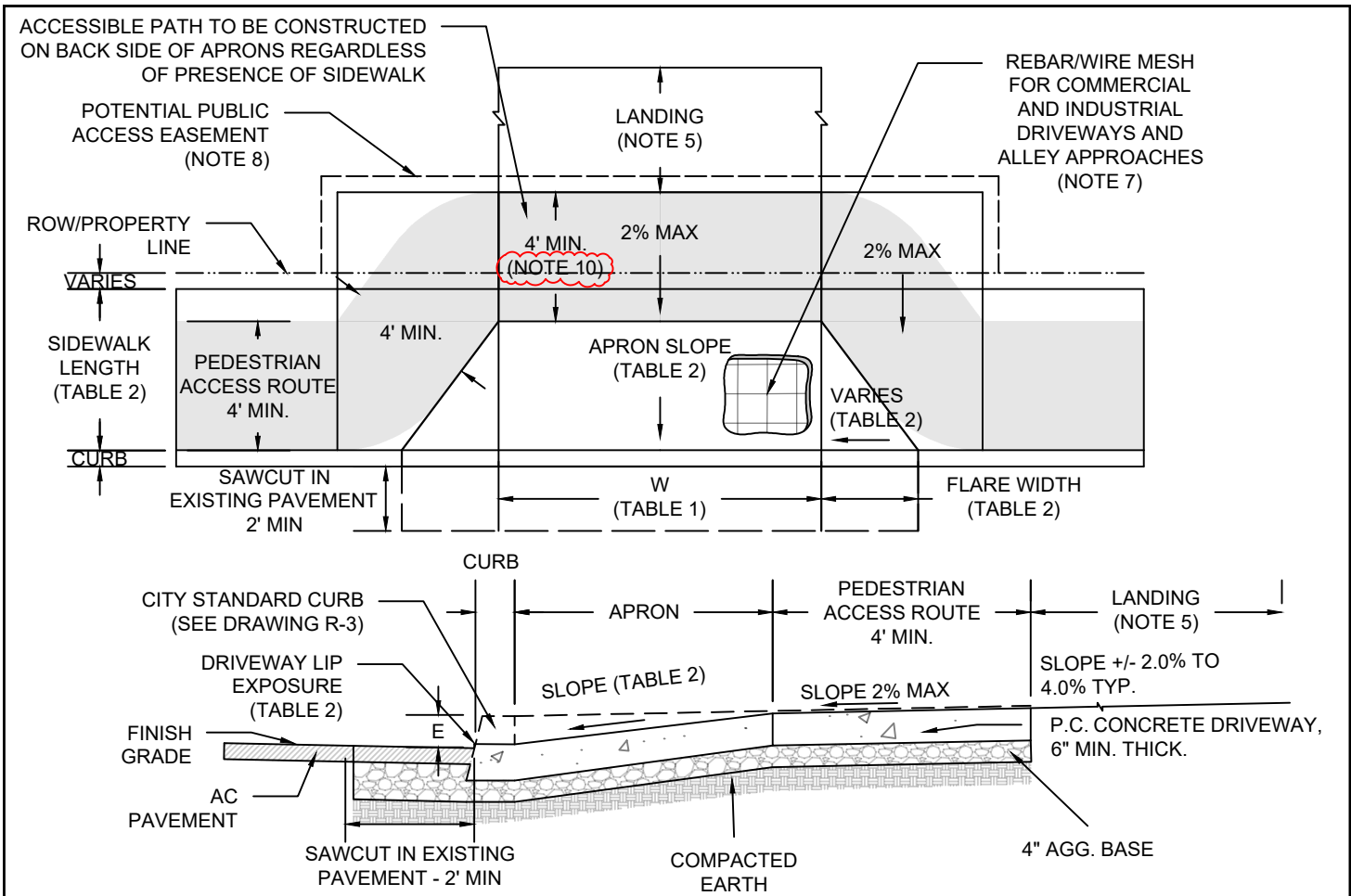
**TYPICAL PLAN AND PROFILE VIEW  
DRIVEWAY APPROACH, CURB-TIGHT, PARTIALLY LOWERED (ALTERNATE D)**

TYPE OF STREET	SIDEWALK WIDTH	LIP EXPOSURE	APRON GRADE, POSITIVE GRADE TO ROW	FLARE WIDTH
LOCAL	PER R-1D THRU R-1F	3/4"	12.0% MAX	3'
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ARTERIAL	PER R-1A	1"	12.0% MAX	6'

- GENERAL NOTES:**
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DIV ROADWAY			DATE 11/01/2024
REV DATE			APPR
<p align="center"><b>CITY OF BEND</b></p>		DRIVEWAY APPROACH, CURB-TIGHT, PARTIALLY LOWERED (ALTERNATE D)	STD DWG R-5D





TYPE	WIDTH
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TRIPLEX	SUM 32' MAX (3 APRON MAX)
QUADPLEX	SUM 32' MAX (4 APRON MAX)
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**TYPICAL PLAN VIEW  
DRIVEWAY APPROACH, CURB-TIGHT, WRAPPING SIDEWALK  
(ALTERNATE E)**

TYPE OF STREET	MINIMUM SIDEWALK WIDTH	LIP EXPOSURE	APRON GRADE, POSITIVE GRADE TO ROW	FLARE WIDTH
LOCAL	PER R-1D THRU R-1F	3/4"	12.0% MAX	3'
COLLECTOR	PER R-1B & R1-C	1"	12.5% MAX	6'
ARTERIAL	PER R-1A	1"	12.5% MAX	6'

- GENERAL NOTES:**
- SIDEWALKS SHALL MEET ALL STANDARDS OF CURRENT PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG).
  - CURB AND SIDEWALK TYPES VARY, SEE PLANS. SEE STD. DRG. R-3 FOR CURB DETAILS. SEE STD. DRGS. R-4A & R-4B FOR SIDEWALK DETAILS.
  - TOOLED JOINTS ARE REQUIRED AT ALL DRIVEWAY SLOPE BREAK LINES.
  - THE LANDING SHALL BE PAVED WITH CONCRETE OR ASPHALT SURFACE FOR A MINIMUM OF 20 FEET ON ALLEYS. LANDINGS ON PRIVATE LOTS SHALL BE IN ACCORDANCE WITH THE BEND DEVELOPMENT CODE. CONSTRUCT AS DIRECTED OR AS SHOWN ON PLANS. DO NOT ENTER PRIVATE PROPERTY WITHOUT APPROPRIATE PERMIT OR EASEMENT. MATERIAL WITHIN THE ROW SHALL BE CONCRETE.
  - CHECK THE GUTTER FLOW DEPTH AT DRIVEWAY LOCATIONS TO ASSURE THAT THE DESIGN FLOOD DOES NOT OVERTOP THE BACK OF SIDEWALK AT DRIVEWAY. IF OVERTOPPING OCCURS PLACE AN INLET AT UPSTREAM SIDE OF DRIVEWAY OR PERFORM OTHER APPROVED DESIGN MITIGATION.
  - #4 REBAR (2'0" ON CENTER, TO BE SUSPENDED TO CENTER OF CONCRETE DEPTH) REQUIRED IN COMMERCIAL AND INDUSTRIAL DRIVEWAYS AND IN PUBLIC ALLEYS. 6"x6" 10 GAUGE MINIMUM WELDED WIRE MAY BE USED IN LIEU OF REBAR.
  - CONCRETE DRIVEWAY APRON REQUIRED WHERE SIDEWALK AND/OR CURB IS EXISTING/PROPOSED, OTHERWISE AN ASPHALT APPROACH CAN BE INSTALLED TO EDGE OF PAVEMENT TO SIMILAR WIDTHS OF THE DRIVEWAY APRON AS APPROVED BY THE CITY ENGINEER.
  - REFER TO THE BEND DEVELOPMENT CODE 3.1.400 AND 3.6.200 FOR NUMBER AND WIDTH OF APRONS PERMITTED PER LOT.
  - THIS SAME STANDARD APPLIES TO ALLEYS
  - WHERE ON A LOW STRESS ROUTE, THE SIDEWALK/PATH WIDTH THROUGH THE DRIVEWAY APRON MUST BE MAINTAINED.
  - NO PAVERS OR HEATED SIDEWALKS/APRONS ARE PERMITTED WITHIN THE RIGHT OF WAY.


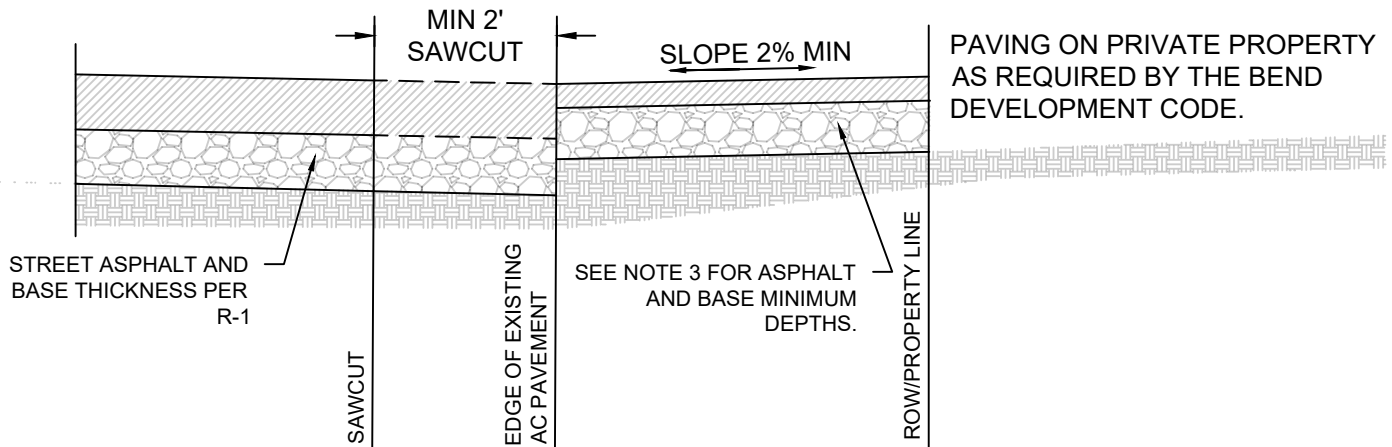
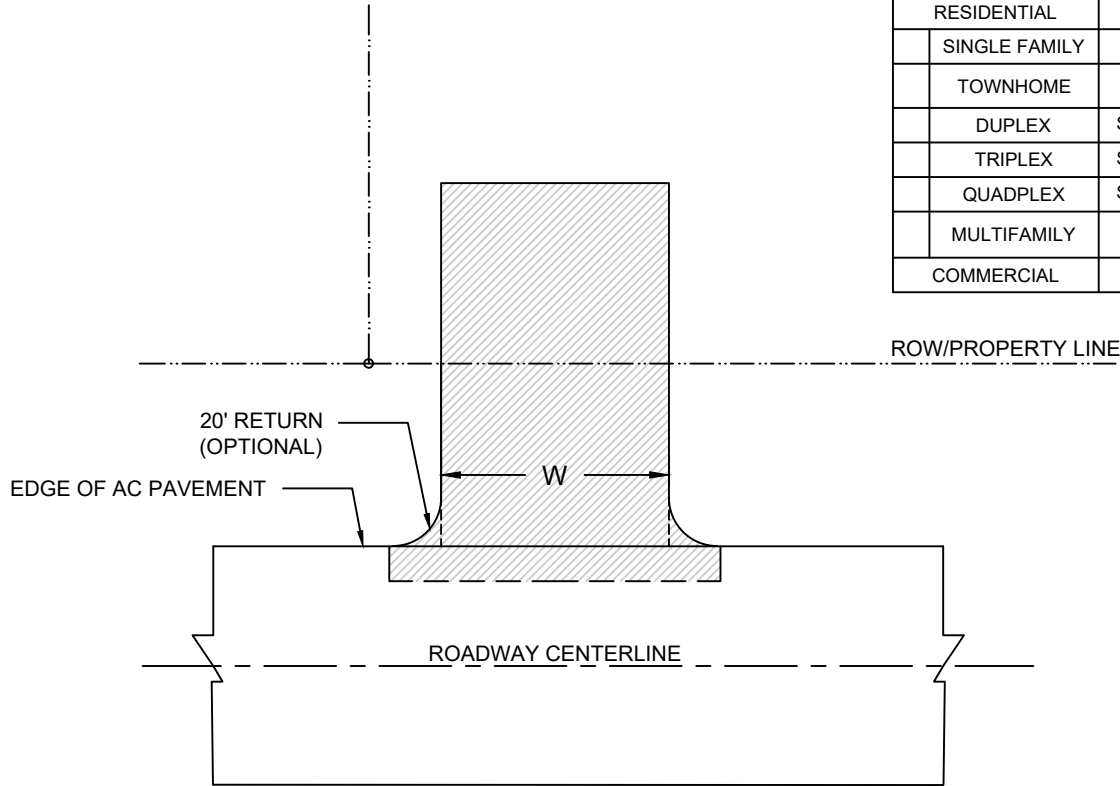
DRAWN CJH	 <b>CITY OF BEND</b>	<b>CITY OF BEND</b> STANDARD DRAWING 710 NW WALL ST., BEND, OREGON 97701	SCALE NTS
DIV ROADWAY		DRIVEWAY APPROACH, CURB-TIGHT, WRAPPING SIDEWALK (ALTERNATE E)	DATE 11/01/2024
REV DATE			APPR
			STD DWG R-5E

TABLE 1 - APPROACH WIDTH (W) - REFER TO NOTE 4	
TYPE	WIDTH
RESIDENTIAL	10' - 24'
SINGLE FAMILY	10'-24'
TOWNHOME	32' MAX SHARED; 16' MAX LOCAL STREETS
DUPLEX	SUM 32' MAX (2 APRON MAX)
TRIPLEX	SUM 32' MAX (3 APRON MAX)
QUADPLEX	SUM 32' MAX (4 APRON MAX)
MULTIFAMILY	20' - 30'
COMMERCIAL	10' - 35'



**GENERAL NOTES:**

1. CONCRETE DRIVEWAY APRONS REQUIRED WHERE SIDEWALK AND/OR CURB IS EXISTING OR PROPOSED, OTHERWISE AN ASPHALT APPROACH CAN BE INSTALLED TO EDGE OF PAVEMENT TO SIMILAR WIDTHS OF THE DRIVEWAY APRON AS APPROVED BY THE CITY ENGINEER.
2. NO PAVERS OR HEATED APRONS ARE PERMITTED WITHIN THE RIGHT OF WAY.
3. COMMERCIAL OR INDUSTRIAL USE MUST BE CONSTRUCTED TO A MINIMUM 4" ASPHALT OVER MINIMUM 6" BASE ROCK. RESIDENTIAL LOTS MUST HAVE NOT LESS THAN 2" ASPHALT OVER 4" BASED ROCK.
4. REFER TO THE BEND DEVELOPMENT CODE 3.1.400 AND 3.6.200 FOR NUMBER AND WIDTH OF APRONS PERMITTED PER LOT.

DRAWN CJH	
DIV ROADWAY	
REV	DATE



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710 NW WALL ST., BEND, OREGON 97701

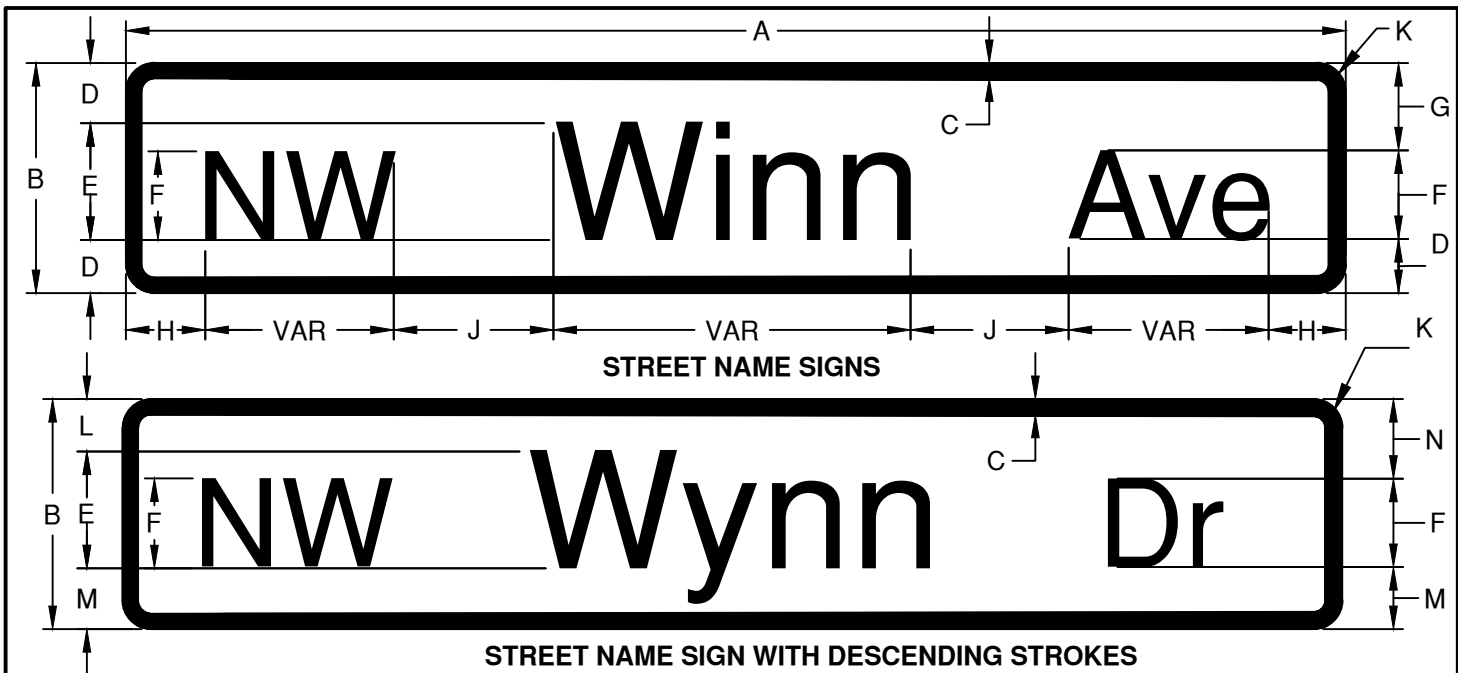
DRIVEWAY APPROACH, ASPHALT

SCALE NTS

DATE 11/01/2024

APPR

STD DWG R-5F




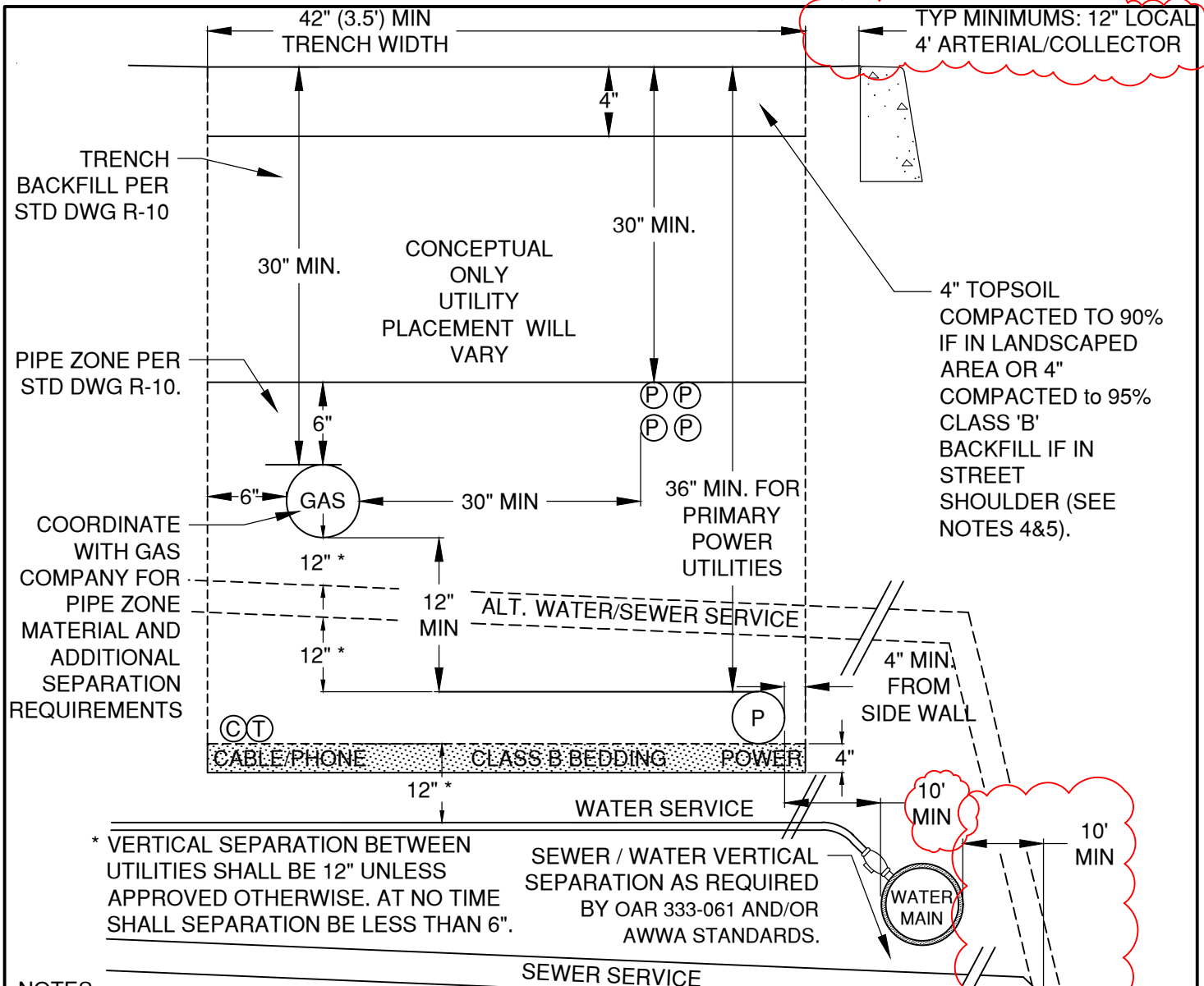
**STREET NAME SIGN WITH DESCENDING STROKES**

SIGN LOCATION	DIMENSIONS												
	A	B	C	D	E	F	G	H	J	K	L	M	N
LOCAL	VAR	8	0.375	2	4C	3C	3	3 MIN	3	1	1.75	2.25	2.75
COLLECTOR/ ARTERIAL ≤ 40MPH	VAR	12	0.5	3	6C	4.5C	5	4.5 MIN	4.5	1.5	2.75	3.25	4.75
COLLECTOR/ ARTERIAL > 40 MPH	VAR	18	0.75	5	8C	6C	7.67	5.33 MIN	6	1.875	5	5	7.67
OVERHEAD	VAR	24	1	6	12C	9C	10	9 MIN	9	2.25	5	6	9.50

**NOTES:**

- SIGNS INSTALLED ALONG PUBLIC STREETS SHALL BE FABRICATED AND INSTALLED TO CONFORM TO THE MUTCD AND CITY OF BEND SPECIFICATIONS.
- UNLESS OTHERWISE SPECIFIED, STREET NAME SIGNS SHALL BE FABRICATED AS FOLLOWS:
  - SIGN SUBSTRATE: SHEET ALUMINUM (GAUGE 0.80 FOR GROUND-MOUNT) WITH ROUNDED CORNERS
  - RETRO-REFLECTIVE SHEETING: GREEN BACKGROUND WITH WHITE LEGEND, USING HIP/TYPAGE G FOR GROUND-MOUNTED SIGNS, AND DIAMOND GRADE/TYPAGE G2 FOR SIGNS MOUNTED OVERHEAD;
  - LETTERING SHALL BE LOWER-CASE WITH INITIAL UPPER-CASE LETTERS;
  - SERIES C2000 FONT, WITH LETTERING AND LETTER SPACING PER THE FEDERAL HIGHWAY ADMINISTRATION'S STANDARD ALPHABETS AS SHOWN IN THE CURRENT EDITION OF THE STANDARD HIGHWAY SIGNS AND PAVEMENT MARKINGS MANUAL. (\* EXCEPT FOR OVERHEAD SIGNS, WHERE SIGNS EXCEED 36" LONG, SERIES B2000 FONT SHALL BE USED);
  - BOTTOM STREET SIGNS (CLOSEST TO THE REGULATORY/STOP SIGN) SHALL BE TWO SINGLE-SIDED WITH PREDRILLED HOLES. SIGNS SHALL BE RIVETED BACK TO BACK ON THE SQUARE TUBE POST, CENTERED ON THE POST.
  - BOTTOM STREET SIGNS SHALL BE USED FOR SIDE STREET (STOP CONTROLLED STREET).
  - TOP STREET SIGN SHALL BE DOUBLE SIDED, TOP SIGN USED FOR MAINLINE STREET (NON-STOP CONTROLLED STREET).
- ALL SIGNS SHALL BE REVIEWED AND APPROVED BY THE CITY OF BEND ENGINEERING DEPARTMENT PRIOR TO FABRICATIONS AND INSTALLATION.
- TYPICAL INSTALLATION INCLUDES 2-INCH SQUARE TUBE CAPS WITH 90-DEGREE ANGLE BRACKETS ON 2-INCH PERFORATED SQUARE TUBE STEEL POSTS. USE 5- OR 6-INCH BLADE MOUNTS FOR SIGNS LESS THAN 36" WIDE; 12-INCH MOUNTS FOR SIGNS 36-INCHES OR WIDER OR OVER 6-INCHES HIGH. SEE STANDARD DRAWINGS R-7 AND R-9.
- SIGN WIDTHS VARY WITH LEGEND. WHERE SITE CONSTRAINTS LIMIT AVAILABLE SPACE, REDUCED LETTER HEIGHT, FONT STYLE, LINE SPACING, OR EDGE SPACING WILL BE CONSIDERED. REDUCTIONS IN SPACING BETWEEN LETTERS OR WORDS IS NOT PERMITTED.
- WHERE PRIVATE STREETS INTERSECT WITH PUBLIC STREETS, INSTALL A BLACK ON YELLOW PRIVATE DR SIGN WITH 4-INCH CAPITAL LETTERS (ODOT SIGN POLICY SIGN #OW14-3) DIRECTLY BELOW THE PRIVATE STREET NAME SIGN (OR ON A SEPARATE POST, IF NOT AT AN INTERSECTION).
- FOR ADDITIONAL INFORMATION, REFER TO MUTCD SECTION 2A AND 2D, AND CITY OF BEND TECHNICAL SPECIFICATION SECTION 00940.
- CONFIRM SIGN SIZE WITH CITY ENGINEER FOR SIGNS ON EXISTING TRAFFIC SIGNAL POLES OR MAST ARMS.
- LARGER SIGNS ARE PLACE PLACED FACING THE FASTER STREET (COLLECTORS AND ARTERIALS).

DRAWN AJD			 <b>CITY OF BEND</b>	<b>CITY OF BEND</b> STANDARD DRAWING 710 NW WALL ST., BEND, OREGON 97701						SCALE NTS	
DIV ROADWAY										DATE 11/01/2024	
REV	DATE	APPR								APPR	
				<b>STANDARD STREET NAME SIGNS</b>						STD DWG R-8	



\* VERTICAL SEPARATION BETWEEN UTILITIES SHALL BE 12" UNLESS APPROVED OTHERWISE. AT NO TIME SHALL SEPARATION BE LESS THAN 6".

NOTES:

1. ALL COMPACTION TO COMPLY WITH CITY OF BEND STANDARDS.
2. ALL FRANCHISE UTILITIES SHALL BE INSTALLED IN CONDUIT AS APPROVED BY THE FRANCHISE UTILITY HAVING OWNERSHIP.
3. UTILITY SIZES AND LOCATION SHALL BE DETERMINED BY THE UTILITY COMPANY. LOCATION TO BE SHOWN AND APPROVED BY CITY WITH A RIGHT OF WAY (ROW) PERMIT.
4. WHERE STORM SWALES ARE PROPOSED WITHIN THE LANDSCAPE STRIP, FRANCHISE UTILITIES SHALL BE INSTALLED OUTSIDE OF THE SWALE AREA.
5. TOP SOIL LAYER TO BE COMPACTIONED TO 90% MAX DENSITY. WHERE SIDEWALK IS PLACED OVER FRANCHISE UTILITY TRENCH, NO TOP SOIL SHALL BE PLACED AND SIDEWALK TO BE CONSTRUCTED TO COMPLY WITH CITY STANDARDS R-4A AND R-4B
6. STANDARD SHOWN FOR NEW CONSTRUCTION. MODIFICATIONS SHALL BE MADE WHEN WITHIN EXISTING DEVELOPMENTS WHERE APPROVED BY THE CITY ENGINEER.
7. UTILITIES OUTSIDE THE RIGHT OF WAY SHALL BE WITHIN A PUBLIC UTILITIES EASEMENT (PUE). BACKFILL AND INSTALLATION REQUIREMENTS STILL COMPLY WITH THE PUE.
8. NO SWALES OR SURFACE STORMWATER DRAINAGE FACILITIES ARE PERMITTED OVER FRANCHISE UTILITIES.

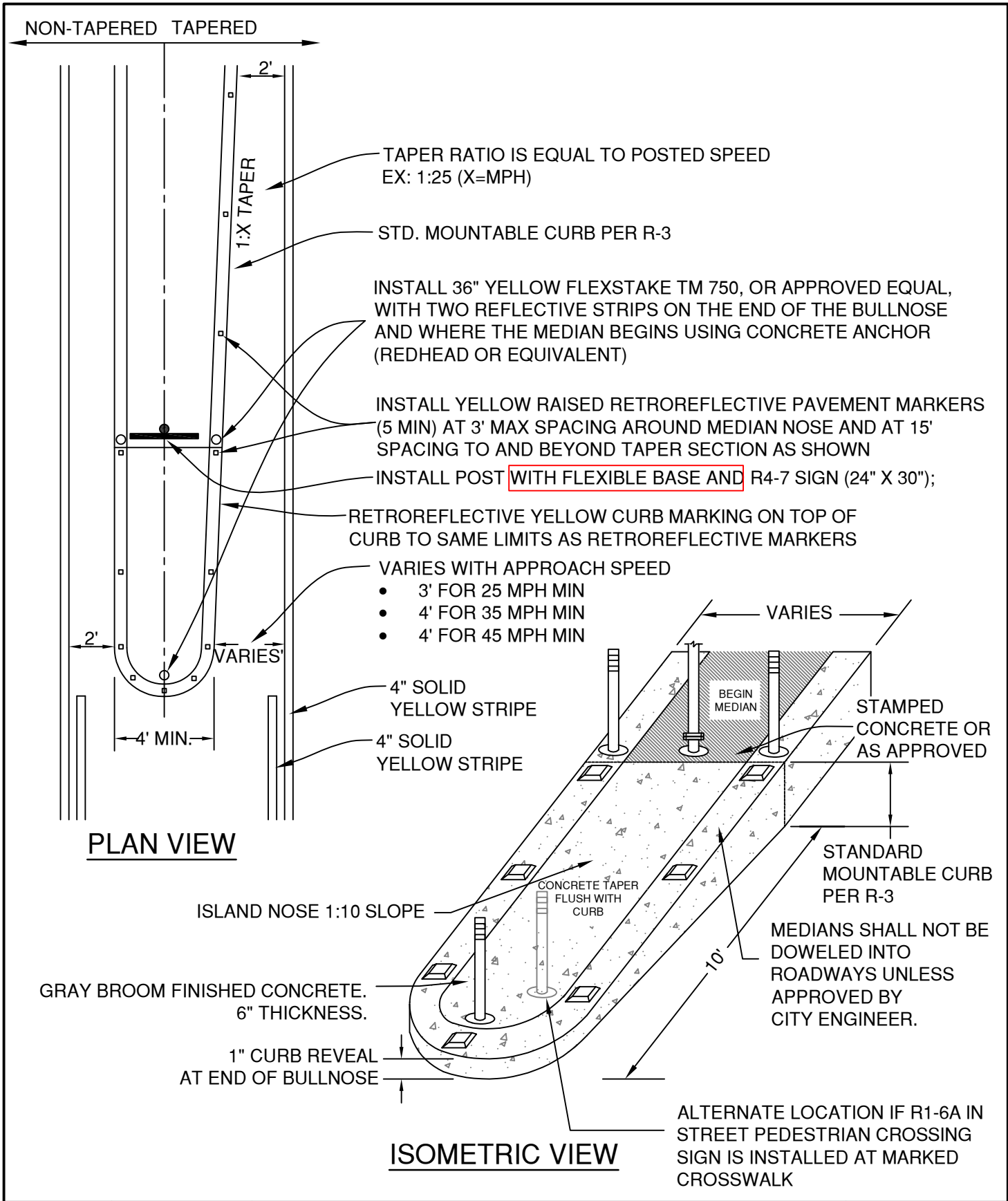
DRAWN CJH	
DIV ROADWAY	
REV	DATE



**CITY OF BEND**  
STANDARD DRAWING  
710 NW WALL ST., BEND, OREGON 97701

**FRANCHISE UTILITY JOINT TRENCH**

SCALE NTS
DATE 11/01/2024
APPR
STD DWG R-10A



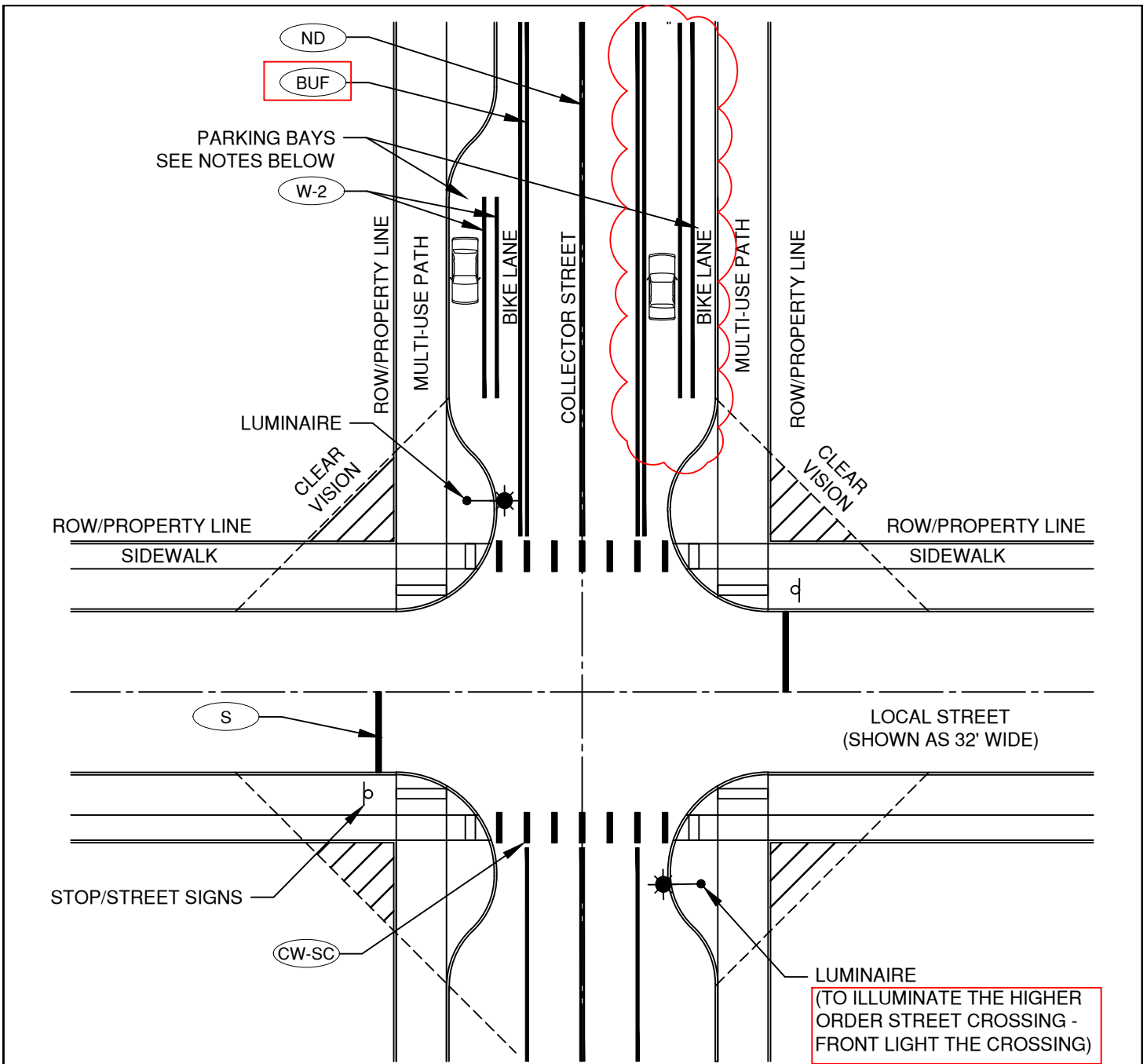
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DIV ROADWAY	
REV	DATE



**CITY OF BEND**  
STANDARD DRAWING  
710 NW WALL ST., BEND, OREGON 97701


**MEDIAN / ISLAND END DETAIL**

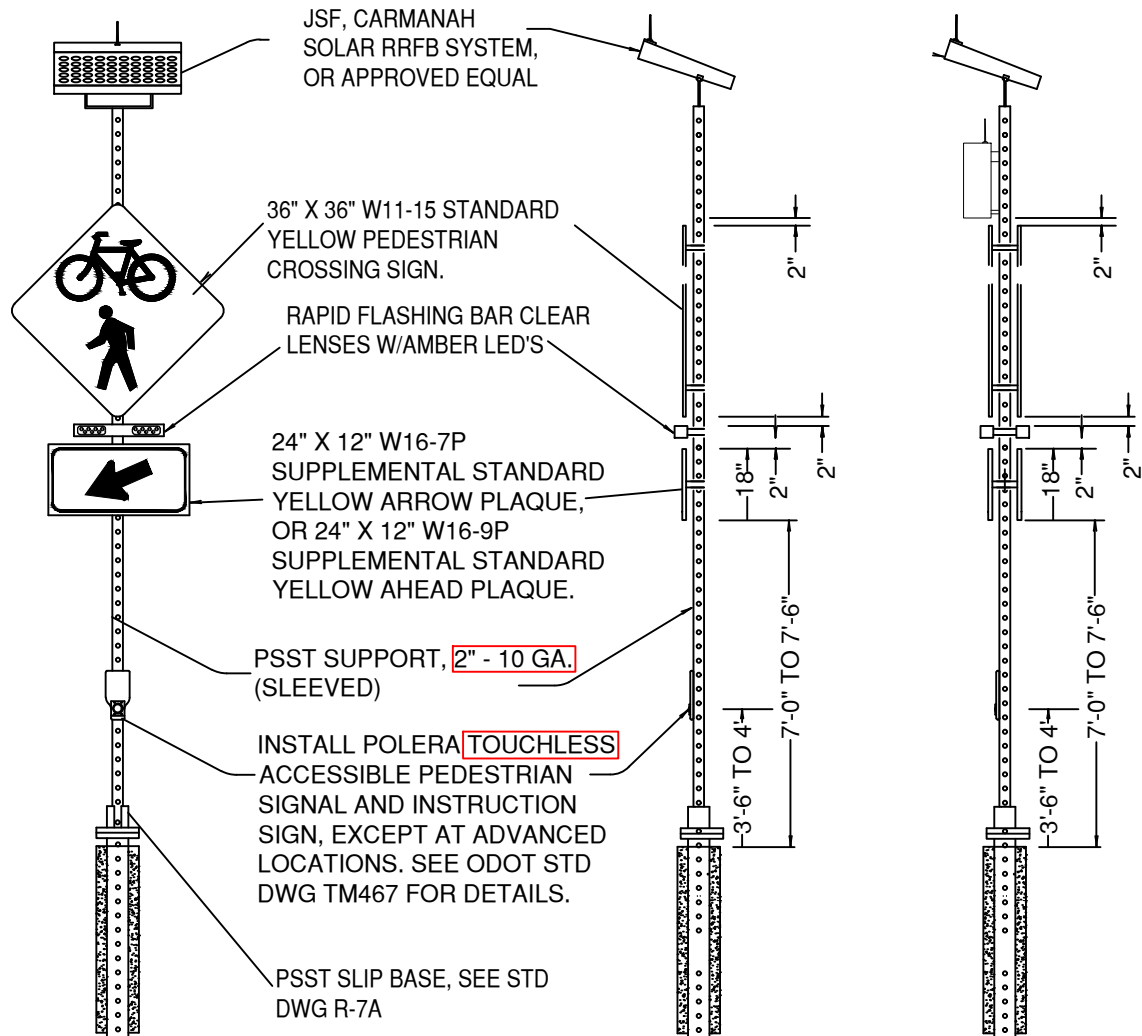
SCALE NTS
DATE 11/01/2024
APPR
STD DWG R-25



**NOTES:**

1. PARKING BAYS SHALL BE DESIGNED OUTSIDE THE CLEAR VISION OF THE INTERSECTION. PARKING WILL BE PERMITTED IF CLEAR VISION AND SIGHT DISTANCE AS ANALYZED AS SAFE BY A PROFESSIONAL ENGINEER.
2. PARKING BAYS ON COLLECTORS ARE PERMITTED AS DIRECTED BY THE DEVELOPMENT CODE.
3. NO MORE THAN 10 PARKING BAYS WILL BE PERMITTED TOGETHER. TERMINATION OF BAYS WILL BE FOR VEGETATION PLANTING, UTILITY INSTALLATION (FRANCHISE UTILITY VAULTS, STORM FACILITIES, ETC).
4. PARKING IS NOT PERMITTED WITHIN THE INTERSECTION'S CLEAR VISION AND SIGHT DISTANCE AS DETERMINED BY AASHTO REQUIREMENTS AND ENGINEER REVIEW.
5. USE PARKING SEPARATED BIKE LANE WHERE PARKING BAYS ARE LONG/BLOCK LENGTH AND ADEQUATE EXIT/ENTER SIGHT DISTANCE CAN BE PROVIDED.

DRAWN AJD DIV ROADWAY REV DATE	 <b>CITY OF BEND</b>	<b>CITY OF BEND</b> STANDARD DRAWING 710 NW WALL ST., BEND, OREGON 97701	SCALE NTS DATE 11/01/2024 APPR STD DWG R-27
<b>COLLECTOR / LOCAL INTERSECTION</b>			



FRONT VIEW

SINGLE SIDED  
SIDE VIEW

TWO SIDED  
SIDE VIEW

**RECTANGULAR RAPID FLASHING BEACON SYSTEM  
PSST INSTALLATION**

**NOTES:**

1. REMOVE SOLAR EQUIPMENT IF USING COMMERCIAL POWER
2. USE APPLIED INFORMATION AI-500-030 LOW POWER MONITORING DEVICE
3. USE SCHOOL CROSSING (S1-1) FOR DESIGNATED SCHOOL CROSSING
4. USE DOUBLE SIDED SIGNS IN MEDIANS
5. ADD RADIO NETWORK CONTROLLER (APPLIED INFORMATION CONTROLLER GLANCE LOW POWER MONITORING DEVICE) INCLUDING ANY RETROFIT / MOUNTING KIT.

DRAWN	AJD
DIV	ROADWAY
REV	DATE



CITY OF BEND

**CITY OF BEND**

STANDARD DRAWING

710 NW WALL ST., BEND, OREGON 97701

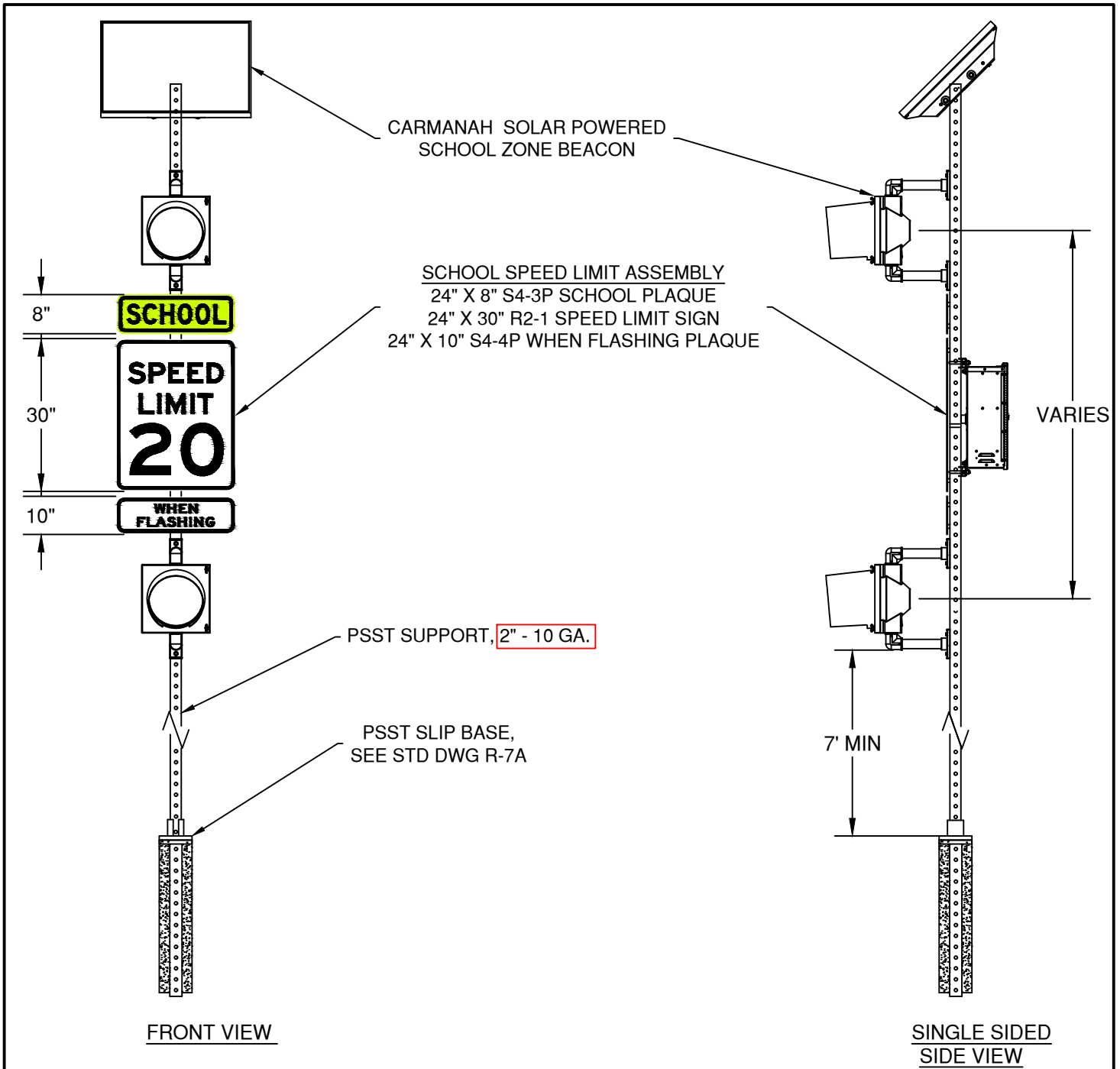
**RECTANGULAR RAPID FLASHING BEACON**

SCALE NTS

DATE 11/01/2024

APPR

STD DWG R-35A



**SOLAR POWER SCHOOL ZONE BEACON  
PSST INSTALLATION**

**NOTES:**

1. REMOVE SOLAR EQUIPMENT IF USING COMMERCIAL POWER
2. ADD RADIO NETWORK CONTROLLER FOR WIRELESS EQUIPMENT IF NEEDED
3. ADD RADIO NETWORK CONTROLLER (APPLIED INFORMATION CONTROLLER GLANCE LOW POWER MONITORING DEVICE) INCLUDING ANY RETROFIT / MOUNTING KIT.

DRAWN AJD	
DIV ROADWAY	
REV	DATE



**CITY OF BEND**

**CITY OF BEND**

STANDARD DRAWING

710 NW WALL ST., BEND, OREGON 97701

**FLASHING SCHOOL ZONE BEACON**

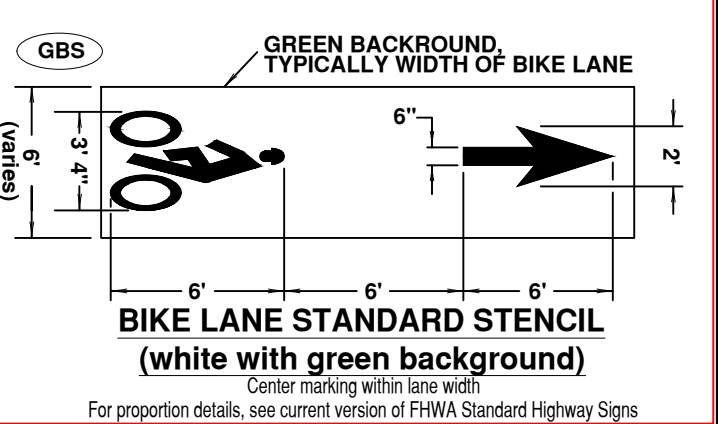
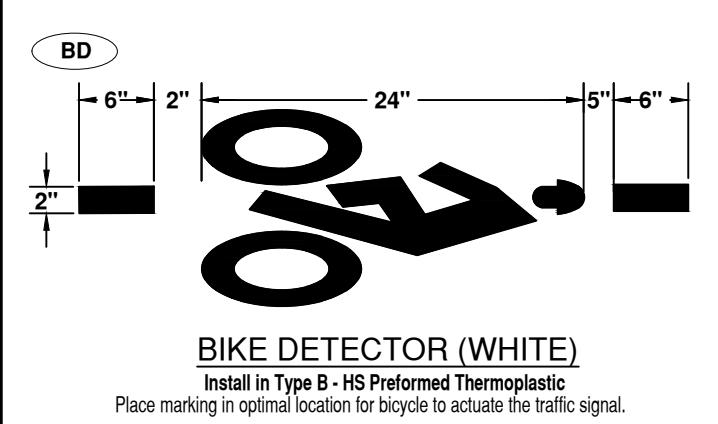
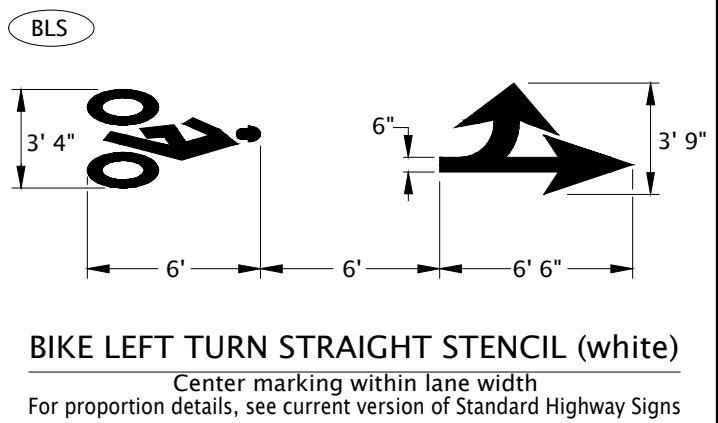
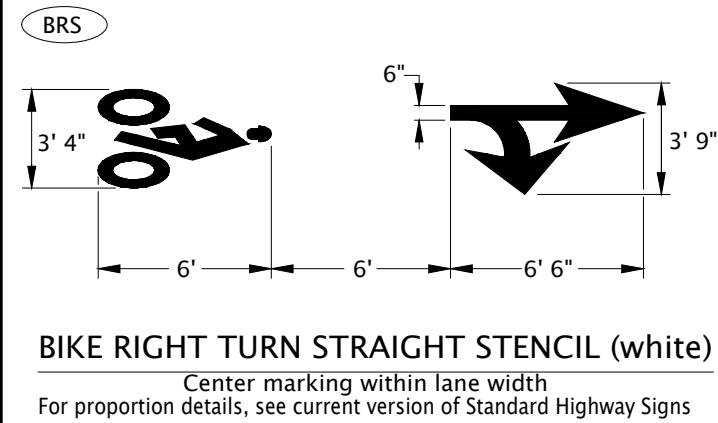
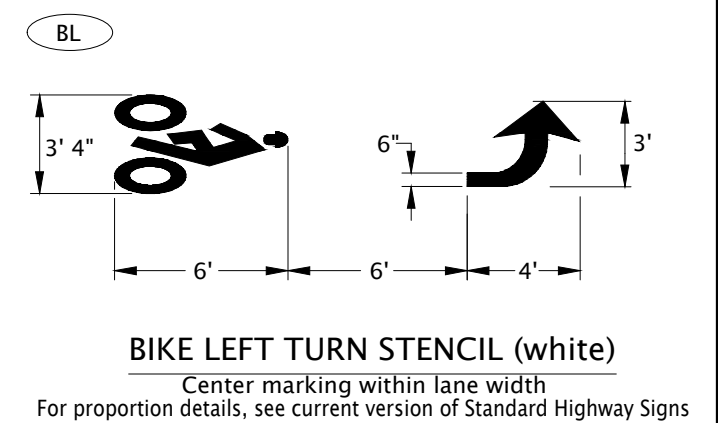
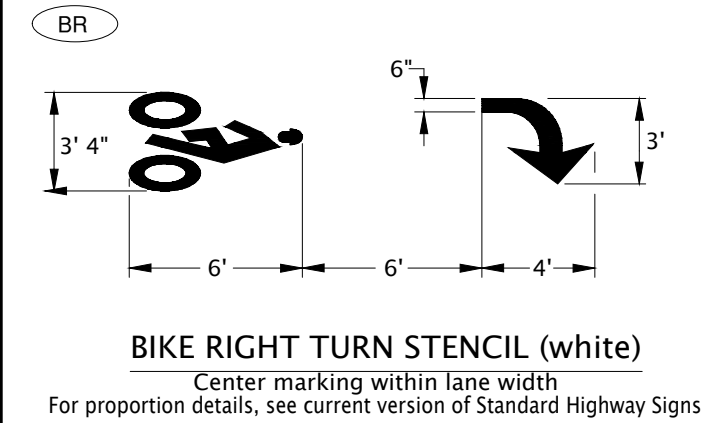
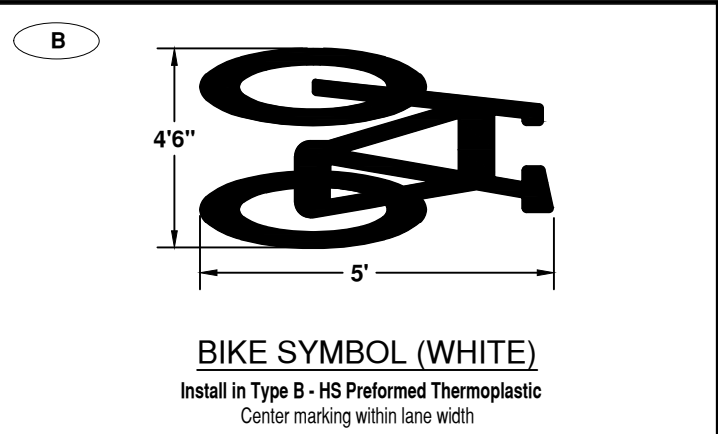
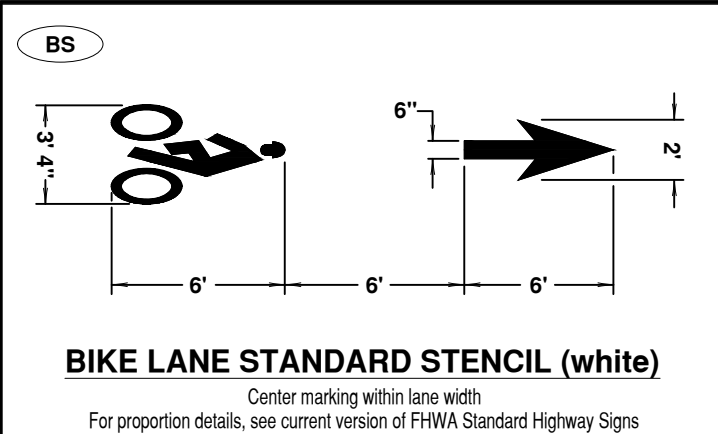
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DATE 11/01/2024

APPR

STD DWG R-35B





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DIV	ROADWAY
REV	DATE

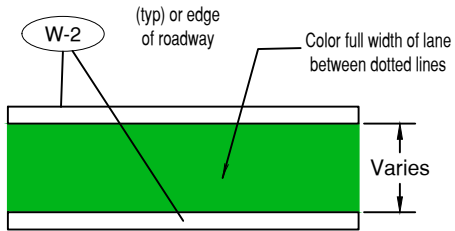


**CITY OF BEND**  
STANDARD DRAWING  
710 NW WALL ST., BEND, OREGON 97701

**PAVEMENT MARKINGS - BIKE**

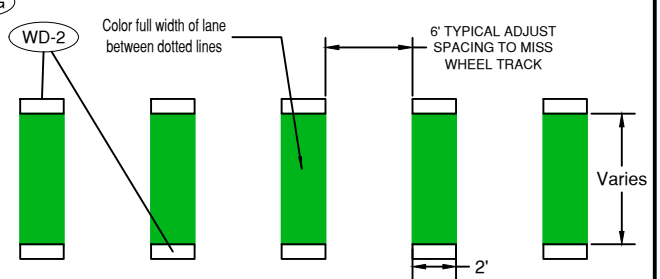
SCALE	NTS
DATE	11/01/2024
APPR	
STD DWG	R-43

GRN

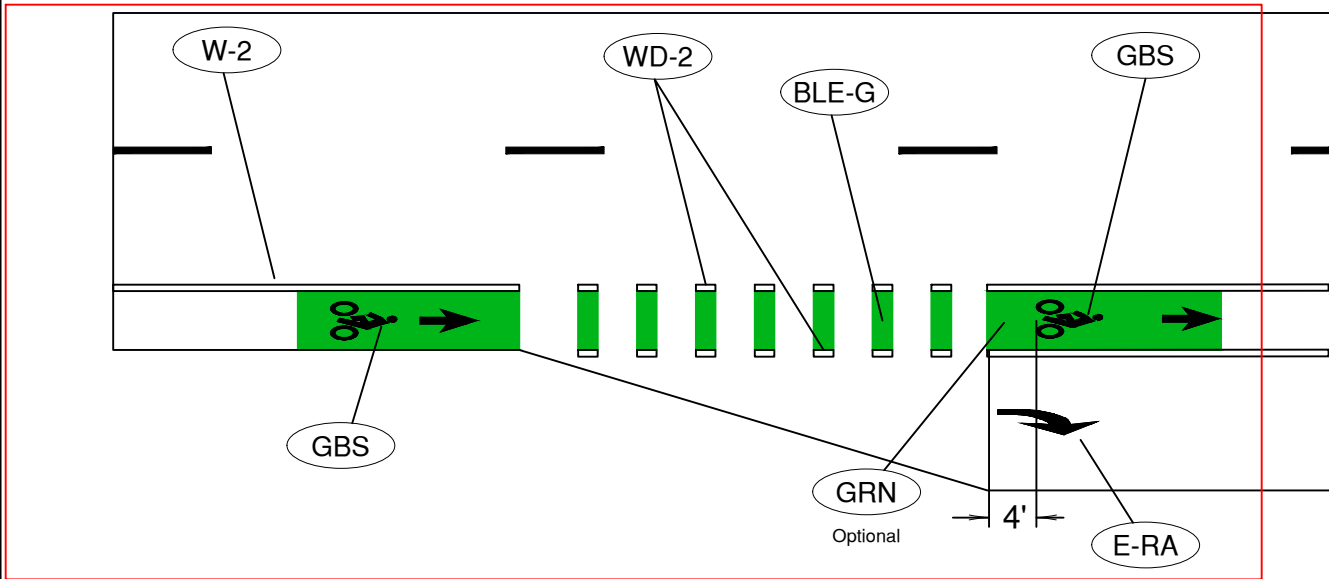


GREEN SUPPLEMENTED BICYCLE LANE  
SOLID LANE

BLE-G



GREEN SUPPLEMENTED BICYCLE LANE  
DOTTED LINE EXTENSION



TYPICAL GREEN SUPPLEMENTED BICYCLE LANE ACROSS AN ADDED RIGHT TURN LANE TAPER

NOTES:

- 1. GREEN PAVEMENT MARKING USE PER DESIGN STANDARDS WITH CITY ENGINEER APPROVAL

DRAWN	AJD
DIV	ROADWAY
REV	DATE



CITY OF BEND

CITY OF BEND

STANDARD DRAWING

710 NW WALL ST., BEND, OREGON 97701

INTERSECTION BIKE SAFETY

SCALE NTS

DATE 11/01/2024

APPR

STD DWG R-44A

Recommended<sup>1</sup>

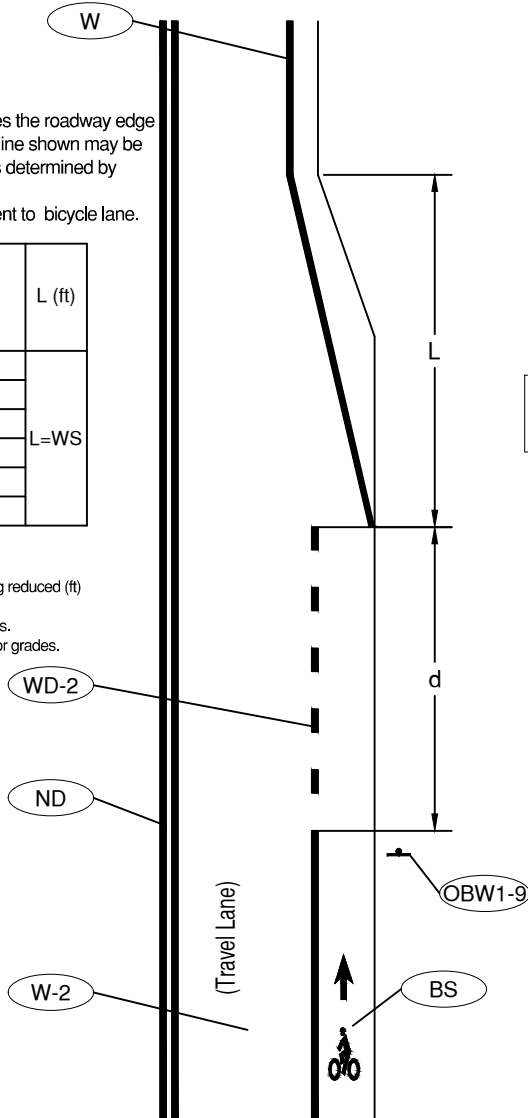
General Notes:

1. Where a curb clearly defines the roadway edge in the taper area, the edge line shown may be omitted in the taper area as determined by engineer judgement.
2. Motor vehicle speed adjacent to bicycle lane.

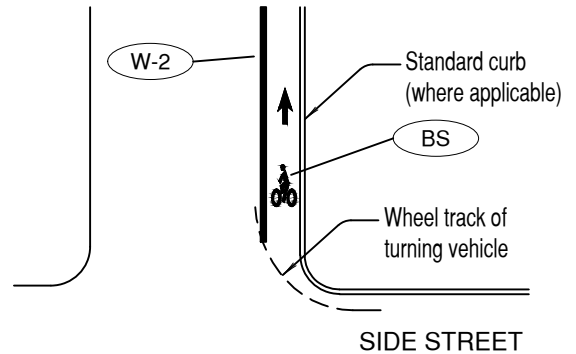
POSTED OR 85TH PERCENTILE SPEED (MPH) <sup>2</sup>	d (FT)	L (ft)
20	128	L=WS
25	152	
30	176	
35	184	
40	192	
45+	200	

WHERE:

L = taper length  
 W = width of bicycle lane being reduced (ft)  
 S = vehicle speed (mph)  
 "d" distances are for level roads.  
 Corrections should be made for grades.



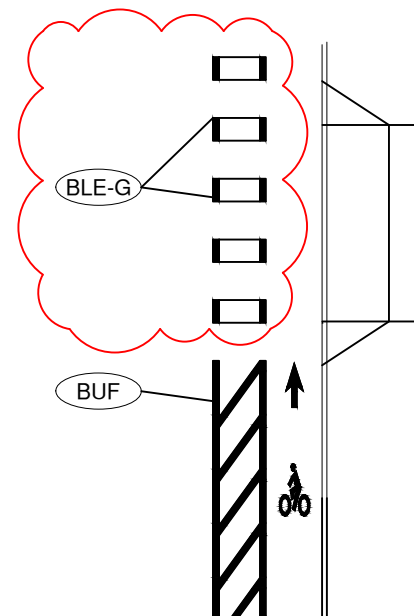
END OF BIKE LANE



General Note:

Install bike lane stencil to avoid right turning vehicle wheel tracks.

INSTALLATION OF BIKE LANE STENCILS FOLLOWING INTERSECTIONS



BUFFER BIKE LANE IN CONFLICT AREA (FOR HIGH VOLUME COMMERCIAL DRIVEWAYS)

To be accompanied by Standard Dwg. Nos. R-40 thru R-43 and R-44A

DRAWN	AJD
DIV	ROADWAY
REV	DATE



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710 NW WALL ST., BEND, OREGON 97701

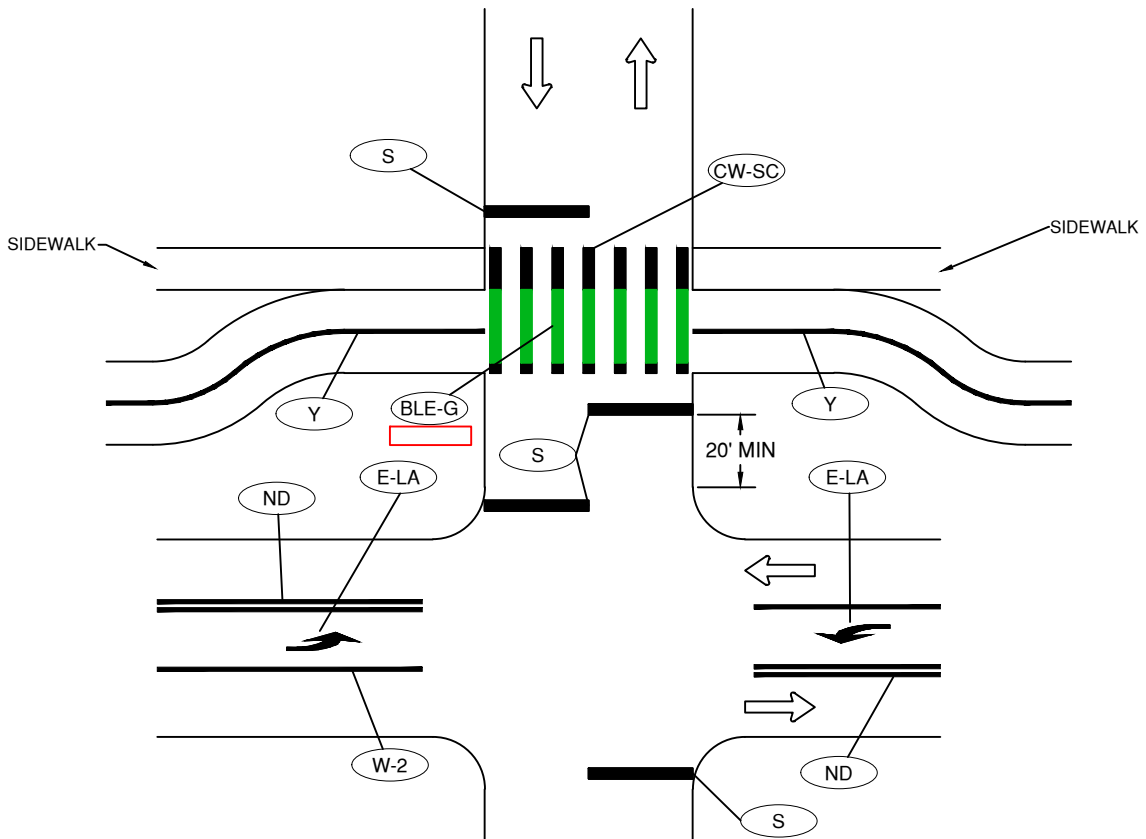
BIKE LANE MARKINGS

SCALE NTS

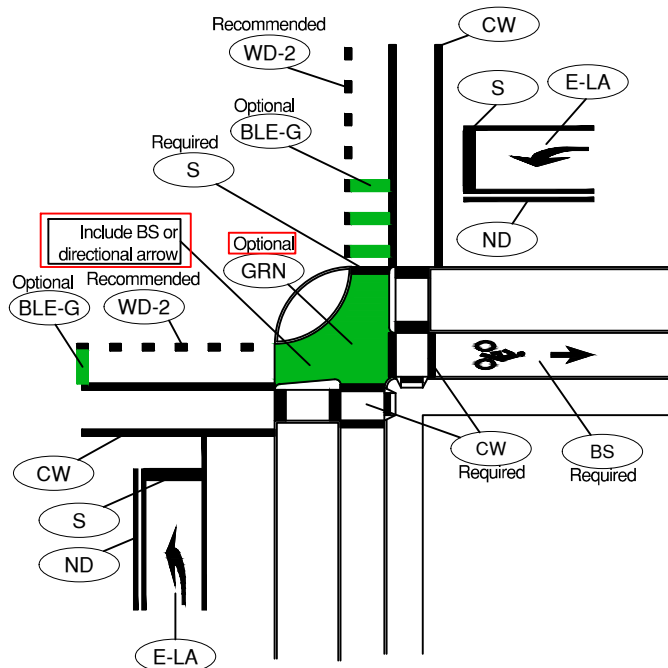
DATE 11/01/2024

APPR

STD DWG R-44B



Typical 2-way separated marked bicycle crossing  
where path offset from street



Example separated bicycle lane markings at a  
signalized intersection

DRAWN AJD  
DIV ROADWAY  
REV DATE



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710 NW WALL ST., BEND, OREGON 97701

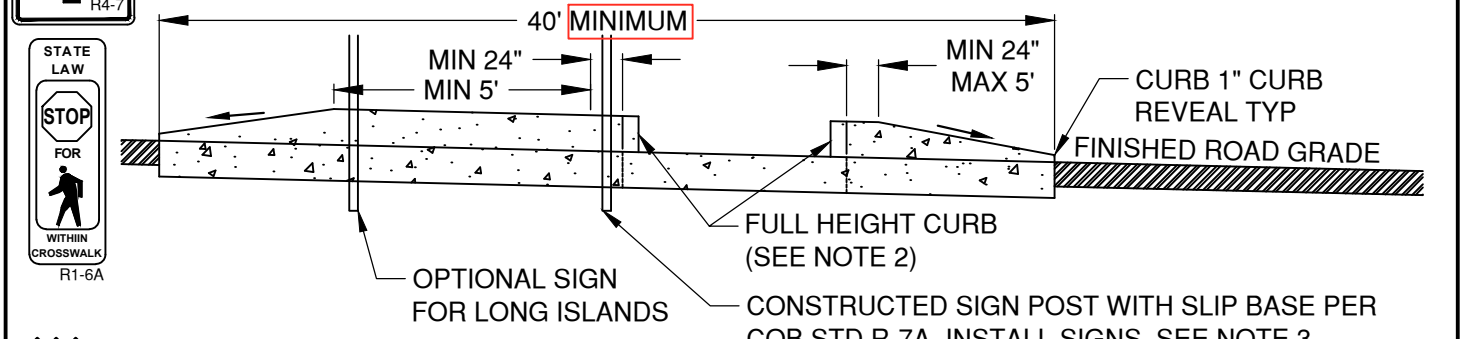
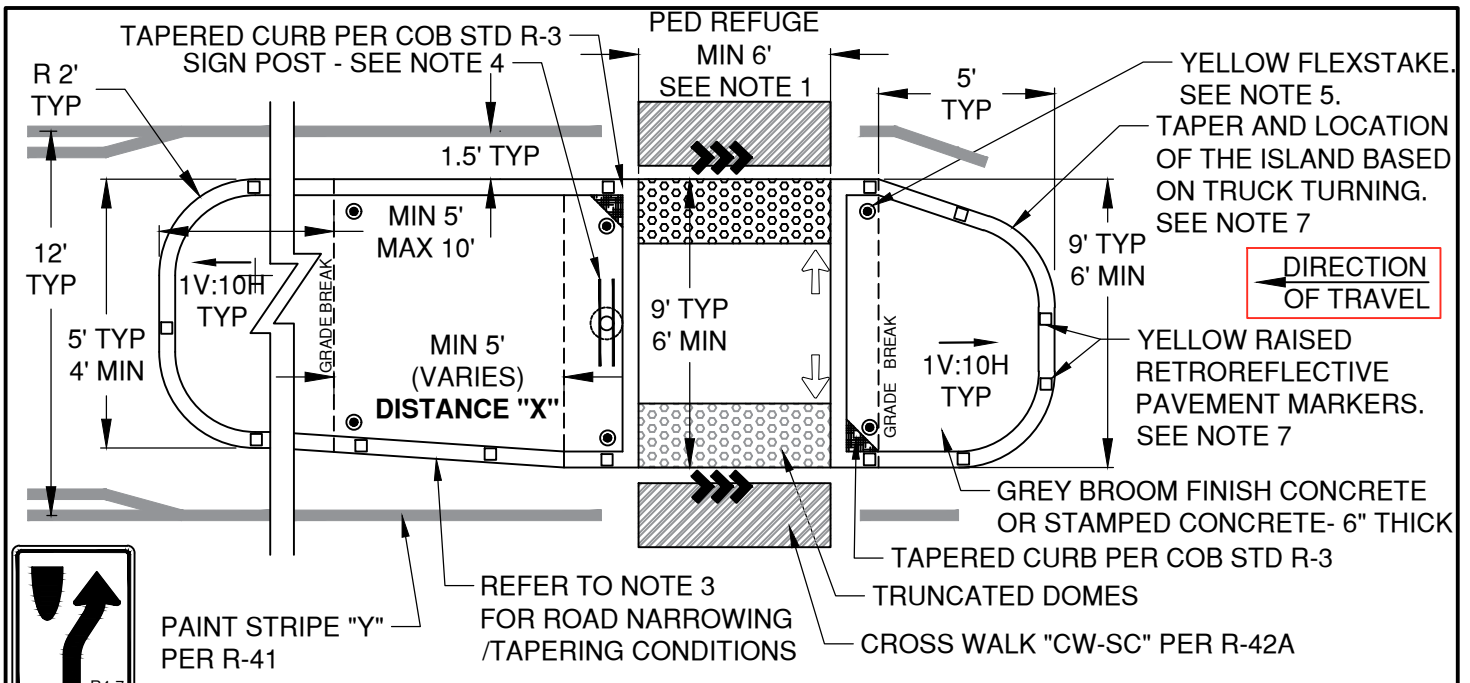
BIKE LANE MARKINGS

SCALE NTS

DATE 11/01/2024

APPR

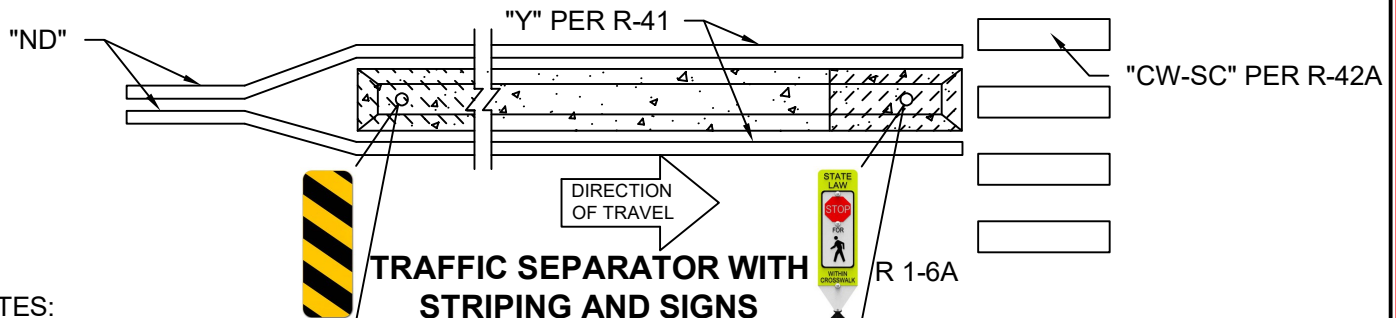
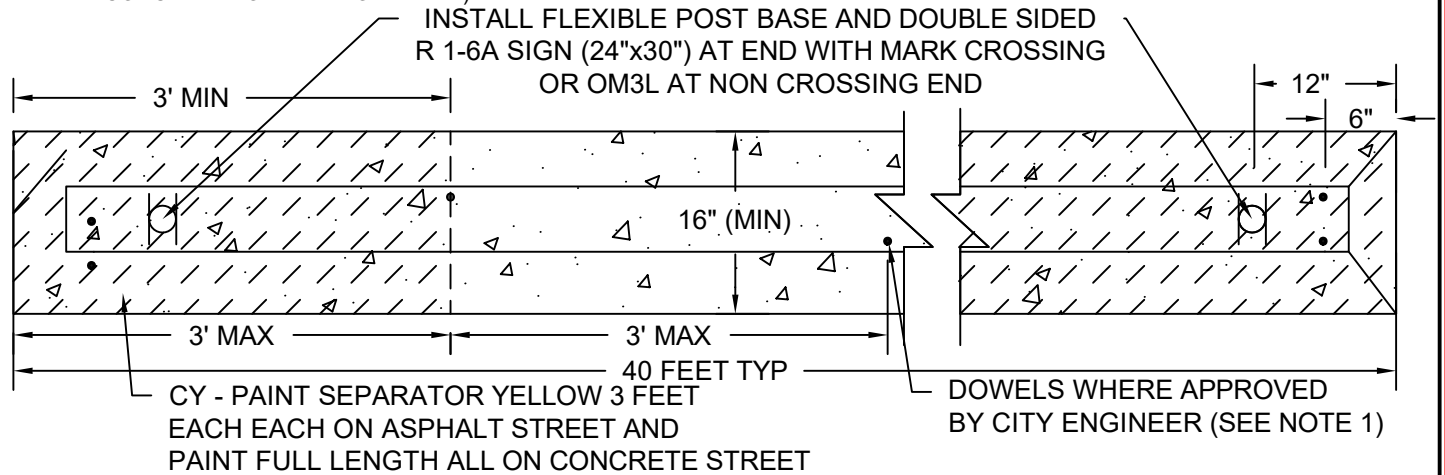
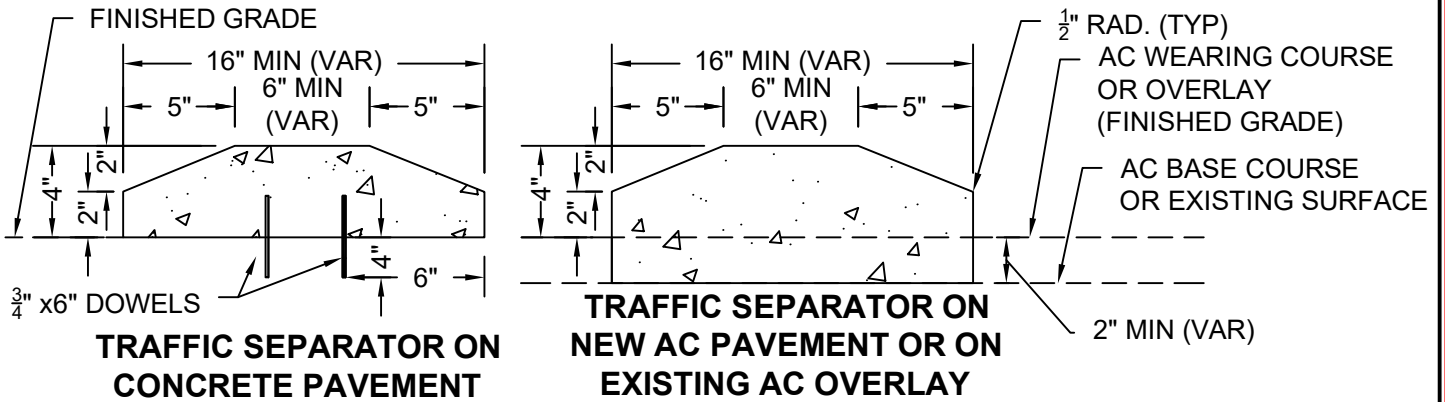
STD DWG R-44C



5.0% MAX (4.5% DESIGN) AT NON-STOP CONTROLLED INTERSECTION - 0.5% MIN  
 2.0% TYPICAL

- NOTES:**
1. PEDESTRIAN REFUGE OPENING TO MATCH THE WIDTH OF THE CURB RAMPS OR SIDEWALKS COMING INTO THE INTERSECTION, BUT NOT LESS THAN 6 FEET WIDE.
  2. INSTALL CURB PER CITY STANDARD R-3. FULL HEIGHT CURB (DEPENDENT ON THE STREET CLASSIFICATION) OUTSIDE THE BULL NOSE / VERTICAL TAPER SECTIONS OF THE ISLAND.
  3. TAPER TO BE  $\frac{1}{2} \times X$  OR AS NEEDED TO TERMINATE THE ISLAND WITH A 4' MIN BULLNOSE.
  4. INSTALL R1-6A AND R4-7 (R1-6A OR R1-6C SIGNS FOR SCHOOL ZONES) SIGNS BACK TO BACK ON SIGN POST ADJACENT TO THE PED REFUGE. INSTALL AN ADDITIONAL POST PER COB STANDARD R-7A FOR INSTALLATION OF A SECOND R4-7 SIGN IF THE ISLAND EXCEEDS 40 FEET IN TOTAL LENGTH, OR AS DIRECTED BY THE CITY ENGINEER.
  5. ISLANDS ARE NOT TO BE DOWELED INTO ROADWAYS UNLESS APPROVED BY THE CITY ENGINEER.
  6. INSTALL 36" YELLOW TUBULAR FLEXSTAKE TM 750, OR APPROVED EQUAL, WITH TWO REFLECTIVE STRIPS DELINEATOR ON THE END OF THE BULLNOSE. OFFSET TO AVOID BLOCKING THE R1-6A SIGN. USE CONCRETE ANCHORS (REDHEAD OR EQUIVALENT).
  7. INSTALL RETROREFLECTIVE YELLOW CURB MARKINGS ON TOP OF CURB AT 3' MAX SPACING AROUND MEDIAN NOSE AND AT 15' SPACING TO AND BEYOND TAPER SECTION AS SHOWN.
  8. PLACEMENT OF ISLAND WILL BE BASED ON A WB-50 TURNING TEMPLATE. LARGER RADII MAY BE REQUIRED BY CITY ENGINEER TO FIT CONTEXT, SUCH AS INDUSTRIAL OR RESIDENTIAL AREA.

DRAWN CJH DIV ROADWAY REV DATE		<b>CITY OF BEND</b> STANDARD DRAWING 710 NW WALL ST., BEND, OREGON 97701	SCALE NTS DATE 11/01/2024 APPR STD DWG R-46
<b>CITY OF BEND</b>		<b>PEDESTRIAN REFUGE ISLAND</b>	



- NOTES:**
1. DOWELS TO BE USED IN LIMITED CASES WERE APPROVED BY THE CITY ENGINEER (TEMPORARY INSTALLATIONS OR WHERE THE SEPARATOR IS SHORT AND MAY NOT HAVE SUBSTANTIAL WEIGHT TO KEEP IN PLACE). WHEN DOWELS ARE APPROVED, THEY MUST BE 3/4" DIAMETER WITH LENGTH EXTENDING A MINIMUM 6" BELOW THE BOTTOM OF THE SEPARATOR. DOWELS TO BE SET BEFORE CONCRETE HARDENS.
  2. TRANSVERSE JOINTS IN CONCRETE TRAFFIC SEPARATORS AND TRANSITIONS TO MATCH JOINTS IN CONCRETE PAVEMENT AND TO BE OF SAME TYPE.
  3. SET JOINT SPACING 200' MAX FOR EXPANSION AND 15' MAX FOR CONTRACTION.
  4. SITE CONDITIONS MAY REQUIRE A PROJECT SPECIFIC DRAIN OPENING WHICH CONSIDERS ROADWAY CONDITIONS (SHEET FLOW LIMITS, CROSS SLOPE, SUPER ELEVATION, ETC). WHERE BREAKS ARE REQUIRED IN THE SEPARATOR, THE OPENS MUST BE 12".
  5. PLACE APPROVED PREFORMED FILLER ALONG ONE SIDE OF THE CONC. TRANSITIONS IN CONCRETE PAVEMENT AND AROUND ALL CURBED POINTS. WHERE SEPARATOR IS INSTALLED WITHIN ASPHALT, APPLY TACK COATS TO CONCRETE PRIOR TO PAVING.

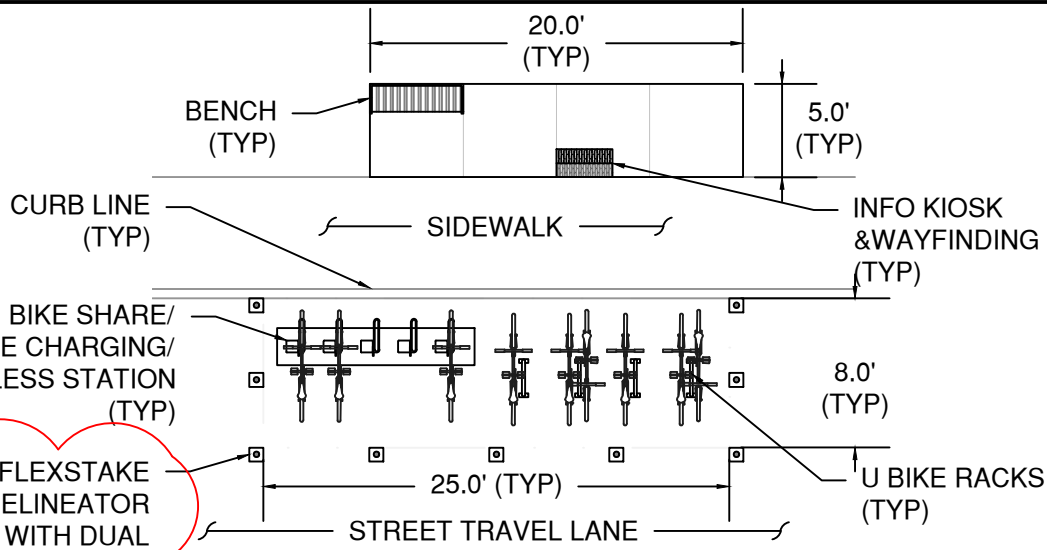
DRAWN AJD	
DIV ROADWAY	
REV	DATE



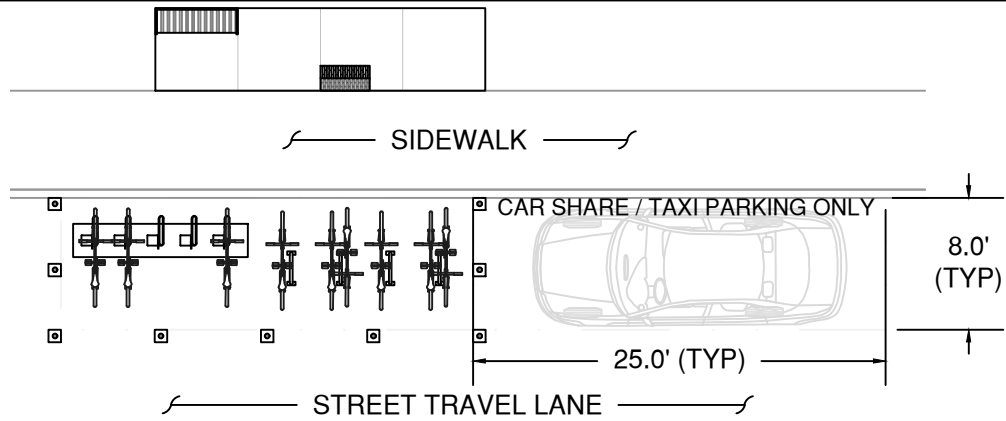
**CITY OF BEND**  
 STANDARD DRAWING  
 710 NW WALL ST., BEND, OREGON 97701

**TRAFFIC SEPERATOR**

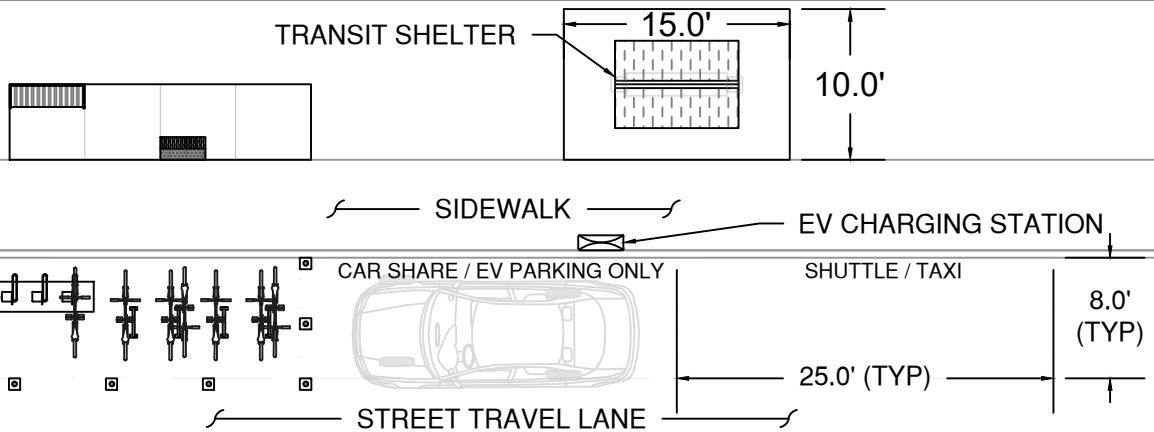
SCALE NTS
DATE 11/01/2024
APPR
STD DWG R-46A



SMALL MOBILITY POINT



MEDIUM MOBILITY POINT



MEDIUM (+) MOBILITY POINT

NOTES:

1. LOCATION & EXISTING CONDITIONS WILL DETERMINE LAYOUT
2. FINAL LAYOUT MUST MEET MINIMUM ADA STANDARDS FOR ACCESSIBLE DESIGN
3. ALL CONCEPTS SHOWN ARE FOR SPATIAL REPRESENTATION ONLY
4. BICYCLE PARKING STATIONS MAY BE PLACED WITH IN ON-STREET PARKING SPACES OR ON PRIVATE PROPERTY

DRAWN	AJD
DIV	ROADWAY
REV	DATE



CITY OF BEND

CITY OF BEND

STANDARD DRAWING

710 NW WALL ST., BEND, OREGON 97701

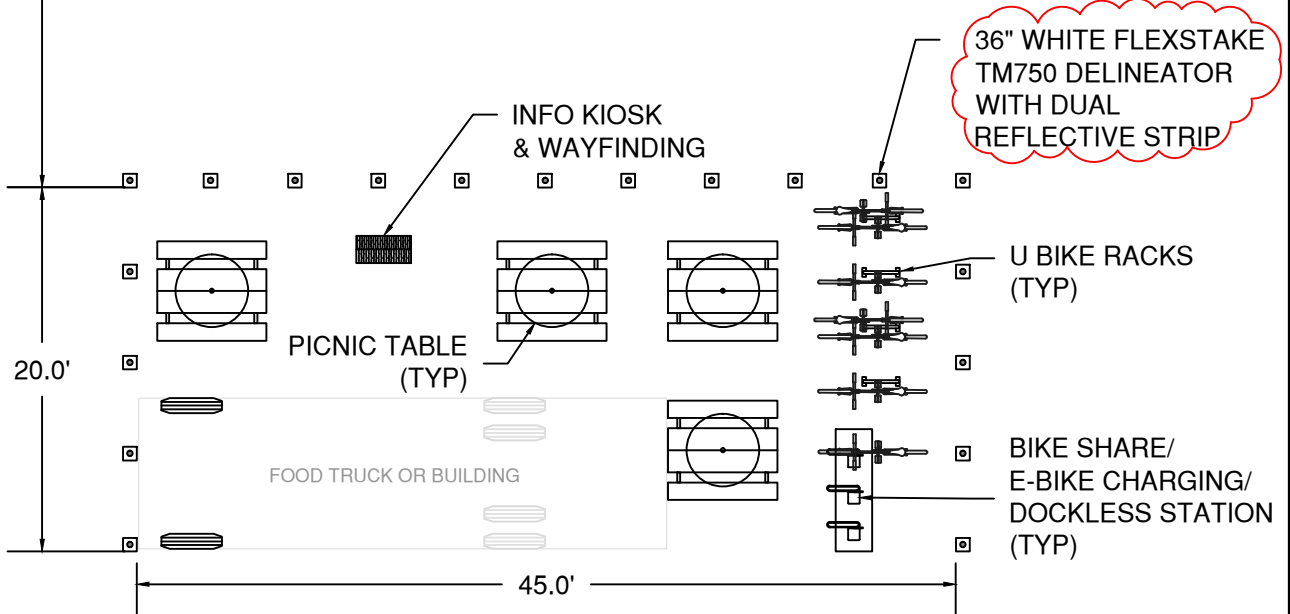
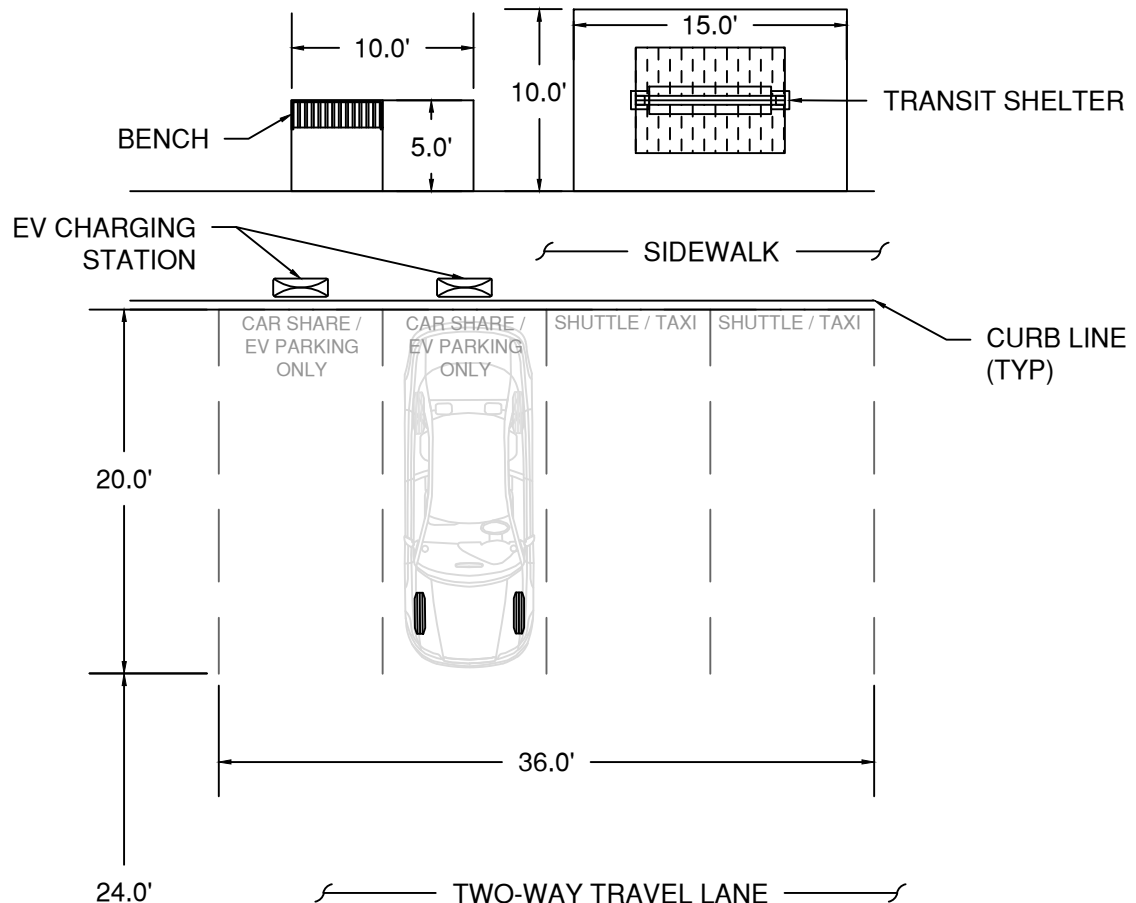
MOBILITY POINTS - SMALL/MEDIUM

SCALE NTS

DATE 11/01/2024

APPR

STD DWG R-50A



- NOTES:
1. LOCATION & EXISTING CONDITIONS WILL DETERMINE LAYOUT
  2. FINAL LAYOUT MUST MEET MINIMUM ADA STANDARDS FOR ACCESSIBLE DESIGN
  3. ALL CONCEPTS SHOWN ARE FOR SPATIAL REPRESENTATION ONLY

DRAWN AJD	
DIV	ROADWAY
REV	DATE



**CITY OF BEND**  
 STANDARD DRAWING  
 710 NW WALL ST., BEND, OREGON 97701  
**MOBILITY POINTS - LARGE**

SCALE NTS
DATE 11/01/2024
APPR
STD DWG R-50B