



BMPO Bend Metropolitan
Planning Organization

2018-2021 Metropolitan Transportation Improvement Program



Adopted May 18, 2017 by the MPO Policy Board

Prepared by: **Bend Metropolitan Planning Organization**

710 NW Wall Street

Bend, OR 97703

www.bendmpo.org

Resolution Number 2017-04
Bend Metropolitan Planning Organization Policy Board

For the Purpose of Adopting the
2018-2021 Metropolitan Transportation Improvement Program

WHEREAS, the US Department of Commerce, Bureau of Census has declared that the City of Bend and the adjoining areas in Deschutes County form an Urbanized Area, named the Bend Urbanized Area; and

WHEREAS, the State of Oregon has designated representatives of the said areas, together with a representative of ODOT, as the Bend Metropolitan Planning Organization (BMPO) to carryout the Metropolitan Transportation Planning Process; and

WHEREAS, among the major requirements of the Metropolitan Transportation Planning Process is the development of a Metropolitan Transportation Improvement Program (MTIP) that enumerates priority transportation projects in the Bend Urbanized Area; and

WHEREAS, the BMPO has developed a FY2018-2021 Metropolitan Transportation Improvement Program in coordination with ODOT and the local transit provider in compliance with all applicable federal and state requirements; and

WHEREAS, the FY2018-2021 Metropolitan Transportation Improvement Program meets the federal requirement of financial constraint; and

WHEREAS, the public has been notified and afforded reasonable opportunities to review and comment on the content of the FY2018-2021 MTIP.

NOW, THEREFORE, BE IT RESOLVED, that the Policy Board of the Bend MPO approves and adopts the FY2018-2021 Metropolitan Transportation Improvement Program and directs staff to submit the document to ODOT for inclusion in the FY2018-2021 Statewide Transportation Improvement Program.

Adopted by the Bend Metropolitan Planning Organization the 18th day of May 2017.

Yes: 3 No: 0 Abstain: 0

Authenticated by the Chair this 18th day of May 2017.



William Moseley, Chair

Attest:



Tyler Deke, MPO Manager

BMPO OVERVIEW

The primary function of the Bend Metropolitan Planning Organization (BMPO) is to conduct a continuing, cooperative and comprehensive transportation planning process that results in plans and programs that consider all transportation modes and support metropolitan community development and social goals. The BMPO was designated on December 18, 2002, by the Governor of Oregon. Local jurisdictions involved in the planning activities of the BMPO include the City of Bend and Deschutes County. In addition, the Oregon Department of Transportation, Oregon State University Cascades, Central Oregon Community College, Cascades East Transit, Central Oregon Intergovernmental Council, Commute Options, Federal Highway Administration, and Federal Transit Administration participate in the MPO process.

The BMPO organizational structure is designed so that it operates as an entity separate from the participating jurisdictions so that no single entity dominates the organization's decision-making processes. A Policy Board oversees the process of the BMPO. The Policy Board is comprised of three members of the Bend City Council, one member of the Deschutes County Board of Commissioners, and the local Area Manager from ODOT Region 4. As future major transportation providers form, such as a Transit District, they will be added to the Policy Board. The intergovernmental agreement specifies that no decisions shall be made by the BMPO Policy Board without representation from all parties. The BMPO planning boundary is shown in Map 1.

Federal and state transportation planning responsibilities for the BMPO can generally be summarized as follows:

- Develop and maintain a Metropolitan Transportation Plan (MTP) and Metropolitan Transportation Improvement Program (MTIP) consistent with state and federal planning requirements.
- Review specific transportation and development proposals for consistency with the MTP.
- Coordinate transportation decisions among local jurisdictions, state agencies, and area transit operators.
- Develop an annual work program (known as the Unified Planning Work Program [UPWP]).
- Maintain the regional travel-demand model for the purposes of assessing, planning, and coordinating regional travel demand impacts. (NOTE: The BMPO currently coordinates with ODOT's Transportation Planning Analysis Unit for modeling support services).

The BMPO entered into an intergovernmental/interagency agreement (IGA) with the City of Bend establishing the City of Bend as the administrative and fiscal agent for BMPO. This agreement is regularly reviewed and renewed as appropriate.

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*Indicates non-voting members

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INTRODUCTION

The BMPO Metropolitan Transportation Improvement Program (MTIP) identifies transportation projects in the BMPO study area that are scheduled in federal fiscal years 2018-2021. The period begins on October 1, 2017, which is the beginning of the 2018 federal fiscal year, and ends September 30, 2021, which is the end of the 2021 federal fiscal year. The MTIP lists most federally funded and some locally funded projects anticipated by local agencies and the Oregon Department of Transportation (ODOT).

The MTIP contains a four-year listing of anticipated expenditures for locally funded projects drawn from the capital improvement programs of Bend and Deschutes County. It also lists projects from the ODOT Statewide Transportation Improvement Program (STIP). The MTIP lists projects for which application of specific federal funds will be made during the programming period. Projects in the MTIP must be consistent with the Metropolitan Transportation Plan.

By adopting the MTIP, the Policy Board has selected the projects identified in Table 3, *Program of FFY 2018-2021 Transportation Projects*, for implementation and funding as scheduled. No additional action by the Policy Board is required for funding these projects. If additional funds become available or if a project experiences an unexpected delay, the Policy Board may select other projects from the first three years of the schedule to take advantage of the additional funds or to replace a delayed project.

FEDERAL REQUIREMENTS

Federal legislation requires that the Bend MPO in cooperation with the state and transit operators develop an MTIP that is updated and approved at least every four years by the Policy Board and the Governor. Copies of the MTIP are provided to the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA). Specific requirements for the MTIP are outlined in various rules developed by FHWA and FTA. A brief description of these requirements is provided in this section.

The Fixing America's Surface Transportation Act (FAST Act) requires the Bend MPO to publish for public review an annual listing of projects for which federal funds have been obligated (49 USC Chapter 53, Section 5303). This provision is intended to increase the transparency of government spending on transportation projects and strategies in the MPO area to state and local officials, and to the public at large. It also helps to ensure that the public will have an accurate understanding of how federal funds are actually being spent on transportation projects.

Over the years, state and federal laws have given MPOs an increasingly important role in financing transportation improvements. At the federal level, the 1991 Intermodal Surface Transportation Efficiency Act and its successor, the Transportation Equity Act for the 21st Century, empowered metropolitan planning organizations to determine the mix of transportation projects best suited to meet their region's needs. Congress reaffirmed the MPOs' role in transportation financing with the subsequent passage of SAFETEA-LU, MAP-21, and now the FAST Act.

Regulations developed to help guide the implementation of the FAST Act include several requirements:

Time Period (23 CFR 450.326)

The MTIP must cover a period of not less than four years. However, if the TIP covers more than four years, FHWA and FTA consider the projects in the additional years as informational.

Public Involvement and Comment (23 CFR 450.326)

There must be reasonable opportunity for public comment prior to approval and the MTIP must be made readily available including electronically accessible formats and means such as publication on the internet.

Projects (23 CFR 450.326)

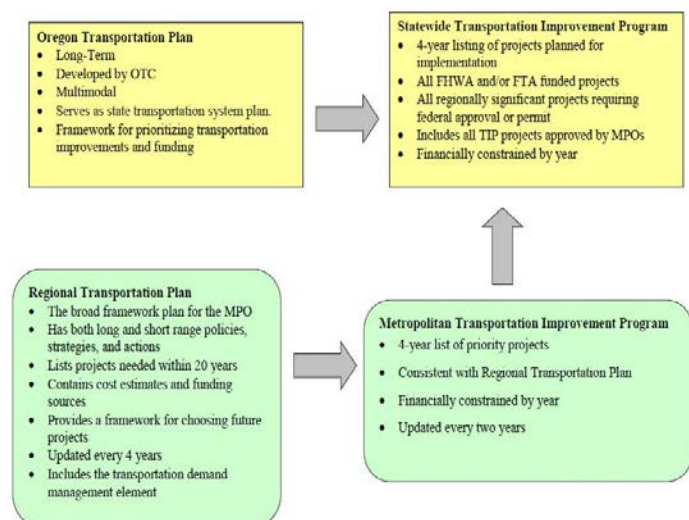
The MTIP must include all federally funded projects (including pedestrian walkways, bicycle transportation facilities, and transportation enhancement projects) to be funded under Title 23 and the Federal Transit Act and regionally significant projects requiring an action by FHWA regardless of funding source. Projects in the MTIP must be consistent with the metropolitan transportation plan (MTP).

Financial Constraint

The MTIP must be consistent with funding that is expected to be available during the relevant period. The MTIP must be financially constrained by year and include a financial plan that demonstrates which projects can be implemented using current revenue sources and which projects are to be implemented using proposed revenue sources. Only projects for which funds are reasonably expected to be available can be included in the MTIP. As the amount of federal funds coming into the region may vary as the result of Congressional actions, the revenues anticipated in the MTIP represent the best estimates possible currently. Programmed projects may need to be delayed or phased over two or more years if less funding is received than originally forecast. The scheduling of projects listed may also change due to delays in funding, project changes, and other unforeseen circumstances.

MTIP and STIP Relationship (23 CFR 450.328)

The STIP is a listing of transportation projects and programs that shows prioritization, funding, and scheduling of transportation projects and programs over four years. It includes projects on Oregon's interstate, federal, state, city, and county transportation systems. The STIP covers highway, passenger rail, public transit, bicycle and pedestrian projects, and includes projects on federal lands in Oregon.



The frequency and cycle for updating the MTIP must be compatible with Oregon's STIP development and approval process. The current MTIP expires when FHWA and FTA approval of the current STIP expires. After approval of the MTIP by the Policy Board and the Governor, the MTIP must be included without modification directly or by reference in the STIP. The portion of the STIP in the metropolitan planning area shall be developed by the Bend MPO in cooperation with the ODOT STIP coordinators.

MTIP DEVELOPMENT

Federal regulations require the MPO to plan a program of transportation investments for the urban area covering a period of at least three years. The 2018-2021 MTIP serves as the mechanism to focus and prioritize the short-term schedule and funding programming for the improvements identified in the long-term MTP. The MTIP and MTP for the Bend MPO are consistent, which is required by 23 CFR 450.236. The MTIP provides the mechanism by which the implementation of the MTP is monitored, managed, and reviewed.

The MTIP is the formal programming mechanism which commits funds to specific transportation projects. MTIP funding levels indicate regional commitments to specific dollar amounts, not necessarily the completion of projects. Some projects may require phasing over multiple years. The MTIP must contain all the transportation projects which either: a) use federal funds; or b) use state and/or local funds and are deemed to be "regionally significant." In addition, the MTIP must describe the selected projects and identify the funding necessary to complete the improvements.

By adopting the MTIP, the BMPO Policy Board prioritizes and selects the projects for implementation and funding as scheduled. No additional action by the Policy Board is required for the funding of these projects up to the dollar amounts programmed in the MTIP. If additional funds become available or if a project experiences an unexpected delay, the Policy Board may select other projects from the MTIP to take advantage of the additional funds or to replace a delayed project.

Regionally Significant Projects

While the Bend MPO area has not been classified as non-attainment for any of the air quality criteria pollutants, the Oregon transportation air quality conformity rule¹ provides a good definition for regionally significant projects.

"Regionally significant project" means a transportation project that is on a facility which serves regional transportation needs, such as access to and from the area outside the region, major activity centers in the region, major planned developments such as new shopping areas, event facilities/complexes, etc., or transportation terminals, and would normally be included in the modeling of a metropolitan area's transportation network.

In the Bend metropolitan area, regionally significant facilities will include, at a minimum:

- All state highways;
- All arterial roadways; and
- Any other facilities determined to be regionally significant by the Policy Board (in consultation with the Technical Advisory Committee).

¹ Oregon Administrative Rule (OAR) 340-252-0030 (Transportation Air Quality Conformity Rule)

[Note: It is the practice of the Bend MPO to include minor arterials and collectors in the travel model for the purpose of accurately modeling regional VMT and associated performance measures. The inclusion of collectors in the travel model, however, does not imply those facilities as being regionally significant.]

MTIP Development

The Technical Advisory Committee (TAC), the staff group responsible for most of the technical details of MPO transportation planning, developed the draft MTIP. The TAC assembled the MTIP from the adopted capital improvement programs (CIPs) and other capital planning documents from the City of Bend and Deschutes County. It also includes projects from the ODOT STIP and projects from the overlapping years of the previous (FY15-18) MTIP.

The TAC recommends the MTIP to the Policy Board for review and adoption. The Policy Board, which is composed of elected or appointed officials from Bend, Deschutes County and ODOT, conducts a public hearing and adopts the MTIP.

MTIP Amendments

Amendments to the MTIP should be submitted to MPO staff for review. Staff will work with the project sponsor, ODOT and United States Department of Transportation (USDOT) to determine if the proposed amendment is classified as a full amendment or an administrative amendment. Full amendments require Policy Board adoption and a 20-day public notification process. See www.bendmpo.org/mtip notifications.

Changes to regionally significant projects may or may not necessitate a full MTIP amendment. The following definitions and processes should be used to determine what changes to project concept and scope are to be considered significant or not for purposes of a full MTIP amendment.

1. Adding or extending expressway auxiliary lanes or weaving lanes between interchanges is not considered a significant change in concept and scope since these lanes are not normally included in the travel model.
2. Adding or extending expressway auxiliary/weaving lanes from one interchange to a point beyond the next interchange is considered a significant change in concept and scope.
3. A change to a regionally significant project defined in the MTP that does not change how the project is defined in the travel model is not considered a significant change in concept and scope. These changes include but are not limited to lane or shoulder widening, changes to cross section (other than the number of through lanes), vertical or horizontal alignment changes, interchange reconfiguration, intersection signalization projects at individual intersections, addition of turn lanes, continuous or center turn lanes, and storage lanes.
4. A change to a regionally significant project defined in the MTP that does alter the number of through lanes, lane capacity, or speed classification as defined in the travel model is considered a significant change in concept and scope.
5. Advancing or delaying the planned implementation of a regionally significant project that does not result in a change in the transportation network described in the travel model for any horizon year (as defined in 40 CFR 93.101) is not considered a significant change in concept and scope.

6. Advancing or delaying the planned implementation of a regionally significant project that does result in a change in the transportation network described in the travel model for any horizon year (as defined in 40 CFR 93.101) is considered a significant change in concept and scope.

7. Other regionally significant projects include the procurement of additional (not replacement) public transportation vehicles and the construction of new park and ride facilities.

All administrative amendments shall be forwarded to the Policy Board and Technical Advisory Committee for informational purposes. The Policy Board may request further review of administrative amendments.

Table 1 provides a detailed description of how amendments are made to the MTIP.

TABLE 1: MTIP AMENDMENT PROCESS

Type of Change	Federal Action***	Full Amend	Admin Amend	Database /Document Change
If it is NOT in the MTIP:				
1. Adding a state or federally funded (FHWA or FTA*) project, or a project that requires an action by FHWA or FTA (any funding source), to the MTIP	Approval if in first 3 years	✓		
2. Adding a regionally significant project to the MTIP (any funding source)	Approval if in first 3 years	✓		
3. Adding a federally funded project that is funded with discretionary funds	Notification		✓	
4. Adding a non-federally funded project that doesn't require FHWA or FTA action to the MTIP	Notification		✓	
If it is already in the MTIP:				
5. Deleting a state or federally funded project, or a project that requires an action by FHWA or FTA (any funding source), from the MTIP	Approval if in first 3 years	✓		
6. Major change in scope of a project with state or federal funds	Approval if in first 3 years	✓		
7. Adding or deleting a CN phase to an approved MTIP project.	Approval	✓		
8. Advancing a project or phase of a project from the fourth year to the first three years of the MTIP**	Approval	✓		
9. Advancing an approved project or phase of a project from year 2 or 3 into the current year of the MTIP	Notification		✓	
10. Slipping an approved project or phase of a project from the current year of the MTIP to a later year	No action			✓

Type of Change	Federal Action	Full Amend	Admin	Database /Document Change
11. Adding PE or ROW phase to an approved project in the first three years of the MTIP	Notification		✓	
12. Combining two or more approved projects into one project	Notification		✓	
13. Splitting one approved project into two or more projects	Notification		✓	
14. Minor technical corrections to make the MTIP consistent with prior approvals	Notification		✓	
15. Adding FHWA funds to an approved FTA-funded project	Notification		✓	
16. Increasing or decreasing the federal funds of an FTA-funded project, without affecting fiscal constraint of the MTIP	Notification		✓	
17. Increasing or decreasing the federal funds of an FHWA-funded project, without affecting fiscal constraint of the MTIP	No action			✓
18. Modifying or deleting local projects that are listed for information purposes only	No action			✓
19. Minor technical corrections to make the MTIP consistent with the prior approvals, such as typos or missing data.	Notification		✓	
20. Changing the name of a project due to change in scope, combining or splitting of projects, or to better conform to naming convention.	Notification		✓	

*Funds from 49 USC Chapter 53 or 23 USC, excluding State Planning & Research funds, Metropolitan Planning funds, and most Emergency Relief funds.

**The federally approved STIP contains years one to three; year four is informational only.

***See Appendix G for the ODOT – FTA – FHWA amendment matrix.

Public Involvement Process

BMPO staff consulted ODOT and local agencies to identify projects scheduled for the 2018-2021 period. Indirect public involvement opportunities were available through the ODOT STIP and City and County CIP processes. ODOT has a formal public process associated with development of the STIP and the City and County also have public processes associated with development of their respective CIPs.

This MTIP was prepared as a coordinated and cooperative effort of the BMPO member jurisdictions. Development of the draft MTIP was included as an agenda item at meetings of the TAC and Policy Board. E-mail notification of all BMPO Policy Board meetings is provided to local and regional media. E-mail notification of all Policy Board meetings is also provided to a list of local stakeholders and agency staff. Each Policy Board agenda includes formal time for visitor introductions and comment. Discussion at Policy Board and TAC meetings is also open to everyone in attendance. In addition, all MPO committee agendas are posted on the MPO website.

Prior to the MTIP review and adoption by the Policy Board, the proposed MTIP undergoes a 30-day public review and comment period.

Response to Public Comment

23 CFR 450.316 requires explicit consideration and response to public comment received during the program development process. Public, staff, agency, and other interested party comments received prior to the adoption hearing are first reviewed by staff. Comments requiring minor revisions are addressed by staff. Such comments might include requests for additional information or clarification of information. Comments on policy issues or specific projects will be considered by the Policy Board at the public hearing. Comment received during the public hearing will be discussed at the public hearing. The Policy Board and staff will determine the most appropriate manner to respond to comments received. If significant changes to the proposed MTIP are recommended as a consequence, a revised final draft document will be resubmitted to the public for an additional review and comment period. A report on the disposition of comments will be included in Appendix F.

MTIP PROJECTS

Description of Project Listings

Individual projects vary enough that their descriptions are necessarily general. For street projects, all are assumed to be urban cross-sections with curb, gutter, drainage, bike lanes, and sidewalks, unless otherwise noted. When provisions for bicycles are anticipated, they are specifically mentioned.

Projects are grouped by agency responsible for carrying out the project.

Project name is prepared based on ODOT conventions, and is the name by which the project is known in the State Transportation Improvement Program (STIP).

Project description is the description provided by the project sponsor; due to STIP constraints, this description may be abbreviated when included in the STIP.

Key number is the project number, assigned by ODOT, by which the project is known in the STIP. A project which covers several years may have a different key number for each year.

Fiscal Year is the Federal fiscal year in which the funds for the indicated project phase or stage are expected to be obligated through a contractual or intergovernmental agreement.

Phase indicates the type of work undertaken in the year indicated. For projects other than transit, this is typically planning (Plan), preliminary engineering (PE), right of way acquisition (ROW), utility relocation (UR), or construction (Cons). Transit projects typically consist of operations support (Ops) and capital support (Cap).

Federal Cost and Source indicates the amount of federal funding that is programmed for this phase, and the type of federal funds (see below).

Federal Required Match Cost and Source indicates the amount of local money that must be programmed in order to match the federal funding. This is typically 10.27%, 20% or 50% of the total project cost, depending on the federal source.

Other Cost and Source indicates local funds that are programmed for the project phase in excess of any federal funds or local match to federal funds.

Total All Sources indicates the cost estimate of the project phase or stage regardless of fund source.

Costs are only estimates, although some are more refined than others.

Funding source refers to the agencies expected to participate in the project. In some cases, funding agreements have not yet been finalized so agencies listed will not necessarily participate in the project listed. A description of the various funding sources is provided in Appendix D. Meanings of the abbreviations used in MTIP tables are as follows:

For a project which began prior to FY18, phases that are either under contract, under construction or completed are included here for informational purposes.

Table 2 lists the abbreviations that are used to identify the funding sources for projects identified in the MTIP.

TABLE 2 - FUND SOURCE ABBREVIATIONS

<i>Federal Sources</i>	
FLAP	Federal Lands Access Program
HSIP	Highway Safety Improvement Program
NHPP	National Highway Performance Program
NHS	National Highway System
SPR	State Planning and Research
STBG /STP	Surface Transportation Block Grant Program
TA	Transportation Alternatives Program
S5303	Transit: Planning, Research and Training
S5307	Transit: Urban Operations Support
S5310	Transit: Elderly and Disabled Services
S5311	Transit: Rural Operations Support
S5339	Transit: Bus and Bus Facilities Program
<i>State Sources</i>	
State Hwy Fund	State Highway Fund
STF	Special Transportation Fund (Transit)
TDM	Transportation Demand Management
<i>Local Sources</i>	
Bend	City of Bend (SDCs, General Fund, etc.)
Des Co	Deschutes County Roadway Fund

Table 3 - LIST OF PROJECTS BY AGENCY

Table 3 lists the projects by agency, including federally funded projects. Projects in this table are consistent with MTP policies and include local projects that implement the MTP.

It should be noted that the costs included for some of the projects may not reflect the full costs associated with the listed projects.

Projects for **Bend Metropolitan Planning Organization**

Key#	20613	Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
MPO - 198	Planning	2019	PL	\$168,652	\$303	\$49,093	MPO in kind match	\$5,619	\$223,364
Project Name	BEND MPO PLANNING SFY20	Design		\$0		\$0		\$0	\$0
		Land Purchase		\$0		\$0		\$0	\$0
		Utility Relocate		\$0		\$0		\$0	\$0
		Construction		\$0		\$0		\$0	\$0
Amendment Details		Other		\$0		\$0		\$0	\$0
	Description	Bend MPO planning funds for Federal fiscal year 2019. Projects will be selected in the future through the MPO process with the Unified Planning Work Program development. PL funds include a 10.27% match (\$17,320.53) from ODOT.						Project Total	\$223,364

Key#	20614	Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
MPO - 199	Planning	2020	PL	\$168,652	\$303	\$49,093	MPO in kind match	\$5,619	\$223,364
Project Name	BEND MPO PLANNING SFY21	Design		\$0		\$0		\$0	\$0
		Land Purchase		\$0		\$0		\$0	\$0
		Utility Relocate		\$0		\$0		\$0	\$0
		Construction		\$0		\$0		\$0	\$0
Amendment Details		Other		\$0		\$0		\$0	\$0
	Description	Bend MPO planning funds for Federal fiscal year 2020. Projects will be selected in the future through the MPO process with the Unified Planning Work Program development. PL funds include a 10.27% match (\$17,320.53) from ODOT.						Project Total	\$223,364

Key#	20615	Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
MPO - 200	Planning	2021	PL	\$168,652	\$303	\$49,093	MPO in kind match	\$5,619	\$223,364
Project Name	BEND MPO PLANNING SFY22	Design		\$0		\$0		\$0	\$0
		Land Purchase		\$0		\$0		\$0	\$0
		Utility Relocate		\$0		\$0		\$0	\$0
		Construction		\$0		\$0		\$0	\$0
Amendment Details		Other		\$0		\$0		\$0	\$0
	Description	Bend MPO planning funds for Federal fiscal year 2020. Projects will be selected in the future through the MPO process with the Unified Planning Work Program development. PL funds include a 10.27% match (\$17,320.53) from ODOT.						Project Total	\$223,364

Key#	NEW - 204	Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
MPO - 204	Planning	2018	PL	\$161,476	\$303	\$46,359	MPO in kind match	\$5,306	\$213,141
Project Name	BEND MPO PLANNING SFY19	Design		\$0		\$0		\$0	\$0
		Land Purchase		\$0		\$0		\$0	\$0
		Utility Relocate		\$0		\$0		\$0	\$0
		Construction		\$0		\$0		\$0	\$0
Amendment Details		Other		\$0		\$0		\$0	\$0
	Description	Bend MPO planning funds for Federal fiscal year 2018. Projects will be selected in the future through the MPO process with the Unified Planning Work Program development. PL funds include a 10.27% match (\$17,320.53) from ODOT.						Project Total	\$213,141

Projects for **Cascade East Transit-COIC**

Key#	19405		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
CET - 14		Planning			\$0		\$0		\$0	\$0
Project Name	Bend Transit Operations/Capital 2017 (5307)	Design			\$0		\$0		\$0	\$0
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction			\$0		\$0		\$0	\$0
Amendment Details	12/2/15-002	Other	2017	5307	\$862,606	Local	\$952,637	5307/5337	\$360,125	\$2,175,368
		Description	5307: Operations/Cap ADA/Fuel/Vehicle Prev. Maintenance support for Bend transit system Funding for FFY 2017 from October 1, 2016 - Sept 30, 2017 Estimated Funding						Project Total	\$2,175,368

Key#	19405		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
CET - 159		Planning			\$0		\$0		\$0	\$0
Project Name	Bend Transit Operations Capital 2017 (5307)	Design			\$0		\$0		\$0	\$0
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction			\$0		\$0		\$0	\$0
Amendment Details		Other	2018	5307 5337	\$783,551	Bend	\$908,358	5307 5337	\$484,669	\$2,176,578
		Description	5307: Ops/ADA/PM/Admin/Transit Improvements 50% match for \$783,551 of 5307 5307: Operations/Cap ADA/Fuel/Vehicle Prev. Maintenance support for Bend transit system Funding for FFY 2017 from October 1, 2017 - Sept 30, 2018 Estimated Funding						Project Total	\$2,176,578

Key#	20935		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
CET - 160		Planning			\$0		\$0		\$0	\$0
Project Name	Bend Transit Operations Capital 2018 (5307)	Design			\$0		\$0		\$0	\$0
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction			\$0		\$0		\$0	\$0
		Other	2019	5307 5337	\$783,551	Bend	\$908,358	5307 5337	\$484,669	\$2,176,578
Amendment Details		Description	5307: Ops/ADA/PM/Admin/Transit Improvements 50% match for \$783,551 of 5307 5307: Operations/Cap ADA/Fuel/Vehicle Prev. Maintenance support for Bend transit system Funding for FFY 2018 from October 1, 2018 - Sept 30, 2019 Estimated Funding						Project Total	\$2,176,578

Key#	20936		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
CET - 161		Planning			\$0		\$0		\$0	\$0
Project Name	Bend Transit Operations Capital 2019 (5307)	Design			\$0		\$0		\$0	\$0
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction			\$0		\$0		\$0	\$0
Amendment Details		Other	2020	5307 5337	\$783,551	Bend	\$908,358	5307 5337	\$484,669	\$2,176,578
		Description	5307: Ops/ADA/PM/Admin/Transit Improvements 50% match for \$783,551 of 5307 5307: Operations/Cap ADA/Fuel/Vehicle Prev. Maintenance support for Bend transit system Funding for FFY 2019 from October 1, 2019 - Sept 30, 2020 Estimated Funding						Project Total	\$2,176,578

Projects for Oregon Department of Transportation

Key#	14020		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 5		Planning			\$0		\$0		\$0	\$0
Project Name	FFO: US97 Bend North Corridor Project	Design	2005	STP	\$5,904,000	OTIA	\$1,066,000	ODOT	\$1,230,000	\$8,200,000
		Land Purchase	2018	IM	\$1,451,603	ODOT	\$532,397		\$0	\$1,984,000
		Utility Relocate	2012	STP	\$89,730	ODOT	\$10,270		\$0	\$100,000
		Construction			\$0		\$0		\$0	\$0
Amendment Details	4/20/17-001 technical correction	Other			\$0		\$0		\$0	\$0
		Description	Corridor planning and development. Land Purchase/ROW in 2018						Project Total	\$10,284,000

Key#	19806		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 147		Planning			\$0		\$0		\$0	\$0
Project Name	ALL ROADS TRANSPORTATION SAFETY (CITY OF BEND)	Design	2017	HSIP	\$224,980	Bend	\$18,980		\$0	\$243,960
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction	2018	HSIP	\$889,038	Bend	\$75,002		\$0	\$964,040
Amendment Details	5/16/18-002 Full Amendment approved	Other			\$0		\$0		\$0	\$0
		Description	All Roads Transportation Safety program purpose is to reduce fatal and serious intersection and roadway departure crashes SIGN UPGRADES, ILLUMINATION, SIGNAL IMPROVEMENTS 7.78% local match required.						Project Total	\$1,208,000

Key#	20011		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 194		Planning			\$0		\$0		\$0	\$0
Project Name	US20: DESCHUTES RIVER BRIDGE - ROBAL RD (BEND)	Design	2019	STP-Flex	\$314,055	ODOT	\$35,945		\$0	\$350,000
		Land Purchase	2019		\$0	ODOT	\$50,000		\$0	\$50,000
		Utility Relocate	2020	STP-Flex	\$44,865	ODOT	\$5,135		\$0	\$50,000
		Construction	2020	STP-Flex	\$1,390,815	HSIP	\$1,071,000	ODOT	\$159,185	\$2,621,000
Amendment Details		Other			\$0		\$0		\$0	\$0
		Description	PAVEMENT PRESERVATION (GRIND OUT EXISTING SURFACE AND INLAY/OVERLAY NEW ASPHALT IN TRAVEL LANES), LIGHTING, SIGNS, FLASHING BEACON AND RURAL MEDIAN ACCELERATION LANE AT OLD REDMOND-BEND HWY						Project Total	\$3,071,000

Key#	20013		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 195		Planning			\$0		\$0		\$0	\$0
Project Name	US97: BOWERY LANE - ROMAINE VILLAGE WAY	Design	2018	STP-FLEX	\$224,325	ODOT	\$25,675		\$0	\$250,000
		Land Purchase	2019	ODOT	\$50,000		\$0		\$0	\$50,000
		Utility Relocate	2020	STP-FLEX	\$44,865	ODOT	\$5,135		\$0	\$50,000
		Construction	2020	STP-FLEX	\$3,589,200	ODOT	\$410,800		\$0	\$4,000,000
		Other			\$0		\$0		\$0	\$0
Amendment Details		Description	PAVEMENT PRESERVATION (GRIND OUT PAVEMENT AND INLAY NEW ASPHALT IN TRAVEL LANES)						Project Total	\$4,350,000

Key#	20073		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 196		Planning			\$0		\$0		\$0	\$0
Project Name	ALL ROADS TRANSPORTATION SAFETY (BEND, PHASE 2)	Design	2019	HSIP	\$127,448	BEND	\$10,759		\$0	\$138,207
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction	2021	HSIP	\$519,014	BEND	\$43,786		\$0	\$562,800
		Other			\$0		\$0		\$0	\$0
Amendment Details		Description	ALL ROADS TRANSPORTATION SAFETY (BEND, PHASE 2) SAFETY IMPROVEMENTS INCLUDING: CURVE & INTERSECTION SIGNS, DELINEATORS, ILLUMINATION AND CLEAR ZONE						Project Total	\$701,007

Key#	20357		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 202		Planning			\$0		\$0		\$0	\$0
Project Name	US97: REDMOND - BEND	Design	2017	HSIP	\$813,000		\$0		\$0	\$813,000
		Land Purchase	2018	HSIP	\$100,000		\$0		\$0	\$100,000
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction	2019	HSIP	\$4,115,000		\$0		\$0	\$4,115,000
		Other			\$0		\$0		\$0	\$0
Amendment Details		Description	SPEED FEEDBACK SIGNS, PAVEMENT MARKINGS, LIGHTING, SIGNING AND STRIPING ENHANCEMENTS						Project Total	\$5,028,000

Key#	20378		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 197		Planning			\$0		\$0		\$0	\$0
Project Name	ARCHIE BRIGGS RD (DESCHUTES RIVER) BRIDGES	Design	2020	NHFP	\$726,221	BEND	\$83,119		\$0	\$809,340
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction			\$0		\$0		\$0	\$0
Amendment Details		Other			\$0		\$0		\$0	\$0
		Description	Design shelf ready plans for replacement of bridge #s 17C05 & 17C040.						Project Total	\$809,340

Key#	20391		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 162		Planning			\$0		\$0		\$0	\$0
Project Name	US20: EMPIRE - GREENWOOD (3RD ST, BEND)	Design	2017	STP-FLEX	\$2,374,266	ODOT	\$271,745		\$0	\$2,646,011
		Land Purchase	2018	ODOT-Fix-It	\$2,057,000		\$0		\$0	\$2,057,000
		Utility Relocate	2019	STP-FLEX	\$806,404	ODOT	\$92,296		\$0	\$898,700
		Construction	2020	HSIP	\$633,500	BIKEWAYS	\$1,100,000	STP-FLEX	\$6,396,528	\$8,130,028
Amendment Details	11/18/16-001 Full Amendment	Other			\$0		\$0		\$0	\$0
		Description	Pavement preservation, ADA upgrades, sidewalks, bike lanes, pedestrian crossing, sign and signal upgrades. 2015-2018 MTIP/STIP only includes the Design element of this project. (CN- STP funds have a 10.27% match that is provided by ODOT state funds)						Project Total	\$13,731,739

Key#	20551		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 188		Planning			\$0		\$0		\$0	\$0
Project Name	REGION 4 ITS	Design	2017	STP-FLEX	\$206,379	ODOT	\$23,621		\$0	\$230,000
		Land Purchase	2018	ODOT	\$10,000		\$0		\$0	\$10,000
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction	2018	STP-FLEX	\$963,700	ODOT	\$110,300		\$0	\$1,074,000
Amendment Details		Other			\$0		\$0		\$0	\$0
		Description	Video cameras and communication infrastructure, replace illuminations with LEDs, Variable Message Sign.						Project Total	\$1,314,000

Key#	20714		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 201		Planning	2018	FLAP	\$222,271	ODOT R4	\$27,729		\$0	\$250,000
Project Name	US97: MULTI-USE TRAIL PLANNING STUDY	Design	2018	FLAP	\$20,000		\$0		\$0	\$20,000
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction			\$0		\$0		\$0	\$0
Amendment Details		Other			\$0		\$0		\$0	\$0
		Description	IDENTIFY AND EVALUATE PLANNING CORRIDORS FOR A BICYCLE AND PEDESTRIAN MULTI-USE TRAIL CONNECTING BAKER/KNOTT ROAD - LAVA LANDS VISITOR CENTER.						Project Total	\$270,000

Financial Constraint

The FAST Act requires development of a financial plan as part of the MTP and MTIP planning processes. The financial plan must demonstrate that the existing system of transportation facilities is being adequately operated and maintained. This financial plan must be developed in cooperation with the local jurisdictions and other affected agencies. The financial plan must demonstrate which projects can be implemented using current revenues and funding sources and which projects are to be implemented using proposed new revenue sources, while at the same time demonstrating that the existing system of transportation facilities is being adequately operated and maintained. Cost estimates and replacement schedule(s) must support this determination.

Table 4 provides a summary of the financial analysis and demonstrates that the MTIP is financially constrained. Revenues in the first two years are committed, as programmed in the capital improvement programs of the local and state jurisdictions.

TABLE 4: FY2018-21 MTIP FINANCIAL CONSTRAINT ASSESSMENT

Bend Metropolitan Planning Organization						\$1,040,791
Cascade East Transit-COIC						\$8,705,102
Oregon Department of Transportation						\$40,767,086
Total Obligations						\$50,512,978
Total funds obligated for Bend Metropolitan Planning Organization 2018-2021						
Planning	Design	Land Purchase	Utility Relocate	Construction	Other	Total Obligated Funding
\$1,290,791	\$13,700,518	\$4,251,000	\$1,098,700	21,466,868	\$8,705,102	\$50,512,978

Statement of Financial Constraint: Each project in the Bend MPO FY 18-21 Metropolitan Transportation Improvement Program has an identified funding source or combination of sources reasonably expected to be available over the program period.

Adequate Maintenance and Operation of Existing System

In order to produce a financial plan that demonstrates that the necessary resources are reasonably available to implement the MTIP, the financial plan must also demonstrate that the responsible operating agencies have the capacity to finance the operations, maintenance, and capital replacement activities required to preserve the existing system of transportation facilities. Although the BMPO has no direct operations or maintenance authority, its responsibilities related to the production of coordinated, comprehensive transportation plans for the urban area involve the cooperative development of a financial statement indicating the ability of the various operating jurisdictions to adequately maintain, operate, and provide for capital replacement of their respective facilities.

The Cascades East Transit system submits an annual financial statement to FTA that is referenced in the MTIP (see Appendix C). Additionally, the FAST Act requires transit providers to develop an Asset Management Plan that is regularly updated and submitted to FTA. The Oregon Transportation Commission (OTC) has adopted a policy that preservation of the existing system is its first priority and has directed funds to that purpose statewide. The MTIP projects on

the ODOT system reflect that commitment and ability. The City of Bend has a significant backlog of roadway preservation needs. The Bend MPO Policy Board has directed much of the discretionary federal Surface Transportation Block Grant Program (STBG) funds to street preservation in the City of Bend. In addition, the City also allocates a percentage of its funding (state shared revenues and local general fund) to maintenance and preservation activities. The City's general fund contribution to street preservation has increased significantly in recent years.

Available and Committed Revenues and Funding Sources

Available funds include those funds derived from an existing source or funds historically dedicated to transportation. Federal funds generally available to the region (funds authorized and/or appropriated) on an annual basis are considered committed. Based on historical authorizations, federal funds distributed by formula can be extrapolated beyond the current authorization and be considered committed. Federal funds distributed on a discretionary basis are regarded as a new source and must be shown to be reasonably available.

In the case of state funds that are not dedicated to or historically used for transportation purposes, only those funds over which the Governor has control may be considered to be committed funds. In this case, approval of the MTIP by the Governor will be considered a commitment of funds.

For local or private sources of funding involved in regionally significant projects, those not dedicated to or historically used for transportation purposes (including donations of property), a commitment in writing or letter of intent by the responsible official or body having control of the funds is needed to establish a commitment. If these commitments cannot be demonstrated, the state, local, or private funding source should be treated as a "new" funding source and must be demonstrated to be "reasonably available."

Reasonably Available Revenues and Funding Sources

These funds may not currently exist or may require some steps before a jurisdiction, agency, or private party can commit such revenues to transportation projects. The financial plan must identify strategies and a specific plan of action that describe the steps that will be taken to ensure the availability of such funding sources within the planning timeframe. The plan of action should provide information on the actions that will be taken to obtain the new funding, including how local match will be obtained. Where efforts are already underway to obtain a new revenue source, information such as the amount of support for the measure by the community should be included in the financial analysis used for the financially constrained MTIP. Appendix D describes the revenue sources that fund the projects contained within the MTIP.

Bend MPO Surface Transportation Block Grant Program Funds

Under a cooperative process, ODOT distributes a portion of its Surface Transportation Block Grant Program (STBG) funds to cities, counties, and Oregon's small Metropolitan Planning Organizations. The Bend MPO receives an annual allocation of federal STBG funds. Historically, the vast majority of STBG funds have been used for street preservation and maintenance in the City of Bend. To simplify access to these funds, the City enters into a fund-exchange agreement with ODOT. Through the exchange program, ODOT retains the federal funds and the City receives state roadway funds. These projects are not typically included in the MTIP. In the future, the funds may remain federal to allow a wider range of project options. If that occurs, the MTIP will be amended as necessary. Future allocations of Bend MPO STBG funds will be distributed on a year-by-year basis.

ODOT Region 4 Transit Funding

ODOT Region 4 has supported transit for many years by allocating a portion of available STBG funds to transit. Available funding amounts can fluctuate but the region tries to maintain an

historical average. The funds are restricted to transit capital improvements. The application and selection process is managed by the ODOT Public Transit Section.

Transportation Demand Management Program

Funding is provided each year in the STIP for Transportation Demand Management (TDM) activities. The funding is listed as "Region 4 TDM Program" in Table 6 of the MTIP.

Funding Flexibility

Under current federal legislation and Oregon constitutional restrictions, some flexibility exists in the use of transportation funds (Table 5). The possible applications, however, are relatively limited. For example, transit operations are eligible under only three categories of funds, and many fund sources are restricted to roadway-related uses or uses within roadway rights-of-ways.

TABLE 5: FUNDING FLEXIBILITY MATRIX
General Guidelines for the Use of Transportation Funding

POTENTIAL USES	Transit			Roadway				
	Transit Operations	Capital Improvements	ADA/Elderly & Disabled	Maintenance & Operations	Roadway Capacity	Bicycle	Pedestrian	Rideshare/TDM
FUNDING SOURCES								
Federal								
National Highway System	no	no	no	yes	yes	yes	yes	no
STGB	no	yes	yes	yes	yes	yes	yes	yes
BMPO STBG	no	yes	yes	yes	yes	yes	yes	yes
FTA Section 5307	yes	yes	yes	no	no	no	no	no
FTA Section 5339	no	yes	yes	no	no	no	no	no
FTA Section 5310 (a)	no	no	yes	no	no	no	no	no
FTA Section 5311	yes	yes	no	no	no	no	no	no
State								
Gas Tax Revenues	no	no	no	yes	yes	yes	yes	yes (b)
Special Transportation Fund (STF) (c)	yes	yes	yes	no	no	no	no	No
Local								
Bend SDC (d)	no	no	no	no	yes	yes	yes	No
Gas Tax Revenue	no	no	no	yes	yes	yes	yes	Yes
Bend Urban Renewal	no	yes	no	no	yes	yes	yes	No

(a) Limited to private, non-profit organizations or public bodies that coordinate transportation services for the elderly and disabled persons.

(b) Potential uses may include park-and-ride facilities only as part of eligible highway improvement projects.

(c) May be used for transit capital improvements and ADA/elderly & disabled operations; cannot be used for general transit system operations.

(d) Limited to roadway capacity projects and bicycle and pedestrian facility improvements.

TABLE 6 - OTHER PROJECTS BY AGENCY

Not all transportation projects and programs need to be included in the 2018-2021 MTIP project list (Table 3). Projects and programs that do not need to be listed include some planning projects, some state-funded transit projects, and some locally funded projects. To help further public knowledge about transportation funding in the Bend MPO area, a list of these projects is provided in Table 6. The table lists the projects by agency. Projects in this table are consistent with MTP policy and include local projects that implement portions of the MTP.

Bend Metropolitan Planning Organization 2015-2018 Metropolitan Transportation Improvement Plan TABLE 6 - OTHER PROJECTS BY AGENCY									
Key#	18650		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Total
CO - 180		Planning			\$0		\$0	\$0	\$0
Project Name	REGION 4 TDM PROGRAM (2018)	Design			\$0		\$0	\$0	\$0
		Land Purchase			\$0		\$0	\$0	\$0
		Utility Relocate			\$0		\$0	\$0	\$0
		Construction			\$0		\$0	\$0	\$0
Amendment Details	MTIP 2018 2/16/17-001	Other	2018	STP FLEX	\$110,368	LOCAL	\$12,632	\$0	\$123,000
		Description	TRANSPORTATION DEMAND MANAGEMENT FOR VARIOUS COUNTIES IN REGION 4.					Project Total	\$123,000
Key#	18653		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Total
CET - 181		Planning			\$0		\$0	\$0	\$0
Project Name	REGION 4 TRANSIT SUPPORT 2018 (STP TRANSFER)	Design			\$0		\$0	\$0	\$0
		Land Purchase			\$0		\$0	\$0	\$0
		Utility Relocate			\$0		\$0	\$0	\$0
		Construction			\$0		\$0	\$0	\$0
Amendment Details		Other	2018	STP-FLEX	\$200,098	LOCAL	\$22,902	\$0	\$223,000
		Description	SUPPORT TRANSIT ACTIVITIES IN REGION 4. Various counties.					Project Total	\$223,000
Key#	18686		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Total
ODOT - 189		Planning			\$0		\$0	\$0	\$0
Project Name	US20: POWELL BUTTE HWY - HORSE RIDGE FRONTAGE RD	Design	2016	NHPP	\$201,893	ODOT	\$23,108	\$0	\$225,000
		Land Purchase	2017	ODOT	\$35,000		\$0	\$0	\$35,000
		Utility Relocate	2018	NHPP	\$4,487	ODOT	\$513	\$0	\$5,000
		Construction	2018	NHPP	\$2,989,804	ODOT	\$342,196	STP	\$11,270
Amendment Details		Other			\$0		\$0	\$0	\$0
		Description	NHPP = NATIONAL HIGHWAY PERFORMANCE PROGRAM. https://www.oregon.gov/ODOT/HWY/REGION4/Pages/US20PowellButteHorseRidge.aspx					Project Total	\$3,608,270

Key#	20009		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 190		Planning			\$0		\$0		\$0	\$0
Project Name	REGION 4 SIGNAL UPGRADES	Design	2018	STP FLEX	\$269,190	ODOT	\$30,810		\$0	\$300,000
		Land Purchase	2019	ODOT	\$250,000		\$0		\$0	\$250,000
		Utility Relocate	2019	STP FLEX	\$44,865	ODOT	\$5,135		\$0	\$50,000
		Construction	2020	STP FLEX	\$358,920	ODOT	\$41,080		\$0	\$400,000
		Other			\$0		\$0		\$0	\$0
Amendment Details		Description	INSTALL ADA COMPLIANT CURB RAMPS AND PUSH BUTTONS AT TRAFFIC SIGNAL INTERSECTIONS. TRANSITION SIGNAL CONTROLLERS TO THE ADVANCED TRAFFIC CONTROLLER STANDARD						Project Total	\$1,000,000

Key#	20075		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 191		Planning			\$0		\$0		\$0	\$0
Project Name	ARTS DESCHUTES COUNTY PHASE 2 AND CROOK COUNTY	Design	2019	HSIP	\$176,472	LOCAL	\$14,888		\$0	\$191,360
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction	2021	HSIP	\$700,540	LOCAL	\$59,100		\$0	\$759,640
		Other			\$0		\$0		\$0	\$0
Amendment Details		Description	CURVE SIGNS, DELINEATORS, INTERSECTION SIGNS, ILLUMINATION AND CLEAR ZONE						Project Total	\$951,000

Key#	20168		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
CET - 193		Planning			\$0		\$0		\$0	\$0
Project Name	CASCADE EAST TRANSIT REGIONAL SHUTTLE BUS	Design			\$0		\$0		\$0	\$0
		Land Purchase			\$0		\$0		\$0	\$0
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction			\$0		\$0		\$0	\$0
		Other	2019	STP FLEX	\$238,053	LOCAL	\$27,247		\$0	\$265,300
Amendment Details		Description	COMMUNITY CONNECTOR SHUTTLE STOPS/SHELTERS						Project Total	\$265,300

Key#	20357		Year	1st Fund	Share	2nd Fund	Share	3rd Fund	Share	Total
ODOT - 186		Planning			\$0		\$0		\$0	\$0
Project Name	US97: REDMOND - BEND	Design	2017	HSIP - FA	\$513,000		\$0		\$0	\$513,000
		Land Purchase	2018	HSIP	\$100,000		\$0		\$0	\$100,000
		Utility Relocate			\$0		\$0		\$0	\$0
		Construction			\$0		\$0		\$0	\$0
Amendment Details		Other			\$0		\$0		\$0	\$0
		Description	SPEED FEEDBACK SIGNS, PAVEMENT MARKINGS, LIGHTING, SIGNING AND STRIPING ENHANCEMENTS						Project Total	\$613,000

Total funds obligated for Bend Metropolitan Planning Organization 2018-2021

Planning	Design	Land Purchase	Utility Relocate	Construction	Other	Total of all Projects
\$440,999	\$1,229,360	\$385,000	\$55,000	\$8,820,042	\$611,300	\$11,541,701

Appendix A: BMPO Resolution Adopting MTIP

Resolution Number 2017-04 Bend Metropolitan Planning Organization Policy Board

For the Purpose of Adopting the 2018-2021 Metropolitan Transportation Improvement Program

WHEREAS, the US Department of Commerce, Bureau of Census has declared that the City of Bend and the adjoining areas in Deschutes County form an Urbanized Area, named the Bend Urbanized Area; and

WHEREAS, the State of Oregon has designated representatives of the said areas, together with a representative of ODOT, as the Bend Metropolitan Planning Organization (BMPO) to carryout the Metropolitan Transportation Planning Process; and

WHEREAS, among the major requirements of the Metropolitan Transportation Planning Process is the development of a Metropolitan Transportation Improvement Program (MTIP) that enumerates priority transportation projects in the Bend Urbanized Area; and

WHEREAS, the BMPO has developed a FY2018-2021 Metropolitan Transportation Improvement Program in coordination with ODOT and the local transit provider in compliance with all applicable federal and state requirements; and

WHEREAS, the FY2018-2021 Metropolitan Transportation Improvement Program meets the federal requirement of financial constraint; and

WHEREAS, the public has been notified and afforded reasonable opportunities to review and comment on the content of the FY2018-2021 MTIP.

NOW, THEREFORE, BE IT RESOLVED, that the Policy Board of the Bend MPO approves and adopts the FY2018-2021 Metropolitan Transportation Improvement Program and directs staff to submit the document to ODOT for inclusion in the FY2018-2021 Statewide Transportation Improvement Program.

Adopted by the Bend Metropolitan Planning Organization the 18th day of May 2017.

Yes: 3 No: 0 Abstain: 0

Authenticated by the Chair this 18th day of May 2017.



William Moseley, Chair

Attest:



Tyler Deke, MPO Manager

Appendix B: Status of Projects from 2015-2018 MTIP

Key#	Project Name	Project Description	Completed Comments	Total Project Cost
Jurisdiction Bend Metropolitan Planning Organization				
19903	Bend MPO Planning (FFY 2016)	Bend MPO planning funds for Federal fiscal year 2016. Projects will be selected in the future through the MPO process with the Unified Planning Work Program development. PL funds include a 10.27% match (\$17,320.53) from ODOT.	Completed 2016-2017	\$157,558.00
19904	Bend MPO 5303 Funds	Planning work as described in the Bend MPO Unified Planning Work Program www.bendoregon.gov/mpobudget (Agreement 31397) obligated through agreement 31397.	Funds obligated in FFY 2016	\$50,094.00
20753	BEND MPO PLANNING SFY18	Bend MPO planning funds for Federal fiscal year 2017. Projects selected by MPO process with the Unified Planning Work Program development. PL funds include a 10.27% match (\$17,320.53) from ODOT.	This is for the 2017-2018 Work Plan. Not Completed.	\$213,141.00
None	Bend MPO Transit Planning 2016	\$65,000 Added to Agreement 30948 6/2016 (52,000 5303 and 13,000 Local In-Kind match to be provided by CET). Bend MPO match requirement of 2,263. (Agreement 30948)	Funds Obligated, work to be complete in Fall 2017.	\$76,315.00
None	Bend MPO 5303 Funds	Planning work as described in the Bend MPO Unified Planning Work Program www.bendoregon.gov/mpobudget (Agreement 31397)	Project in process for TBEST transit fixed route planning with CET 2017	\$104,329.00
Jurisdiction Bend Park and Recreation District				
19425	Mill District Reach of the Deschutes River Trail	The project will complete a section of the planned Deschutes River Trail (DRT), known as the "Mill District Reach" in two segments. RTP: Recreation Trail Program	Completed 2015	\$222,387.00

Key#	Project Name	Project Description	Completed Comments	Total Project Cost
Jurisdiction Cascade East Transit-COIC				
17848	Mt. Bachelor Shuttle Bus	Project to purchase shuttle buses to provide a means for alternative modes of access to Deschutes National Forest. Full Grant award - \$998,700 with Local Match of \$210,000 from Mt. Bachelor.	Funds fully obligated in FFY 2015	\$94,368.00
18367	COIC - 5310 ENHANCED MOBILITY E&D (FY13)	Deschutes Cnty Prev Maint, Vehicle Replacement, Bend Dial a Ride (DAR), Redmond and Sisters Purchased services. (combined description of Key#: 18365, 18368, 18366, 18367)MPO Specific Bend DAR 2013-2014 \$297,014; Grant\$266,817(5310)\$41,497(Local/PTD)	Funds obligated in FFY 2014	\$306,394.00
18368	COIC - 5310 ENHANCED MOBILITY E&D (FY14)	Deschutes Cnty Prev Maint, Vehicle Replacement, Bend Dial a Ride (DAR), Redmond and Sisters Purchased services. (combined description of Key#: 18365, 18368, 18366, 18367)MPO Specific Bend DAR 2013-2014 \$297,014; Grant\$266,817(5310)\$41,497(Local/PTD)	Funds obligated in FFY 2015	\$250,908.00
18943	Bend Hawthorne Station Improvements 5311 funds	Grant award final on 7/21/16 at the OTC (*STIP: Funds are not split out by project, see statewide funds in Key 18943) This project is for construction of loading platforms and heated sidewalks at the transit center in Bend.	Funds obligated in FFY 2016	\$107,000.00
19084	COIC Transit Vehicle Replacement (2015)	Bend MPO Funded CET Fixed Route Transit Vehicle Replacement FFY 2014 \$143,000/match\$16367 (from MPO STP fund FFY 2014 - 10/1/2013 to 9/30/2014) and STP fund FFY 2015 \$147,000/match\$15097.	Funds obligated in FFY 2016	\$321,464.00
19085	Bend Transit Operations/Capital 2014 (5307)	5307: Operations/Cap ADA/Fuel/Vehicle Prev. Maintenance support for Bend transit system Funding for FFY 2014 from October 1 2013 - Sept 30, 2014	Funds obligated in FFY 2014	\$2,194,557.00

Key#	Project Name	Project Description	Completed Comments	Total Project Cost
19086	Bend Transit Operations/Capital 2015 (5307)	5307: Operations/Cap ADA/Fuel/Vehicle Prev. Maintenance support for Bend transit system Funding for FFY 2015 from October 1 2014 - Sept 30, 2015	Funds obligated in FFY 2016	\$2,175,368.00
19404	Bend Transit Operations/Capital 2016 (5307)	5307: Ops/ADA/PM/Admin/Transit Improvements 50% match for \$783,551 of 5307 (ops)=\$783,551. The 20% match for \$484,669 of 5307/5337 (capital)=\$121,167. 10.27% match for 31,800 of STPU=\$3,640 [7/8/16 - 001 ADMIN]	Funds obligated in FFY 2016	\$2,176,578.00
19405	Bend Transit Operations/Capital 2017 (5307)	5307: Operations/Cap ADA/Fuel/Vehicle Prev. Maintenance support for Bend transit system Funding for FFY 2017 from October 1, 2016 - Sept 30, 2017 Estimated Funding	Funds obligated in FFY 2017	\$2,175,368.00
19472	COIC Bus & Bus Facilities Prog (5339) - 2014	Transit Vehicle Replacement. 1/20/15-001Add new project from STIP k#18281 and k#18282. Previously in MTIP as Key 19082.	Funds obligated in FFY 2014	\$990,000.00
19556	COIC - 5310 E&D TRANSIT CAPITAL STP TRANSFER (15-17)	Capital Funds for Cascade East Transit http://www.oregon.gov/ODOT/PT/programs/enhanced-mobility/awards-enhanced-mobility.pdf	Funds obligated in FFY 2016	\$700,074.00
19623	REGION 4 STP - COIC TRANSIT CAPITAL PROGRAM (15-17)	VEHICLE & EQUIPMENT PURCHASE AND CONTRACTED SERVICE	Funds obligated in FFY 2016	\$742,440.00
19906	Bus Purchase (5339)	One Bus Replacement Awarded from the Discretionary Bus Package, which includes 5307 Mass Transit, 5339 Bus and Bus Facility	Funds obligated in FFY 2016	\$395,000.00
19907	Cascade East Transit Winter Shuttle	OR FS DES 2016(1), Cascade East Transit Winter Shuttle - Federal Lands Access Program (FLAP) funds are administered by FHWA.	Funds obligated in FFY 2016	\$360,200.00

Key#	Project Name	Project Description	Completed Comments	Total Project Cost
19922	COIC MASS TRANSIT PROGRAM (15-17)	One Bus Replacement Awarded from the Discretionary Bus Package, which includes 5307 Mass Transit, 5339 Bus and Bus Facility	Funds obligated in FFY 2016	\$395,001.00
Jurisdiction City of Bend				
17731	S 3rd Street: Wilson Ave to Powers Rd (Bend)	BIKE AND PED IMPROVEMENTS FROM WILSON-POWERS.	Work underway, to be completed July 2017	\$3,555,129.19
19162	City of Bend CNG Fueling Infrastructure	Construct a compressed natural gas fueling station. Project Cancelled.	Project Cancelled.	\$395,000.00

Key#	Project Name	Project Description	Completed Comments	Total Project Cost
Jurisdiction Oregon Department of Transportation				
14020	FFO: US97 Bend North Corridor Project	Corridor planning and development. Land Purchase/ROW in 2018	Project in design phase, EIS published July 2014. Land Purchase/ROW in 2018	\$10,284,000.00
17807	US97: Romaine Village Way - Lava Butte	Widen median and install concrete median barrier	Completed 2016	\$8,401,500.00
18270	US 97/Murphy Rd: Brookwood - Parrell (Bend) Phase 2	REVISED PROJECT DESCRIPTION 9/24/13: EXTEND & REALIGN MURPHY ROAD, CONSTRUCT ROUNDABOUT, INSTALL VARIABLE MSG SIGN FOR SB LANES. Approved 10/17/13.	Project Completed. On/Off Ramps for South Bound deleted from project. Funding shortage.	\$6,909,191.00
18670	US 20: OB Riley Rd & River Mall Ave (Bend)	Traffic Signal Replacement, Operations	combined with 20391	\$1,060,000.00
18672	OB Riley Rd Central Signal System Upgrade (Bend)	Communication Infrastructure for signals to ODOT Facility	combined with 20391	\$136,000.00
18672	Empire Ave: Central Signal System Upgrade (Bend)	Communication Infrastructure for signals to ODOT Facility		\$175,000.00
19166	REGION 4 HSIP TRANSITION URBAN	SIGNAL UPGRADES	Completed 2016	\$502,696.00
19450	US97 @ Powers Rd and Bike/Ped Crossing	PE Funding only identified	Funds Obligated. This project is not completed.	\$219,300.00
19550	COMMUTE OPTIONS - TRANSPORTATION OPTIONS PROGRAM (2015-17)	Promote & encourage the use of carpools, vanpools, transit, bicycling, walking and teleworking. Continues existing carpool matching, regional vanpool services and community outreach programs.	Funds obligated in FFY 2015	\$532,114.00
19571	Bend Central Signal System Phases 3C-3E	Project connects all of the signals in the north corridor (US 97, US 20, Empire, Robal) as well connecting the downtown and MMA US 20 signals into Region 4 HQ	ombined with 20391, to be done in 2018-2021	\$225,001.00

Key#	Project Name	Project Description	Completed Comments	Total Project Cost
19572	US20 @ Empire Ave Signal Rebuild	US 20 at Empire Avenue Signal Rebuild: Design the rebuild of the US 20 at Empire Avenue signal. Upgrade the controller and communications. Design will be compatible with the North Corridor improvements.	ombined with 20391, to be done in 2018-2021combined with 20391	\$75,000.00
19626	Data Warehouse	PE Funding only identified	Work underway, to be completed Fall 2017	\$75,000.00
19639	Region Safety Plans Update	PE Funding only identified-Obligated in 2017	Funds obligated in 2017. Project delayed to 2018.	\$50,000.00
19806	ALL ROADS TRANSPORTATION SAFETY (CITY OF BEND)	All Roads Transportation Safety program purpose is to reduce fatal and serious intersection and roadway departure crashes SIGN UPGRADES, ILLUMINATION, SIGNAL IMPROVEMENTS 7.78% local match required.	Obligated 2017, work to be completed in 2018	\$1,208,000.00
19944	3RD STREET (BEND)	PEDESTRIAN CROSSINGS INCLUDING RAPID FLASHING BEACONS, SIGNS AND ILLUMINATION	Combined with 20391, to be done in 2018-2021	\$126,341.00

Appendix C: Cascades East Transit Statement of Financial Capacity

Each year, Cascades East Transit (CET) provides Federal Transit Agency (FTA) with a signed assurances and certifications through the Transportation Electronic Award Management (TEAM) system. Please contact CET for copies of the certificates and assurances. CET can be contacted at the Bend Operations Center 1250 NE Bear Creek, Bend, OR 97701 (541) 385-8680 or online via <http://www.cascadeseasttransit.com/commentform.html>

Appendix D: Transportation Project Funding Sources

The jurisdictions in the BMPO use a variety of federal, state, and local funding sources for implementing the transportation projects and programs outlined in this MTIP. A brief description of each of the fund sources, along with project programming information is provided below. A breakdown of funding sources by program year is provided in Table 3 of the MTIP.

FEDERAL FUNDING PROGRAMS

In December 2015, the Fixing America's Surface Transportation (FAST) Act was signed into law. This legislation built upon the initiatives established in the several prior transportation legislation packages. Under these Acts, State and local governments were given more flexibility in determining transportation solutions, whether transit, highways, or multimodal projects.

FEDERAL AID HIGHWAY FUNDING PROGRAMS

The FAST Act contains several major funding programs for roadway, safety, and multimodal projects, including the: National Highway Performance Program (NHPP); Surface Transportation Block Grant Program (STBG); Congestion Mitigation and Air Quality Improvement Program (CMAQ); Highway Safety Improvement Program (HSIP); Metropolitan Planning; and Transportation Alternatives (TA). Some of these major programs contain sub-programs. A brief description of several federal aid highway funding programs is provided below. Many, but not all, of these programs are administered by the Federal Highway Administration.

NATIONAL HIGHWAY PERFORMANCE PROGRAM (NHPP) FUNDS

The NHPP provides support for the condition and performance of the National Highway System (NHS), for the construction of new facilities on the NHS, and to ensure that investments of Federal-aid funds in highway construction are directed to support progress toward the achievement of performance targets established in a State's asset management plan for the NHS.

NHPP funds are primarily used to fund upgrade and improvement projects on the Interstate system and U.S. numbered routes (the NHS system). The NHS became the new focus of the Federal Aid Program following the completion of the Interstate Highway System.

SURFACE TRANSPORTATION BLOCK GRANT PROGRAM (STBG)

The STBG is a flexible multi-modal block grant-type program. It provides funds for a broad range of transportation uses and may be used by States and localities for projects on any Federal-aid highway, including the NHS, bridge projects on any public road, transit capital projects, and intracity and intercity bus terminals and facilities. A percentage of the STP funds allocated to the state of Oregon are distributed to cities and counties on a formula basis by the Oregon Transportation Commission.

BEND METROPOLITAN PLANNING ORGANIZATION (BMPO) STBG FUNDS

Under a cooperative process, ODOT distributes a portion of its STBG funds to the small Metropolitan Planning Organizations (MPOs) in Oregon. The Bend MPO receives approximately \$1,000,000 annually in federal STBG funds. STBG funding has increased over time and project identification occurs through the MPO. Future allocations of Bend MPO STBG funds will be distributed on a year-by-year basis.

HIGHWAY SAFETY IMPROVEMENT PROGRAM (HSIP)

The HSIP is a core Federal-aid funding program. The intent of the program is to achieve a significant reduction in traffic fatalities and serious injuries on all public roads. Funds may be used for projects on any public road or publicly owned bicycle and pedestrian pathway or trail. Each State must have a Strategic Highway Safety Plan (SHSP).

TRANSPORTATION ALTERNATIVES PROGRAM (TAP)

The TAP provides funding for programs and projects defined as transportation alternatives, including on- and off-road pedestrian and bicycle facilities, infrastructure projects for improving non-driver access to public transportation and enhanced mobility, community improvement activities, and environmental mitigation; recreational trail program projects; safe routes to school projects; and projects for planning, designing, or constructing boulevards and other roadways largely in the right-of-way of former Interstate System routes or other divided highways.

METROPOLITAN PLANNING

Metropolitan Planning funds are available for MPOs to carry out the metropolitan transportation planning process required by 23 U.S.C. 134, including development of metropolitan area transportation plans and transportation improvement programs. Under 23 U.S.C. 134, MPOs are responsible for developing, in cooperation with the State and affected transit operators, a long-range transportation plan and a metropolitan transportation improvement program (MTIP) for the area.

STATEWIDE PLANNING AND RESEARCH (SPR)

SPR funds may be used for engineering and economic surveys and investigations; the planning of future highway programs and local public transportation systems, and the planning of the financing of such programs and systems including metropolitan and statewide planning; development and implementation of management systems; studies of the economy, safety, and convenience of highway usage and the desirable regulation and equitable taxation thereof; research, development, and technology transfer activities necessary in connection with the planning, design, construction, and maintenance of highways, public transportation, and intermodal transportation systems; and study, research, and training on engineering standards and construction materials for the above systems, including evaluation and accreditation of inspection and testing and the regulation and taxation of their use.

EMERGENCY RELIEF PROGRAM (ER)

The ER program assists State and local governments with the expense of repairing serious damage to Federal-aid and Federal Lands highways resulting from natural

disasters or catastrophic failures. ER funds can be used only for emergency repairs to restore essential highway traffic, to minimize damage resulting from a natural disaster or catastrophic failure, or to protect the remaining facility and make permanent repairs.

FEDERAL LANDS ACCESS PROGRAM (FLAP)

The FLAP provides funds for projects on Federal Lands access transportation facilities that are located on or adjacent to, or that provide access to Federal lands. Funds are distributed by formula among States that have Federal lands managed by the National Park Service, the U.S. Forest Service, the U.S. Fish and Wildlife Service, the Bureau of Land Management, and the U.S. Army Corps of Engineers. 80% of funds go to States that contain at least 1.5% of the national total of public lands, and the remaining 20% going to States with less than 1.5% of the national total.

FEDERAL TRANSIT ADMINISTRATION (FTA) FUNDING

The Federal Transit Administration carries out the federal mandate to improve public transportation systems. It is the principal source of federal assistance to help urban areas (and, to some extent, non-urban areas) plan, develop, and improve comprehensive public transportation systems. The funding programs administered by the FTA include, but are not limited to, the following:

Section 5303 Metropolitan Planning

Section 5307 Urbanized Area Formula Program

Section 5310 Elderly/Disabilities

Section 5311 Rural and Small Urban Areas Formula Program

Section 5339 Bus and Bus Facilities Program

A summary of these programs follows:

TRANSIT SECTION 5303 PROGRAM

Transit Section 5303 funds are part of the Transit Planning and Research Program. These funds are allocated among the following programs: Metropolitan Transit Planning, Rural Transportation Assistance Program, and Statewide planning, research, and training. The Metropolitan planning funds are allocated to states under a formula apportionment on behalf of MPOs based on a state formula cooperatively developed with MPOs and approved by the FTA.

TRANSIT SECTION 5307 PROGRAM

The Section 5307 Formula Grant Program makes funds available on the basis of a statutory formula to all urbanized areas in the country. The funds may be used for public transportation capital, planning, job access and reverse commute projects, as well as operating expenses in certain circumstances. These funds constitute a core investment in the enhancement and revitalization of public transportation systems in the nation's urbanized areas, which depend on public transportation to improve mobility and reduce congestion. For capital projects, the match rate is 80% federal, 20% state or local. Capital funds are used for transit maintenance (e.g., replacing buses), as well as other projects. For operating assistance, the match rate is 50% federal, 50% state or local.

TRANSIT SECTION 5310 PROGRAM

The Section 5310 program provides funding for transportation services for the elderly and persons with disabilities. This program is intended to enhance mobility for seniors and persons with disabilities by providing funds for programs to serve the special needs of transit-dependent populations beyond traditional public transportation services and Americans with Disabilities Act (ADA) complementary paratransit services. The funds may go to private, nonprofit organizations or to public bodies that coordinate service. Funds may be used for capital costs or for capital costs of contracting for services. Section 5310 funds are awarded on an annual competitive basis.

TRANSIT SECTION 5311 PROGRAM

The Section 5311 program provides funding for transportation services for residents in non-urban areas. The funds may be used for both operations support and capital support. 5311 funds have a 50 percent match requirement.

TRANSIT SECTION 5339 PROGRAM

Funds for the Section 5339 provide capital funding to replace, rehabilitate and purchase buses and related equipment and to construct bus-related facilities. The basic matching ratio for capital projects is 80 percent federal, the same as for highway projects in the FHWA program.

OTHER FEDERAL FUNDING PROGRAMS

other federal funding programs are also available to fund transportation projects. A brief description of two programs is provided below.

SECURE RURAL SCHOOLS AND COMMUNITY SELF-DETERMINATION ACT

This Act was reauthorized in 2015. These funds are used to fund improvements to public schools, roads and stewardship projects. Deschutes County receives roadway funding through this program.

COMMUNITY DEVELOPMENT BLOCK GRANTS (CDBG)

Community Development Block Grants are administered by the Department of Housing and Urban Development and could potentially be used for transportation improvements in eligible areas.

STATE FUNDING PROGRAMS

OREGON HIGHWAY FUND

The major source of funding for transportation capital improvements and activities statewide is the Oregon Highway Fund. The Highway Fund derives its revenue through fuel taxes, licensing and registration fees, and weight-mile taxes assessed on freight carriers. Historically, revenues were divided as follows: 16% to cities, 24% to counties, and 60% to ODOT. HB 2001, approved during the 2009 legislative session, increased the gas tax by 6 cents. Revenue from the new taxes is divided as follows: 20% to cities, 30% to counties, and 50% to ODOT. In addition, new revenues were generated as part of the Oregon Transportation Investment Act (OTIA) – OTIA I, OTIA II, and OTIA III.

Revenue sharing from these programs falls somewhere between the old formula and the HB 2001 formula. County shares of the Fund are based on the number of vehicle registrations, while the allocations to the cities are based on population. ORS 366.514 requires at least 1% of the Highway Fund received by ODOT, counties, and cities be spent on the development of footpaths and bikeways. ODOT administers its bicycle/pedestrian funds, handles bikeway planning, design, engineering and construction, and provides technical assistance and advice to local governments concerning bikeways.

SPECIAL PUBLIC WORKS FUND (SPWF)

The State of Oregon allocates a portion of state lottery revenues for economic development. The Oregon Economic Development Department provides grants and loans through the SPWF program to construct, improve and repair infrastructure in commercial/industrial areas to support local economic development and create new jobs. The SPWF provides a maximum grant of \$500,000 for projects that will help create or retain a specified number of jobs. Loans for development (construction) projects range from less than \$100,000 to \$10 million. The Infrastructure Finance Authority offers very attractive interest rates that reflect tax-exempt market rates for highly qualified borrowers. Initial loan terms can be up to 25 years or the useful life of the project, whichever is less. Loans also are available to plan a construction project. SPWF projects will be programmed as awards are made.

IMMEDIATE OPPORTUNITY FUND (IOF)

The Immediate Opportunity Fund is intended to support economic development in Oregon by providing road improvements where they will assure job development opportunities by influencing the location or retention of a firm or economic development. The fund may be used only when other sources of funding are unavailable or insufficient, and is restricted to job retention and committed job creation opportunities. To be eligible, a project must require an immediate commitment of road construction funds to address an actual transportation problem. The applicant must show that the location decision of a firm or development depends on those transportation improvements, and the jobs created by the development must be “primary” jobs such as manufacturing, distribution, or service jobs.

TRAFFIC CONTROL PROJECTS

The state maintains a policy of sharing installation, maintenance, and operational costs for traffic signals and luminar units at intersections between state highways and city streets or county roads. Intersections involving a state highway and a city street (or county road), which are included on the statewide priority list are eligible to participate in the cost sharing policy. ODOT establishes a statewide priority list for traffic signal installations on the State Highway System. The priority system is based on warrants outlined in the Manual for Uniform Traffic Control Devices (MUTCD). Local agencies are responsible for coordinating the statewide signal priority list with local road requirements.

STATE SPECIAL TRANSPORTATION FUND (STF)

ODOTs Public Transit section administers a discretionary grant program derived from state cigarette tax revenues and a percentage of revenues of from sales of photo ID cards

that provides supplementary support for transportation services for the elderly and people with disabilities. A competitive process has been established for awarding STF funds. STF funds are programmed on a bi-annual basis.

LOCAL FUNDING PROGRAMS

SYSTEMS DEVELOPMENT CHARGES (SDCs)

Systems Development Charges are fees paid by land developers intended to reflect the increased capital costs incurred by a jurisdiction or utility as a result of a development. Development charges are calculated to include the costs of impacts on adjacent areas or services, such as parks and recreation use or traffic congestion. The SDC typically varies by the type of development (residential, commercial, industrial, etc.). Transportation SDCs are collected by the City of Bend and Deschutes County.

FRANCHISE FEES

Cities may collect franchise fees from local utility companies that utilize public right-of-ways for the conveyance of their services. The City of Bend currently collects franchise fees from Pacific Power and Light, Central Electric Co-op, Cascade Natural Gas Company, Bend Cable Communications and Century Link. A percentage of the funds derived from the franchise fees are allocated for maintenance and street improvement needs.

DEVELOPER EXACTIONS

Prior to the establishment of transportation SDCs, the City of Bend used a different method of exacting transportation system improvements. Development financial requirements have continued since the adoption of the transportation SDCs where transportation impacts have exceeded possible development related fee collection, but these “contributions” have lessened considerably since the adoption of the transportation SDC. These exactions, typically for qualified transportation improvements, are eligible for SDC reimbursement. Additionally, developers are required, without reimbursement, to build the local streets serving their developments.

STREET UTILITY FEES

In some cities, residents pay water and sewer utility fees. Street utility fees apply the same concepts to city streets. A fee is assessed to all businesses and households in the city for use of streets based on the amount of traffic typically generated by a particular use. The fees are typically used to pay for maintenance projects. Street utility fees are not currently collected within the BMPO area.

SPECIAL ASSESSMENTS/LOCAL IMPROVEMENT DISTRICTS (LIDS)

Special assessments are charges levied on property owners for neighborhood public facilities and services, with each property assessed a portion of total project cost. They are commonly used for such public works projects as street paving, drainage, parking facilities and sewer lines. The justification for such levies is that many of these public works activities provide services to or directly enhance the value of nearby land, thereby providing direct financial benefits to its owners. Local Improvement Districts are legal

entities established by local government to levy special assessments designed to fund improvements that have local benefits. Through an LID, streets or other transportation improvements are constructed and a fee is assessed to adjacent property owners. LIDs have been used within the BMPO area.

LOCAL PARKING FEES

Parking fees are a common means of generating revenue for public parking maintenance and development. Most cities have some public parking and many charge nominal fees for use of public parking. Cities also generate revenues from parking citations. These fees are generally used for parking-related maintenance and improvements. Parking fees are collected on a limited basis in the BMPO area.

REVENUE BONDS

Revenue bonds are financed by user charges, such as service charges, tolls, admissions fees and rents. If revenues from user charges are not sufficient to meet the debt service payments, the bond issuer generally is not legally obligated to levy taxes to avoid default, unless they are also backed by the full faith and credit of the issuing governmental unit. In that case, they are called *Indirect General Obligation Bonds*. Revenue bonds can be secured by a local gas tax, street utility fee or other transportation-related stable revenue stream. Revenue bonds were issued for 2 projects in Bend in approximately year 2000. The City has several years remaining to fully pay those bonds. The bonds are paid using SDC revenues.

GENERAL OBLIGATION BONDS

All taxpayers of the issuing governmental unit, which must pay the interest and principal on the debt as they come due, finance general Obligation (GO) bonds. *Municipal bonds* are GO bonds issued by a local governmental subdivision, such as a city, and are secured by the full faith and credit of the issuing municipality. Oregon law requires GO bonds to be authorized by popular vote. General obligation bonds are currently being used to finance transportation projects within the BMPO area. See the City of Bend website for details on the bond projects.

PROPERTY TAXES

Local property taxes are used to fund various transportation services, including roadway projects and Cascades East Transit services.

FAREBOX REVENUE

A percentage of the Cascades East Transit operating funds are received from farebox revenues.

TRANSIENT ROOM TAX (TRT)

The City currently levies a tax on gross room receipts on Hotel/Motel rooms in the City. A percentage of the revenues could be used to fund transportation services.

LOCAL GAS TAX

A local gas tax is assessed at the pump and added to existing state and federal taxes. A local gas tax has not been implemented in the BMPO area.

VEHICLE REGISTRATION FEES

In Oregon, counties may implement a local vehicle registration fee. The fee would be similar to the state vehicle registration fee. A portion of a county's fee could be allocated to local jurisdictions. Deschutes County does not currently have a vehicle registration fee.

URBAN RENEWAL FUNDING

Urban renewal, or tax increment financing, has been a financing tool that has been used by the City of Bend to improve certain areas of the community. Urban renewal areas have been established in NE Bend and in southern Bend. It is anticipated that a percentage of revenues generated from these districts will be dedicated to transportation improvements in the respective taxing areas.

PUBLIC/PRIVATE PARTNERSHIPS:

The City and an alliance of local developers and property owners (*The West Side Traffic Consortium*) negotiated an agreement that provides for the construction of several significant transportation improvements. These were/are funded through a combination of developer provided improvements and those provided through local improvement district financing. Agreements such as this may serve as a model for future similar such agreements when the cost and timing of various needed improvements exceed the financial ability of any one entity including the City.

SPECIAL ROAD DISTRICTS

Special road districts provide a means for funding specific improvements that benefit a specific group of property owners (comparable to a Local Improvement District). These districts require owner approval and a specific project definition. The residents forming the district agree to pay property taxes to support the special district. Special road districts exist throughout unincorporated Deschutes County. One special road district exists within the BMPO area. Road District Commissioners are appointed by the Deschutes County Board of Commissioners to operate the district.

LOCAL JURISDICTION FUNDING

DESCHUTES COUNTY

Deschutes County's primary source of transportation revenue is shared revenue from the Oregon Highway Fund. The County's revenues are grouped into one large fund known as the Road Fund for purposes of developing the road department budget and capital improvement program.

CITY OF BEND

The city of Bend receives transportation revenues from many sources including: Oregon gas tax and vehicle registration revenues; systems development charges; and franchise

fees. These revenues are used for local projects and also provide the necessary match for federally funded projects.

CASCADES EAST TRANSIT

Cascades East Transit receives revenues from the City of Bend general fund, farebox revenues, advertising revenues, the Federal Transit Administration, and the Oregon Department of Transportation.

Appendix E: Transportation Planning Acronyms and Terms

ACT:	Area Commission on Transportation (see COACT)
ADA:	Americans with Disabilities Act
ADT:	Average Daily Traffic
CBD:	Central Business District
CFR:	Code of Federal Regulations
COACT:	Central Oregon Area Commission on Transportation
DLCD:	Department of Land Conservation and Development
EMME:	Computerized Transportation Modeling Software
FAST:	Fixing America's Surface Transportation (FAST) Act (federal surface transportation legislation)
FFO:	The Federal Hwy Administration will conduct in-depth reviews of the methods & procedures used during development & construction of the selected transportation projects. To clearly identify the projects selected for Full Federal Oversight, it was proposed to add the letters "FFO" to the beginning of each project's name. The US97: Bend North Corridor Project was one of the projects in Region 4 identified for Full Federal Oversight (FFO) review.
FFY:	Federal Fiscal Year: from October 1 to September 31.
FHWA:	Federal Highway Administration
FTA:	Federal Transit Administration
FY:	Fiscal Year: (Oregon state fiscal year from July 1 to June 30)
GIS:	Geographic Information Systems
HPMS:	Highway Performance Monitoring System
ITS:	Intelligent Transportation Systems
LOS:	Level of Service, a measure of traffic congestion from A (free-flow) to F (grid-lock)
MAP-21:	Moving Ahead for Progress in the 21 st Century (federal surface transportation legislation)
MOU:	Memorandum of Understanding
MPO:	Metropolitan Planning Organization, a planning body in an urbanized area over 50,000 population which has responsibility for developing transportation plans for that area
MTP:	Metropolitan Transportation Plan
MTIP:	Metropolitan Transportation Improvement Program (same as TIP)
NHS:	National Highway System
OAR:	Oregon Administrative Rules
ODOT:	Oregon Department of Transportation
OHP:	Oregon Highway Plan
ORS:	Oregon Revised Statutes
OTC:	Oregon Transportation Commission, ODOT's governing body
OTP:	Oregon Transportation Plan
PL Funds:	Public Law 112, Federal Planning Funds
SOV:	Single Occupancy Vehicle
STBG:	Surface Transportation Block Grant Program

STIP:	Statewide Transportation Improvement Program
STP:	Surface Transportation Program
TAC:	Technical Advisory Committee
TAZ:	Transportation Analysis Zone
TDM:	Transportation Demand Management
TIP:	Transportation Improvement Program (same as MTIP)
TOD:	Transit Oriented Development
TPAU:	Transportation Planning Analysis Unit (ODOT)
TPR:	Transportation Planning Rule
TSM:	Transportation Systems Management
TSP:	Transportation System Plan
UGB:	Urban Growth Boundary
UPWP:	Unified Planning Work Program
USDOT:	U.S. Department of Transportation
VMR:	Vehicle Miles Reduced
VMT:	Vehicle Miles of Travel

Allocation - An administrative distribution of funds for programs that do not have statutory distribution formulas.

Appropriation - Legislation that allocates budgeted funds from general revenues to programs that have been previously authorized by other legislation. The amount of money appropriated may be less than the amount authorized.

Appropriations Act - Action of a legislative body that makes funds available for expenditure with specific limitations as to amount, purpose, and duration. In most cases, it permits money previously authorized to be obligated and payments made, but for the highway program operating under contract authority, the appropriations act specifies amounts of funds that Congress will make available for the fiscal year to liquidate obligations.

Apportionment - A term that refers to a statutorily prescribed division or assignment of funds. An apportionment is based on prescribed formulas in the law and consists of dividing authorized obligation authority for a specific program among the States. It also refers to the distribution of funds as prescribed by a statutory formula.

Authorization - Federal legislation that creates the policy and structure of a program including formulas and guidelines for awarding funds. Authorizing legislation may set an upper limit on program spending or may be open ended. General revenue funds to be spent under an authorization must be appropriated by separate legislation.

Capital Costs - Non-recurring or infrequently recurring cost of long-term assets, such as land, buildings, vehicles, and stations.

Federal-aid Highways - Those highways eligible for assistance under Title 23 of the United States Code, as amended, except those functionally classified as local or rural minor collectors.

Functional Classification - The grouping of streets and highways into classes, or systems according to the character of service that they are intended to provide, e.g., residential, collector, arterial, etc.

Key Number - Unique number assigned by ODOT to identify projects in the MTIP/STIP.

Maintenance - Activities that preserve the function of the existing transportation system.

Regionally Significant – From OAR 340-252-0030 (39) "Regionally significant project" means a transportation project, other than an exempt project, that is on a facility which serves regional transportation needs, such as access to and from the area outside the region, major activity centers in the region, major planned developments such as new retail malls, sports complexes, etc., or transportation terminals as well as most terminals themselves, and would normally be included in the modeling of a metropolitan area's transportation network.

In the Bend metropolitan area, regionally significant facilities will include, at a minimum:

- All state highways;
- All arterial roadways; and
- Any other facilities determined to be regionally significant by the Policy Board (in consultation with the Technical Advisory Committee).

3C - "Three C's" = continuing, comprehensive, and cooperative - This term refers to the requirements set forth in the Federal Highway Act of 1962 that transportation projects in urbanized areas be based on a "continuing, comprehensive transportation planning process carried out cooperatively by states and local communities." ISTEA's planning requirements broadened the framework for such a process to include consideration of important social, environmental and energy goals, and to involve the public in the process at several key decision making points.

Appendix F: Summary of Comments and Responses

This section will be updated as comments are received during the comment period.

Appendix F lists comments received on the draft MTIP. The comments are listed by agency or stakeholder. BMPO response is listed in parentheses below each specific comment.

PUBLIC COMMENTS

- None

TECHNICAL ADVISORY COMMITTEE COMMENTS

- Formatting and grammatical corrections (Corrections made)
- Presentation made to Technical Advisory Committee on May 3, 2017

POLICY BOARD COMMENTS

- Presented draft document at April 20, 2017 Policy Board Meeting

ODOT COMMENTS

- Technical corrections to project tables (Corrections made)

FHWA COMMENTS

- None

FTA COMMENTS

- None

A copy of the public notice is included for reference. Posted online on 1/27/17.

Bend MPO Public Comment

Transportation Improvement Program

Post Date: 01/27/2017 4:56 PM

MTIP for 2018-2021 for your review and comment.



The BMPO Metropolitan Transportation Improvement Program (MTIP) identifies transportation projects in the BMPO study area that are scheduled in federal fiscal years 2018-2021. The period begins on October 1, 2017, which is the beginning of the 2018 federal fiscal year, and ends September 30, 2021, which is the end of the 2021 federal fiscal year. The MTIP lists most federally funded and some locally funded projects anticipated by local agencies and the Oregon Department of Transportation (ODOT).

The MTIP contains a four-year listing of anticipated expenditures for locally funded projects drawn from the capital improvement programs of Bend and Deschutes County. It also lists projects from the ODOT Statewide Transportation Improvement Program (STIP). The MTIP lists projects for which application of specific federal funds will be made during the programming period. Projects in the MTIP must be consistent with the Metropolitan Transportation Plan.

By adopting the MTIP, the Policy Board has selected the projects identified in Table 3, *Program of FFY 2018-2021 Transportation Projects*, for implementation and funding as scheduled. No additional action by the Policy Board is required for funding these projects. If additional funds become available or if a project experiences an unexpected delay, the Policy Board may select other projects from the first three years of the schedule to take advantage of the additional funds or to replace a delayed project.

Please [contact us](#) for any inquiries, comments or concerns.

Public comment or testimony is also available during the regular meeting of the [Bend MPO Policy Board](#) held on February 16, 2017 at 4:00PM in the Deschutes Services Building at 1300 NW Wall Street Bend, Oregon .

Appendix G: ODOT-FTA-FHWA AMENDMENT MATRIX

This matrix is for information only and was provided by ODOT to help Metropolitan Planning Organizations to determine when an amendment to the Statewide Transportation Improvement Program would be a full or administrative amendment. Table 1 in this document identifies how the Bend MPO would process an amendment to the MTIP.

AMENDMENTS TO THE STIP	
Type of Change	
FULL AMENDMENTS	
1. Adding or cancelling a federally funded, and regionally significant project to the STIP and state funded projects which will potentially be federalized	
2. Major change in project scope. Major scope change includes: <ul style="list-style-type: none"> • Change in project termini - greater than .25 mile in any direction • Changes to the approved environmental footprint • Impacts to AQ conformity • Adding capacity per FHWA Standards • Adding or deleting worktype 	
3. Changes in Fiscal Constraint by the following criteria: <ul style="list-style-type: none"> • FHWA project cost increase/decrease: <ul style="list-style-type: none"> • Projects under \$500K – increase/decrease over 50% • Projects \$500K to \$1M – increase/decrease over 30% • Projects \$1M and over – increase/decrease over 20% • All FTA project changes – increase/decrease over 30% 	
4. Adding an emergency relief permanent repair project that involves substantial change in function and location.	
ADMINISTRATIVE/TECHNICAL ADJUSTMENTS	
1. Advancing or Slipping an approved project/phase within the current STIP (If slipping outside current STIP, see Full Amendments #2)	
2. Adding or deleting any phase (except CN) of an approved project below Full Amendment #3	
3. Combining two or more approved projects into one or splitting an approved project into two or more, or splitting part of an approved project to a new one.	
4. Splitting a new project out of an approved program-specific pool of funds (but not reserves for future projects) or adding funds to an existing project from a bucket or reserve if the project was selected through a specific process (i.e. ARTS, Local Bridge...)	
5. Minor technical corrections to make the printed STIP consistent with prior approvals, such as typos or missing data.	
6. Changing name of project due to change in scope, combining or splitting of projects, or to better conform to naming convention. (For major change in scope, see Full Amendments #2)	
7. Adding a temporary emergency repair and relief project that does not involve substantial change in function and location.	