



ORDINANCE NO. NS-2553

An ordinance of the City Council to amend Bend Development Code Article XXVIII, Timber Yards Master Planned Development.

Findings

- A. On December 11, 2025, the applicant, Kennedy Wilson, submitted a Quasi-judicial application for a Bend Development Code amendment to the Timber Yards Master Planned Development.
- B. On March 9, 2026, the Planning Commission held a public hearing and issued a recommendation that the City Council adopt an ordinance to amend Chapter 2.7, Article XXVIII of the Bend Development Code to amend the Timber Yards Master Plan, as proposed by the applicant. with one minor revision to the Transportation Mitigation Table.
- C. Public notice for the City Council hearing was provided in accordance with the requirements of Bend Development Code 4.1.423-4.1.425. On March 20, 2026, notice was mailed by the Planning Division to surrounding owners of record and addresses of property within 750 feet of the subject properties and to the designated representatives of the Old Bend, Larkspur and Southern Crossing Neighborhood Districts. A "Proposed Development" sign was also posted at each property frontage of the master plan, visible from the adjacent rights-of-way of Bond Street, Industrial Way, Aune Street and Scalehouse Loop.
- A. The Bend City Council held a public hearing on April 15, 2026, to consider the Planning Commission recommendation.
- D. The quasi-judicial Bend Development Code text amendments approved by this Ordinance meet all applicable Development Code criteria, policies of the Bend Comprehensive Plan and Oregon Statewide Planning Goals.

Ordinance

Based on these findings, the City of Bend ordains as follows:

- Section 1.** Article XXVIII of the Bend Development Code is amended as shown on the attached Exhibit A.



Section 2. In addition to the findings set forth above, the City Council adopts and incorporates the findings in Exhibit B.

Section 3. If any provision, section, phrase, or word of this ordinance or its application to any person or circumstance is held invalid, the invalidity does not affect other provisions that can be given effect without the invalid provision or application.

Section 4. All other provisions of the Bend Development Code remain unchanged by this ordinance and remain in effect.

First Reading

April 15, 2026

Second Reading

May 6, 2026

Adoption by Roll Call Vote

Yes: Kebler, Franzosa, Méndez, Norris, Perkins, Platt, Riley

No:

Melanie Kebler, Mayor

Attest:

Ashley Bontje, City Recorder

Approved as to form:

Ian Leitheiser, City Attorney

Exhibit A

Bend Development Code Update

File #: PLTEXT2025679

Note:

Text in underlined typeface is proposed to be added

Text in ~~striketrough~~ typeface is proposed to be deleted

***Indicates where text from the existing code has been omitted because it will remain unchanged.

Staff comments are ***bold and italicized***

Bend Development Code

Chapter 2.7

SPECIAL PLANNED DISTRICTS, REFINEMENT PLANS, AREA PLANS AND MASTER PLANS

ARTICLE XXVIII. Timber Yards Master Planned Development

2.7.4710 Purpose.

The purpose of the Timber Yards Master Planned Development is to implement the policies in the Bend Comprehensive Plan (BCP) regarding the KorPine Opportunity Area, and to create appropriate development standards for the residential and commercial uses within the Timber Yards Master Plan area. The development standards will:

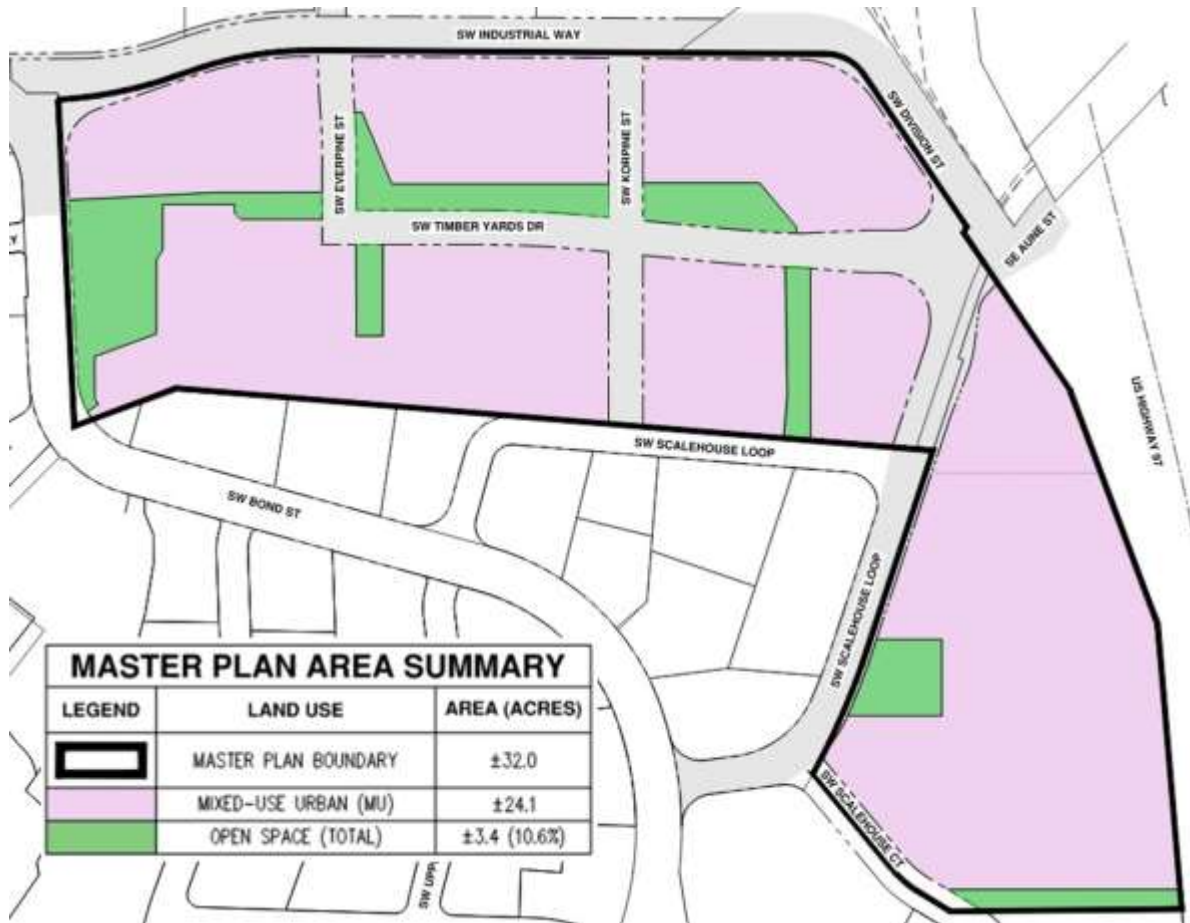
- A. Transform vacant land and a historically industrial area into a vibrant, urban, and mixed-use neighborhood with housing in close proximity to employment and commercial services, which are scaled to serve the needs of area residents and workers.
- B. Establish an interconnected street system consistent with the Transportation System Plan with cross-sections appropriate to the characteristics of the site and that consider adjacent land uses and regional connectivity needs.
- C. Create safe, attractive, and efficient pedestrian routes and other multi-modal transportation options.

2.7.4720 Applicability.

A. The Timber Yards Master Planned Development standards apply to the property identified in Figure 2.7.4720, which constitutes the majority of the Korpine Opportunity Area in BCP Chapter 11. The special standards of the Master Plan supersede the standards of the underlying zone and other applicable standards of the Bend Development Code. Where there is a conflict between the provisions of the Timber Yards Master Plan and those of the underlying zone or other portions of the Development Code, the provisions of this district will control. The final determination of the overlay zone or district boundary will be established at the time of land division platting and right-of-way dedication.

Figure 2.7.4720. Timber Yards Master Plan

(Delete the following figure)



(Add the following figure)



B. Open space locations in Figure 2.7.4720 are conceptual. Final location and acreage will be established at subdivision and/or site plan approval. The minimum acreage that must be platted with open space tracts is 3.2 acres.

2.7.4730 Review Procedures.

The following review procedures are applicable to uses and structures within the Timber Yards Master Planned Development:

A. **Site Plan Review.** Multi-unit residential, commercial, and mixed-use developments are subject to BDC 4.2.500, Site Plan Review.

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- B. **Design Review.** Multi-unit residential, commercial, and mixed-use developments are subject to BDC 4.2.600, Design Review, and the design standards in BDC 2.7.4750, Design Standards.

2.7.4740 Mixed Use Urban District.

- A. The standards of the underlying Mixed-Use Urban (MU) Zone apply, except all buildings can be no more than 75 feet in height.

- B. **Loading and On-Street Parking Standards.** On-site loading spaces are not required.

C. **Mobility Hub.**

1. The mobility hub within the Timber Yards Master Plan must include the following components:

- a. Transit stop.
- b. Flex mobility space(s) a minimum of 250 square feet.
- c. Secured bicycle parking for a minimum of 10 bicycles.
- d. Garbage and recycling cans.
- e. Benches.
- f. Kiosk, signage and wayfinding.

2. The mobility hub may also provide any of the optional facilities and services listed in the City standards for mobility hubs.

2.7.4750 Design Standards.

- A. Multi-unit residential, commercial, and mixed-use developments in Timber Yards are subject to the following design standards. These standards replace design standards of BDC 2.3.500, including the standards in BDC 2.1.900, Architectural Design Standards, and BDC 2.2.600, Commercial Design Review Standards.

1. **Building and Entry Orientation.**

- a. **Primary Entrance.** Each building must provide a primary building entrance oriented to at least

one street or to a street-oriented plaza.

- b. Building facades located within 10 feet of SW Bond Street must include a minimum of one entrance every 60 feet onto SW Bond Street.

2. *Facade Height Minimum.*

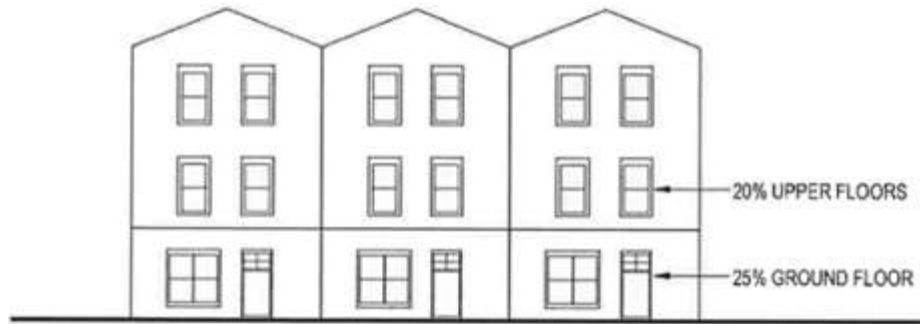
- a. For buildings located on SW Bond Street, the minimum height for the facade(s) facing SW Bond Street is 20 feet.
- b. The minimum height for facades facing other streets is 15 feet.

3. *Transparency.*

- a. Facades that face SW Bond Street must include transparency/glazing (i.e., glass windows and/or glass doors) totaling a minimum of 40 percent of the ground level wall area and 20 percent on the upper level(s) wall area, as shown in Figure 2.7.4750.A. Ground level wall area includes the exterior wall area up to 10 feet above the finished grade.
- b. Facades that face other streets must include transparency/glazing (i.e., glass windows and/or glass doors) totaling a minimum of 25 percent of the ground level wall area and 20 percent on the upper level(s) wall area, as shown in Figure 2.7.4750.B. Ground level wall area includes the exterior wall area up to 10 feet above the finished grade.

Figure 2.7.4750.A. SW Bond Street Facade



Figure 2.7.4750.B. Other Street Facade

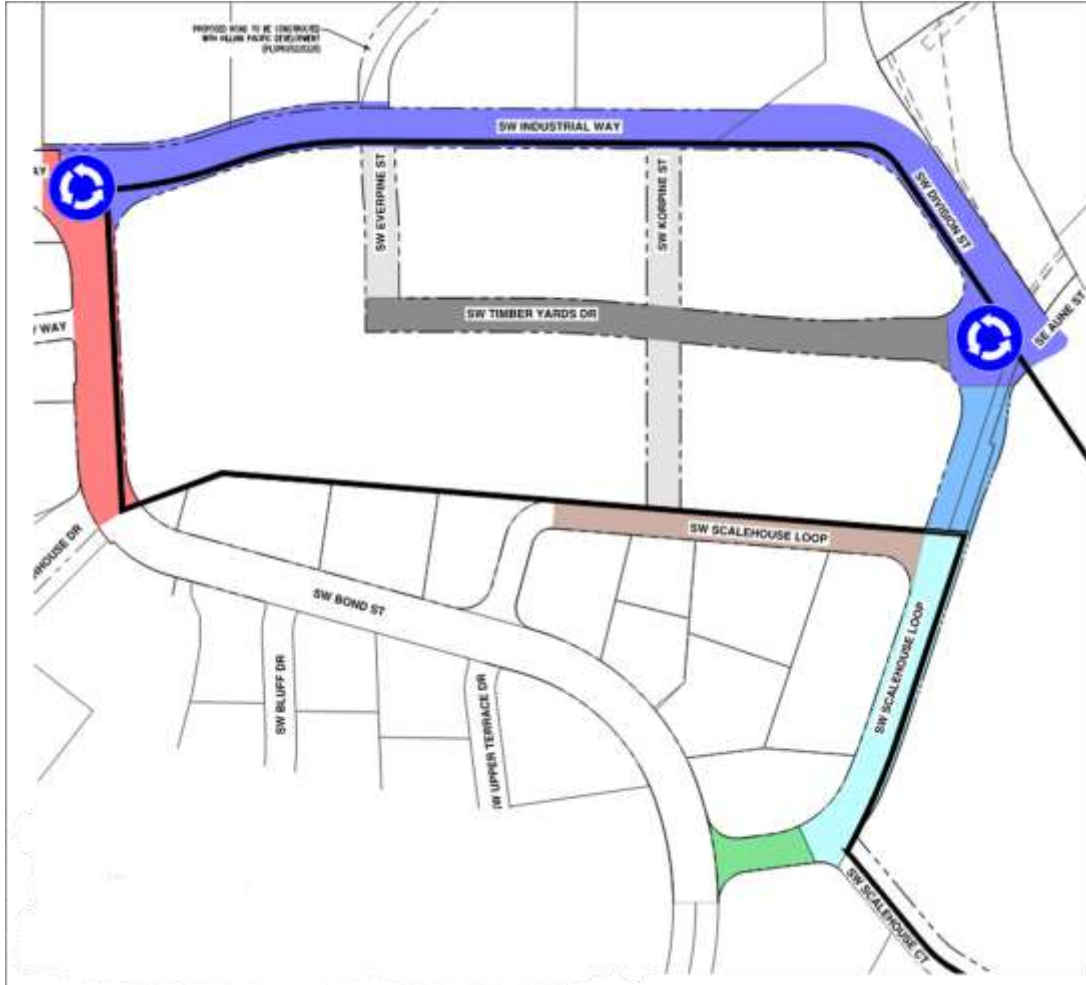
4. **Building Articulation.** One or more of the following facade treatments must be incorporated on each level of all street-facing facades:
 - a. Building offsets (projections or recesses) at least two feet deep and six feet wide;
 - b. Upper level balconies (projecting or recessed) at least six feet wide;
 - c. Building canopies, awnings, pergolas, architectural sun shade structures, or similar weather protection (minimum projection of four feet for overhead weather protection when located on the ground level and over a sidewalk or other pedestrian space);
 - d. Transparency greater than the minimum required by subsection (A)(3) of this section;
 - e. Recessed building entries at least six feet wide;
 - f. Use of masonry material (i.e., brick, brick veneer, stone, concrete, or stucco) for a minimum of 75 percent of the total wall area of the street-facing facade level as measured from floor-to-floor height, not including windows; and/or
 - g. On upper levels, use of two or more facade materials and/or facade colors.
5. ~~Exterior colors shall be of low reflectance, subtle, neutral, or earth tone colors. The use of high intensity, neon, glossy, or fluorescent colors for the facade and/or roof of the building is prohibited except as approved for building trim that does not exceed 25 percent of each facade.~~

2.7.4750.C**Illustration of Use of Architectural Features****2.7.4760 Special Street Standards.**

- A. Figure 2.7.4760 depicts the street type, tentative street location and alignment in the Timber Yards Master Planned Development. The precise street alignment will be established through the land division process. The Timber Yards Master Planned Development Street Type Plan, Figure 2.7.4760, applies to the Timber Yards Master Planned Development as illustrated except when an alternate standard is permitted under this section or through the tentative plan approval process. The travel lanes for local streets may be increased to 13 feet wide, as required to meet applicable fire codes.
- B. Block length and perimeter maximums do not apply to streets located or aligned in general conformance with Figure 2.7.4760, Timber Yards Street and Circulation Plan.
- C. Any City street standard adopted after the effective date of the ordinance codified in this chapter, which permits a lesser street standard, may be applied to the Timber Yards Master Planned Development during the land division or site plan review process.

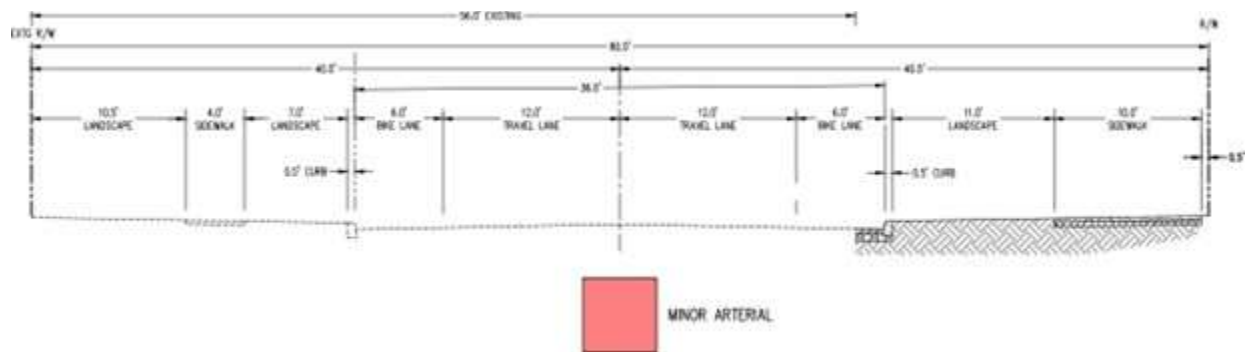
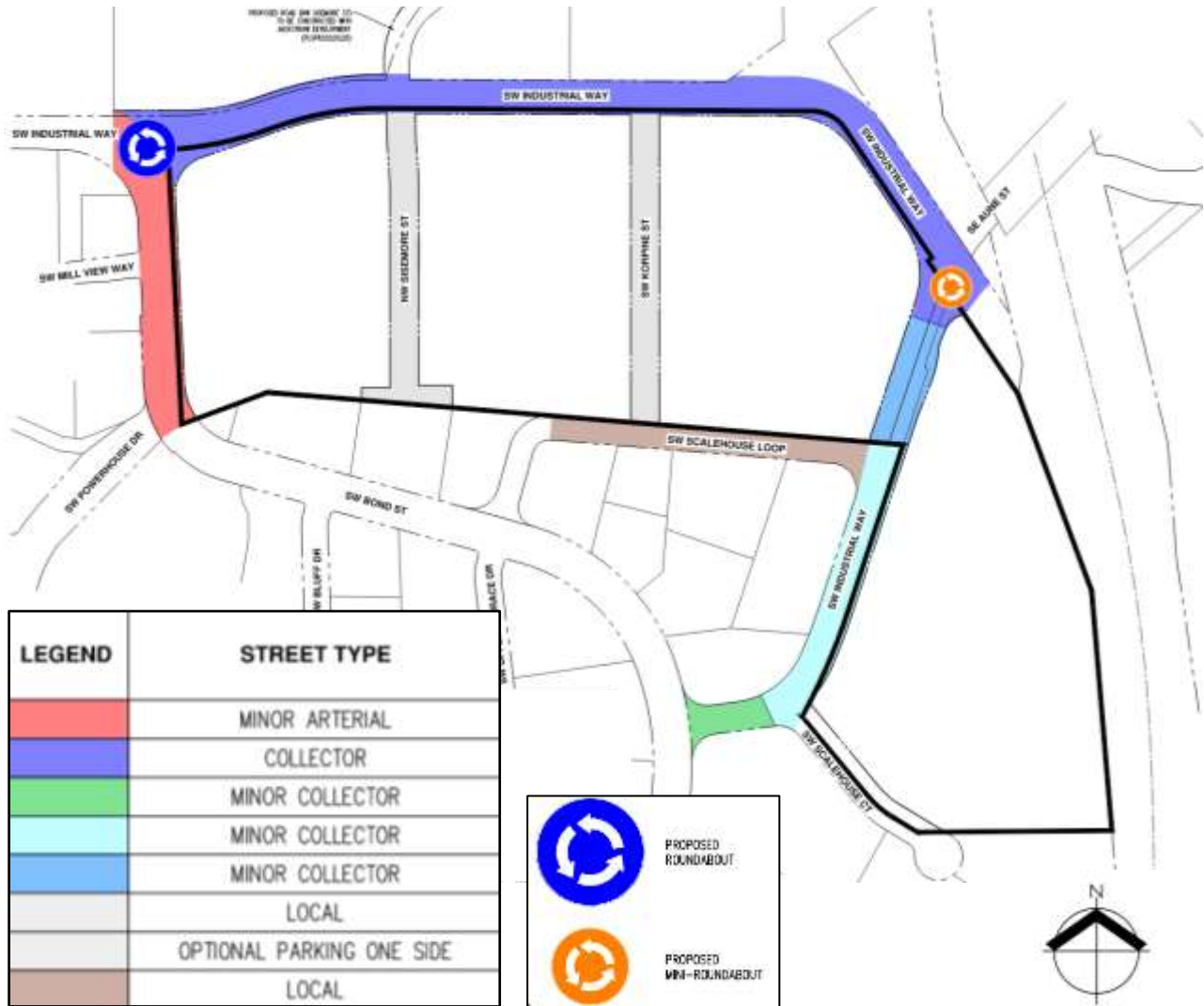
Figure 2.7.4760. Timber Yards Street and Circulation Plan

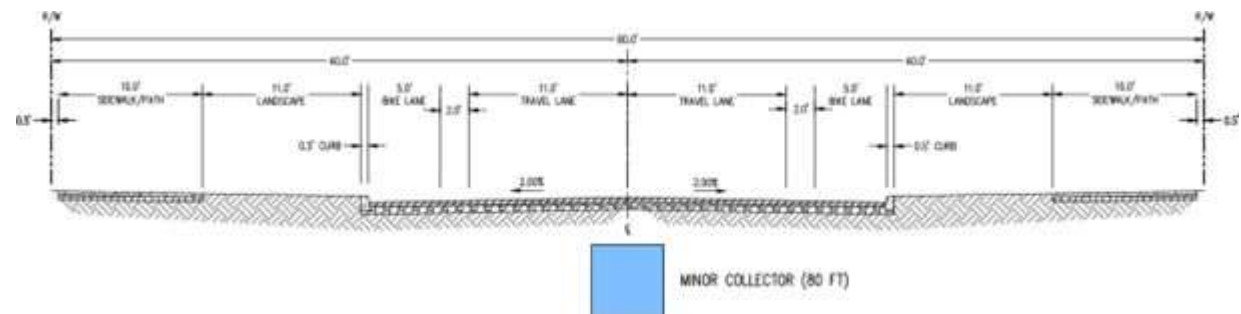
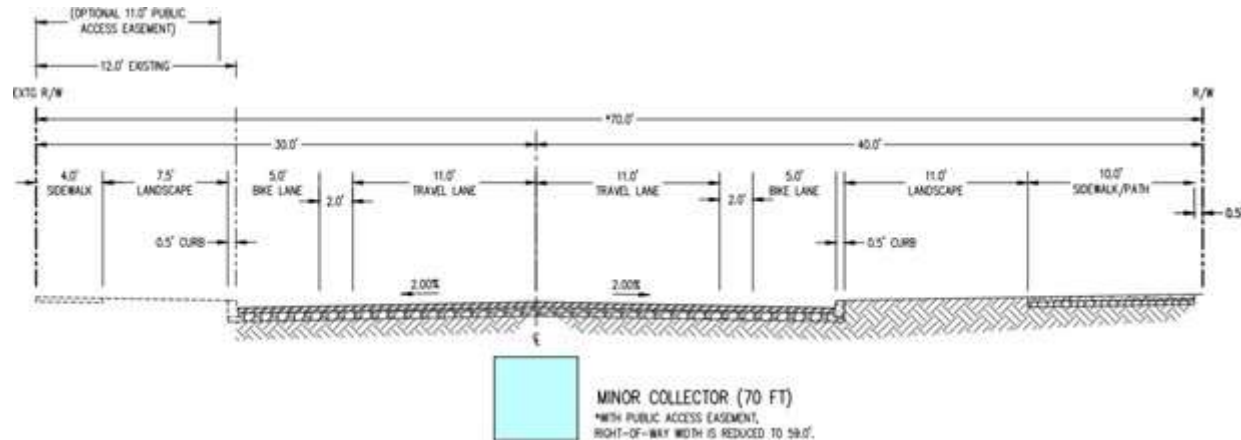
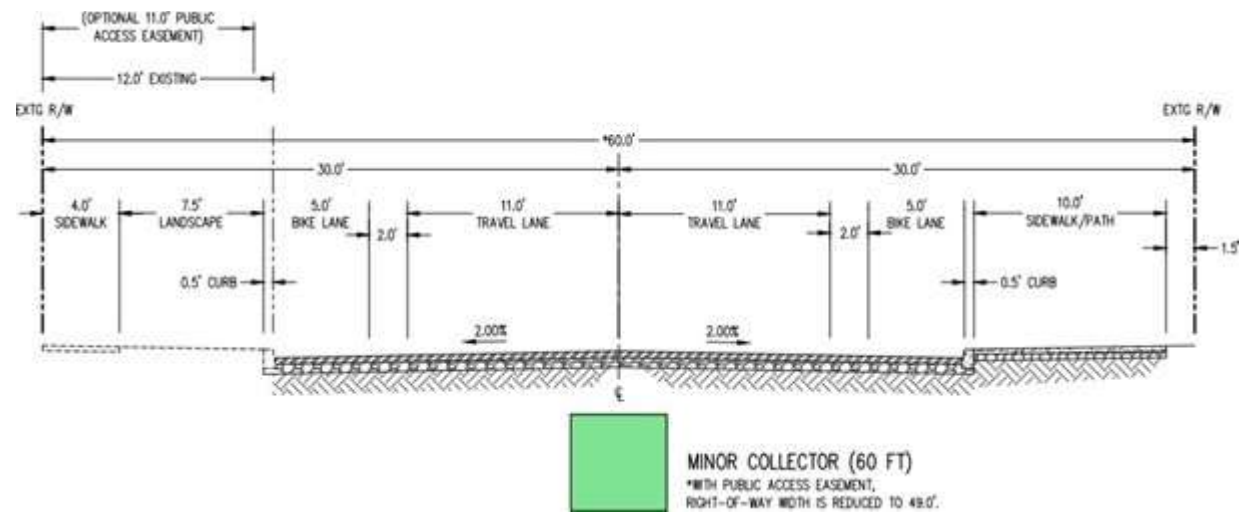
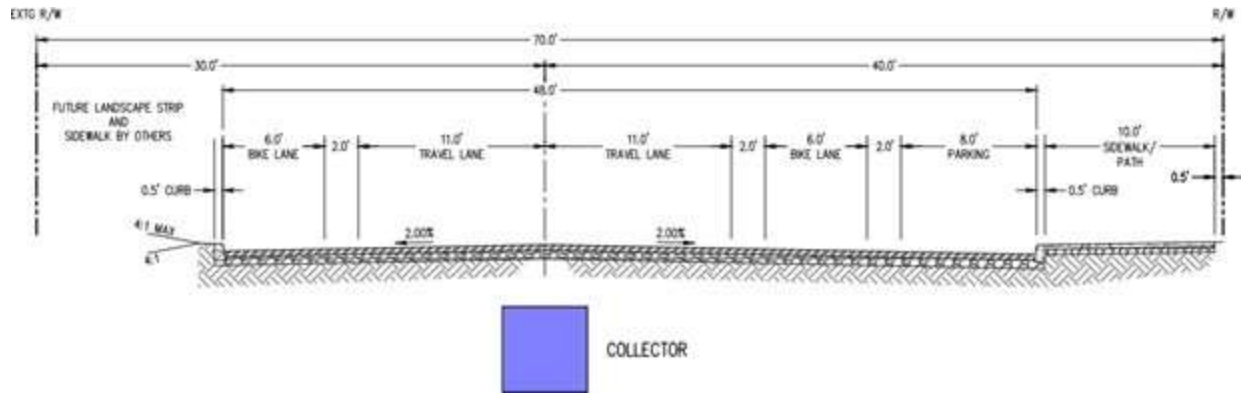
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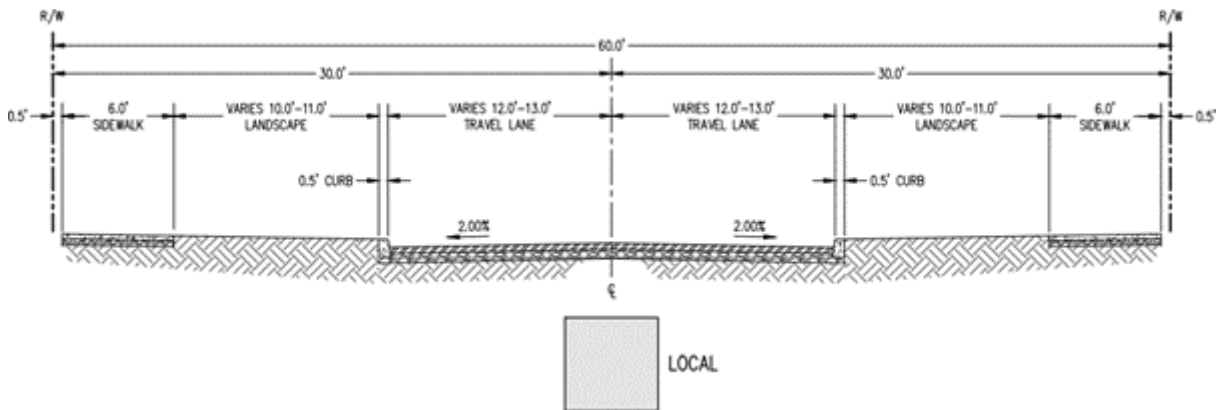
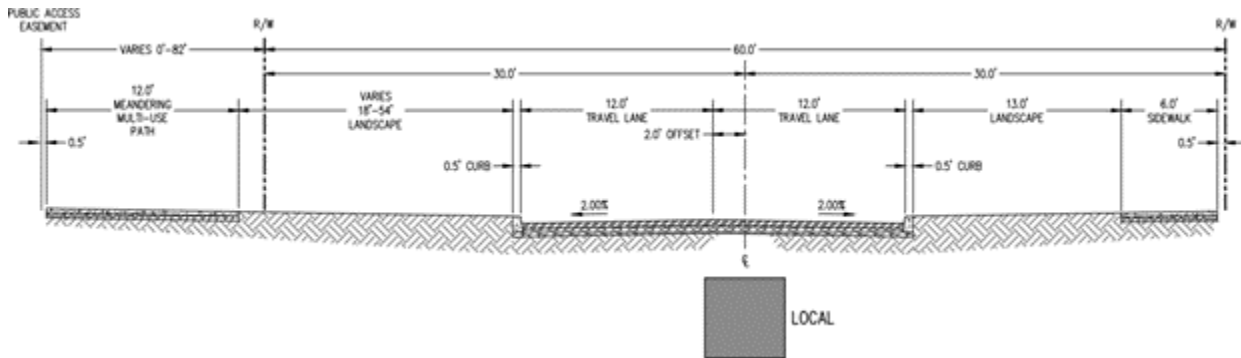
LEGEND	STREET TYPE
	MINOR ARTERIAL
	COLLECTOR
	MINOR COLLECTOR
	MINOR COLLECTOR
	MINOR COLLECTOR
	LOCAL
	OPTIONAL PARKING BOTH SIDES
	LOCAL
	OPTIONAL PARKING ONE SIDE
	LOCAL

(Add the following figure)

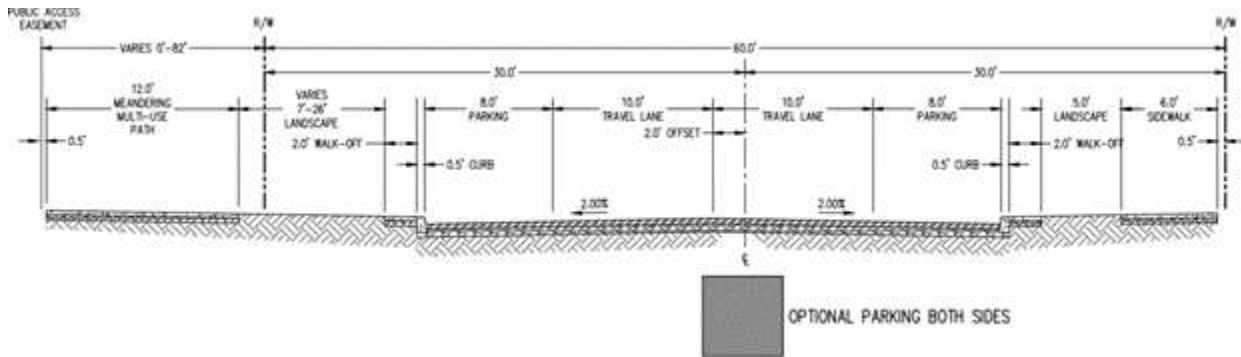


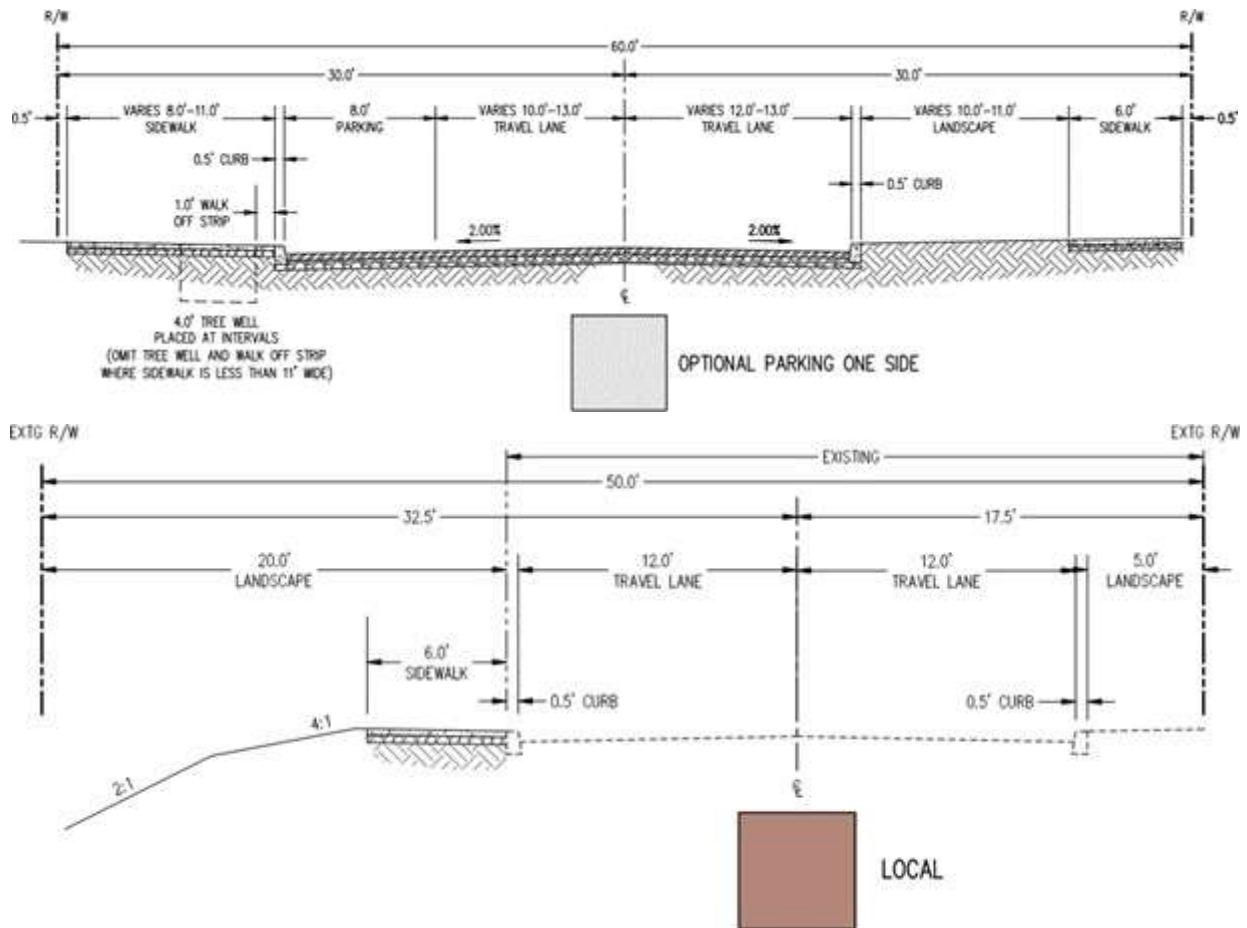


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2.7.4770 Transportation Mitigation Plan.

- A. **Applicability.** The following Transportation Mitigation Plan applies to all development within the Timber Yards Master Planned District.
- B. **Alternate Transportation Design Standards.** Transportation facilities within the Timber Yards Master Planned District must comply with the standards set forth in BDC 2.7.4760 and Figure 2.7.4760.
- C. **Transportation Mitigation.** The following transportation mitigation measures must be constructed pursuant to the schedule set forth in Table 2.7.4770. The timing of the mitigation measures is tied to the phases depicted on Figure 2.7.4770; the phasing does not need to occur in any specific order.

Table 2.7.4770 – Transportation Mitigation

Mitigation Requirement	Trigger
Widen Sisemore Street pavement width to provide bicycle lanes and widened sidewalks from Colorado Avenue to Arizona Avenue within existing ROW and construct a safety improvement at the intersection of Colorado Avenue and Sisemore Street.	Prior to Occupancy of first building in any phase after Sisemore Street between Industrial Way and Arizona Avenue is fully constructed by others
Southbound Hwy 97 Colorado intersection curb returns and accessible curb ramps must be constructed. The curb return radii must conform to City of Bend standards unless otherwise approved by the City Engineer during right of way permit review. Changes to the ramp radii may require traffic signal modification to the push buttons and a review of the signal phasing such as a southbound right turn overlap phasing requiring additional	Prior to Occupancy of the first building in any Phase concurrent with the

Mitigation Requirement	Trigger
<p>vehicle indication. Developer may elect to utilize payment in lieu of fee per 4.7.600F(2)(c), estimated under the proportionate share calculation in BDC 4.7.700 at \$300,000 in 2023.</p>	<p>improvement of Industrial Way to Division Street</p>
<p>Dedication of right-of-way (ROW) and completion of the Bond Street/Industrial Way single-lane asphalt roundabout adjacent to Phase A. Construction of the roundabout will require continued access to impacted uses and development. The implementation of a detour plan will be required.</p> <p>Construction of an access road from the sewer pump station (south of Phase B or Phase D) along the sewer alignment and to the sewer outfall in Aune Street must be constructed.</p> <p>Design and reconstruction of Aune Street to existing conditions within the available ROW under Hwy 97 undercrossing to 2nd Street over the required water and sewer main installation with the 1st phase of development. Developer to coordinate detour plans for review and approval by the City of Bend describing how it will maintain public and emergency ingress/egress for impacted businesses during construction of area infrastructure improvements.</p> <p>As deemed practicable by the City of Bend, detour plans will endeavor to minimize impacts to existing businesses fronting Industrial Way and Division Street.</p> <p>The existing gate on Industrial Way (fronting Phase E) must have a revocable agreement recorded against the property prior to permit issuance. With no revocable agreement, the gate must be removed.</p> <p>Each phase of development review will provide a trip debit letter that will track available trips banked by the Transportation Review. Trips in excess of the Traffic Impact Analysis will require an additional transportation review in accordance with BDC Chapter 4.7.</p>	<p>Prior to Occupancy of the first building in any Phase</p>

Mitigation Requirement	Trigger
<p>Industrial Way – ROW dedication required to obtain 40 feet ROW width from ROW centerline to frontage property line and completion of ¾ street improvements from Bond Street through property frontage – full asphalt street bound by curb on both sides and a wide sidewalk along the frontage.</p> <p>Bond Street – ROW dedication of 40 feet from ROW centerline to property line and completion of a multi-use path along Bond Street frontage to Powerhouse Drive.</p> <p>Provision of an enhanced pedestrian crossing at Bond Street and Powerhouse Drive intersection. The City may allow the limited deferral of the crossing with an acceptable agreement in place between Timber Yards and the Old Mill to construct a broader intersection improvement that includes an equivalent enhanced pedestrian crossing.</p> <p>Everpine Street – Dedication of ROW and completion of ¾ street improvements – full asphalt street bound by curb on both sides and sidewalk along the property frontage.</p> <p>Dedication of public access easement for non-vehicular and emergency access route from Everpine Street west to Bond Street, gated (or similar vehicular restriction) at Bond Street, aligning with Mill View Way (south of Phase A frontage).</p> <p>Construction of Mobility Hub along SW Bond Street frontage.</p>	<p>Prior to Occupancy of the first building in Phase A</p>
<p>Bond Street – ROW dedication required to obtain 40 feet ROW width from ROW centerline to frontage property line and completion of a multi-use path along Bond Street frontage to Powerhouse Drive.</p> <p>Industrial Way – ROW dedication required to obtain 40 feet ROW width from ROW centerline to frontage property line and completion of ¾ collector street improvements from Bond Street through Parcel A property frontage – full asphalt street bound by curb on both sides and a multi-use path along the frontage.</p> <p>Everpine Street (north) – Dedication of ROW and completion of ¾ local street improvements from Industrial Way to the property – full asphalt street bound by curb on both sides and sidewalk along the property frontage.</p>	<p>Prior to Occupancy of the first building in Phase B</p>

Mitigation Requirement	Trigger
<p>Everpine Street (south) – Dedication of public access easement and completion of private street to local street standards from Timber Yards Drive south to a turnaround used for site access and public sewer pump station maintenance – full asphalt street bound by curb on both sides and sidewalk.</p> <p>Provision of an enhanced pedestrian crossing at Bond Street and Powerhouse Drive intersection. The City may allow the limited deferral of the crossing with an acceptable agreement in place between Timber Yards and the Old Mill to construct a broader intersection improvement that includes an equivalent enhanced pedestrian crossing.</p> <p>Construction of Mobility Hub along SW Bond Street frontage.</p> <p>Dedication of public access easement for non-vehicular and emergency access route from Everpine Street west to Bond Street, gated at Bond Street, aligning with Mill View Way (north of Phase B frontage).</p>	
<p>Industrial Way – ROW dedication required to obtain 40 feet ROW width from ROW centerline to frontage property line and completion of ¾ collector street improvements from Bond Street through property frontage – full asphalt street bound by curb on both sides and a wide sidewalk along the frontage.</p> <p>Everpine Street (north), Timber Yards Drive and Korpine Street – Dedication of ROW and completion of ¾ local street improvements – full asphalt street bound by curb on both sides and sidewalk along the property frontage.</p> <p>If constructed prior to Phase D, Korpine Street must be constructed between the intersection of Timber Yards Drive and Scalehouse Loop. Korpine Street south of Phase C can either be constructed as a full street improvement or as a temporary gated emergency access road, gate installed south of Timber Yards Drive.</p> <p>Provision of an enhanced pedestrian crossing at Bond Street and Powerhouse Drive intersection. The City may allow the limited deferral of the crossing with an acceptable agreement in place between Timber Yards and the Old Mill to construct a broader intersection improvement that includes an equivalent pedestrian crossing.</p>	<p>Prior to Occupancy of the first building in Phase C</p>

Mitigation Requirement	Trigger
<p>Construction of Mobility Hub along SW Bond Street frontage.</p>	
<p>Industrial Way – ROW dedication required to obtain 40 feet ROW width from ROW centerline to frontage property line and completion of ¾ collector street improvements from Bond Street through the Korpine Street intersection – full asphalt street bound by curb on both sides and a wide sidewalk along the frontage.</p> <p>Timber Yards Drive and Everpine Street (north) – Dedication of ROW and completion of ¾ local street improvements – full asphalt street bound by curb on both sides and sidewalk along the property frontage.</p> <p>Everpine Street (south) – Dedication of public access easement and completion of private street to local street standards from Timber Yards Drive south to a turnaround used for site access and public sewer pump station maintenance – full asphalt street bound by curb on both sides and sidewalk.</p> <p>Korpine Street – Dedication of ROW and completion of ¾ local street improvements – full asphalt street bound by curb on both sides and sidewalk from Industrial Way to and through Scalehouse Loop. Korpine Street will connect to Scalehouse Loop with development of Phase D and/or Phase F.</p> <p>Construction of sidewalk on Scalehouse Loop along the property frontage within a public access easement or right of way dedication.</p>	<p>Prior to Occupancy of the first building in Phase D</p>
<p>Industrial Way & Division Street – ROW dedication required to obtain 40 feet ROW width from ROW centerline to frontage property line and completion of ¾ collector street improvements from Bond Street to and through the site frontage – full asphalt street bound by curb on both sides and a wide sidewalk along the frontage.</p> <p>Prior to occupancy, the existing fire gate on Industrial Way must be removed unless otherwise approved by the City Engineer.</p> <p>Modification of the existing Industrial Way / Division Street cycle track to a 10-foot multiuse pathway on northeast side of the street</p>	<p>Prior to Occupancy of the first building in Phase E</p>

Mitigation Requirement	Trigger
<p>Timber Yards Drive and Korpine Street – Dedication of ROW and completion of ¾ local street improvements – full asphalt street bound by curb on both sides and sidewalk along the property frontage.</p> <p>Dedication of ROW and construction of the Division Street/Aune Street single lane roundabout, if not already constructed with Phases G or H.</p>	
<p>Industrial Way – Dedication of ROW and completion of ¾ collector street improvements from Bond Street through the Korpine Street intersection – full asphalt street bound by curb on both sides and a multi-use path along the frontage.</p> <p>Timber Yards Drive and Korpine Street – Dedication of ROW and completion of ¾ local street improvements – full asphalt street bound by curb on both sides and sidewalk along the property frontage.</p> <p>Korpine Street – Dedication of ROW and completion of ¾ local street improvements – full asphalt street bound by curb on both sides and sidewalk from Industrial Way to and through Scalehouse Loop. Korpine Street will connect to Scalehouse Loop with development of Phase D and/or Phase F. An agreement with Old Mill District must be established to permit the connection of this street to the private Scalehouse Loop street prior to occupancy.</p> <p>Construction of sidewalk on Scalehouse Loop along the property frontage within a public access easement or right of way dedication.</p>	<p>Prior to Occupancy of the first building in Phase F</p>
<p>Division Street – Dedication of ROW and completion of ¾ collector street improvements northeast corner of Phase E to Aune Street – full asphalt street bound by curb on both sides.</p> <p>Prior to occupancy, the existing fire gate on Industrial Way must be removed unless otherwise approved by the City Engineer.</p>	<p>Prior to Occupancy of the first building in Phase G</p>

Mitigation Requirement	Trigger
<p>Modification of the existing Industrial Way / Division Street cycle track to a 10-foot multiuse pathway on northeast side of the street.</p> <p>Timber Yards Drive – Dedication of ROW and completion of ¾ local street improvements – full asphalt street bound by curb on both sides and sidewalk along the property frontage.</p> <p>Dedication of ROW and construction of the Division Street/Aune Street single lane roundabout, if not already constructed with Phases E or H.</p> <p>Scalehouse Way (south of Aune Street roundabout to and through the property) – Dedication of ROW and completion of ¾ collector street improvements – full asphalt street bound by curb on both sides.</p> <p>Scalehouse Way (south of Phase G) is to be dedicated to the City of Bend and reconstructed to City of Bend collector street standards (street reconstruction, multiuse pathway to Bond/Wilson roundabout, and reconstruction of Bond Street/Scalehouse Loop curb radii).</p> <p>Construction of sidewalk on northern side of Scalehouse Loop along frontage.</p>	
<p>Scalehouse Loop is to be dedicated to the City of Bend and reconstructed to City of Bend collector street standards (street reconstruction, multiuse pathway to Bond/Wilson roundabout, and reconstruction of Bond Street/Scalehouse Loop curb radii).</p> <p>Dedication of ROW and construction of the Division Street/Aune Street single lane roundabout, if not already constructed with Phases E or G.</p> <p>Scalehouse Way (south of Aune Street roundabout to and through the property) – Dedication of ROW and completion of ¾ collector street improvements – full asphalt street bound by curb on both sides.</p> <p>If Phase H anticipated access to Scalehouse Court, either a 1) public access easement and public utility easement must be recorded over the entirety of the street and sidewalk (if the southern half of the street is still under the ownership of a separate landowner, a</p>	<p>Prior to Occupancy of the first building in Phase H</p>

Mitigation Requirement	Trigger
<p>maintenance agreement must be created), OR 2) Scalehouse Court is to be dedicated to the City of Bend and reconstructed to City of Bend local street standards. If a private street is retained, public access easements must be established within the property boundary.</p>	
<p>Scalehouse Way is to be dedicated to the City of Bend and reconstructed to City of Bend collector street standards (street reconstruction, multiuse pathway to Bond/Wilson roundabout, and reconstruction of Bond Street/Scalehouse Loop curb radii). Dedication of ROW and construction of the Division Street/Aune Street single lane roundabout. Scalehouse Way (south of Aune Street roundabout to and through the property) -- Dedication of ROW and completion of ¾ collector street improvements -- full asphalt street bound by curb on both sides. -- Prior to occupancy of Phase I that needs access from Scalehouse Court, either a 1) public access easement and public utility easement must be recorded over the entirety of the street and sidewalk (if the southern half of the street is still under the ownership of a separate landowner, a maintenance agreement must be created), OR 2) Scalehouse Court is to be dedicated to the City of Bend and reconstructed to City of Bend local street standards. If a private street is retained, public access easements must be established within the property boundary.</p>	<p>Prior to Occupancy of the first building in Phase I</p>

Mitigation Requirement	Trigger
<p><u>Each phase of development review will provide a trip debit letter that will track available trips banked by the Transportation Review. Trips in excess of the Traffic Impact Analysis will require an additional transportation review in accordance with BDC Chapter 4.7.</u></p>	<p>Ongoing</p>

<u>Mitigation Requirement</u>	<u>Trigger</u>
<p><u>Widen Sisemore Street pavement width to provide bicycle lanes and widened sidewalks from Colorado Avenue to Arizona Avenue within existing ROW.</u></p>	<p><u>Prior to occupancy of the 100th unit in the first phase or 38 PM peak trips on any lot.</u></p>
<p><u>Dedication of right-of-way (ROW) and completion of the Bond Street/Industrial Way single-lane asphalt roundabout adjacent to Lot A. Construction of the roundabout will require continued access to impacted uses and development. The implementation of a detour plan will be required. Unless otherwise dictated by CET, a bus turn out on Bond Street will be constructed with the roundabout, maintaining the existing bus stop prior to the roundabout.</u></p> <p><u>Dedication of ROW and completion of the Industrial Way/Aune Street mini-roundabout. The design must incorporate the full width multi-use pathway along the eastern leg and the surface treatment with concrete or asphalt to be determined with right-of-way permit review.</u></p> <p><u>Dedication of ROW and completion of Industrial Way to collector street standards from the Aune Street roundabout north and west to Bond Street including the multiuse path along the Phase A frontage.</u></p> <p><u>60 foot ROW Dedication of Sisemore Street and construction of a utility access road from the south property line of Timber Yards to Industrial Way. Road for utility access to the City sewer pump station and City water and sewer mains (adjacent to Lots B & D).</u></p> <p><u>Reconstruction of Aune Street to existing conditions within the available ROW under Hwy 97 undercrossing to 2nd Street over the required water and sewer main installation with the first Lot of development. Developer to coordinate detour plans for review and approval by the City of Bend describing how it will maintain public and emergency ingress/egress for impacted businesses during construction of area infrastructure improvements. Developer to coordinate utility</u></p>	<p><u>Prior to occupancy of the first building in any Lot</u></p>

<u>Mitigation Requirement</u>	<u>Trigger</u>
<p><u>locations in Aune Street during ROW permit review to avoid conflicts with CIP road improvement project.</u></p> <p><u>The existing gate on Industrial Way (fronting Lot E) must be removed from the ROW.</u></p>	
<p><u>Sisemore Street – Completion of 3/4 local street improvements – full asphalt street bound by curb on both sides. Sidewalk construction will be required along the applicable Lot frontage or as determined during site plan review. Design and construction of a City approved turnaround used for site access, public and fire/emergency access, and public sewer pump station and street maintenance.</u></p>	<p><u>Prior to occupancy of Lots A, B, C and/or D</u></p>
<p><u>Industrial Way – Dedication of ROW and completion of collector street improvements from Aune Street roundabout south to Bond Street – full asphalt street bound by curb on both sides and a multi-use path along the Lot frontage.</u></p>	<p><u>If any of these are constructed:</u></p> <p><u>Public vehicular connection of Industrial Way to Scalehouse Loop intersection.</u></p> <p><u>Prior to occupancy of Lots A, Phase B, H and/or I.</u></p>
<p><u>Bond Street – ROW dedication of 50 feet from ROW centerline to property line and completion of a multi-use path along Bond Street frontage to Powerhouse Drive.</u></p> <p><u>Provision of an enhanced pedestrian crossing at Bond Street and Powerhouse Drive intersection. The City may allow the limited deferral of the crossing with an acceptable agreement in place between Timber Yards and the Old Mill to</u></p>	<p><u>Prior to occupancy of the first building in Lot A</u></p>

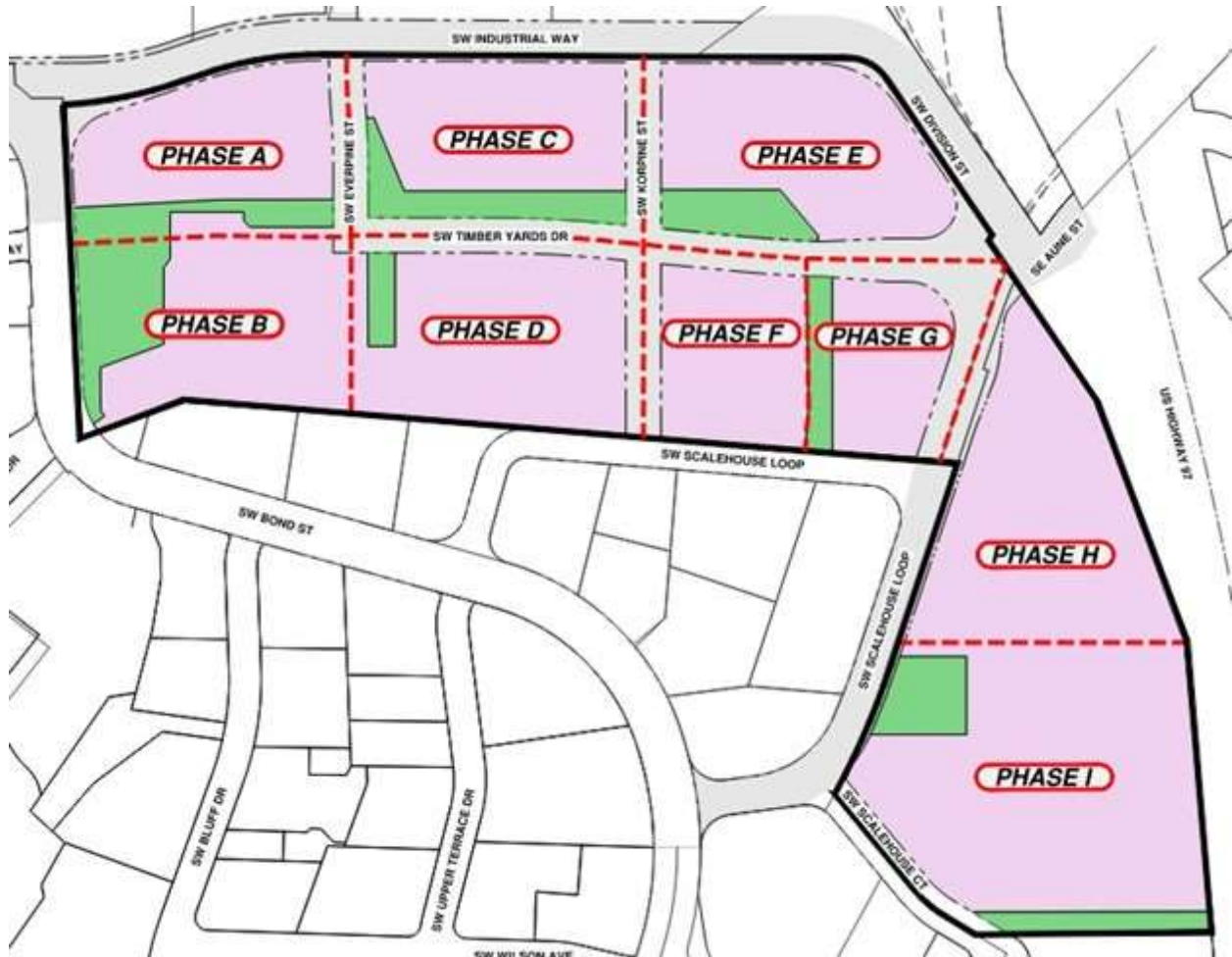
<u>Mitigation Requirement</u>	<u>Trigger</u>
<p><u>construct a broader intersection improvement that includes an equivalent enhanced pedestrian crossing.</u></p> <p><u>Dedication of public access easement for non-vehicular/pedestrian/bike and emergency access route from Sisemore Street (south of Industrial Way) west to Bond Street, gated (or similar vehicular restriction) at Bond Street, aligning with Mill View Way (south end of Lot A frontage).</u></p> <p><u>Construction of mobility hub along SW Bond Street frontage.</u></p>	
<p><u>Bond Street – ROW dedication required to obtain 50 feet ROW width from ROW centerline to frontage property line and completion of a multi-use path along Bond Street frontage to Powerhouse Drive.</u></p> <p><u>Provision of an enhanced pedestrian crossing at Bond Street and Powerhouse Drive intersection. The City may allow the limited deferral of the crossing with an acceptable agreement in place between Timber Yards and the Old Mill to construct a broader intersection improvement that includes an equivalent enhanced pedestrian crossing.</u></p> <p><u>Construction of mobility hub along SW Bond Street frontage.</u></p> <p><u>Dedication of public access easement for non-vehicular/pedestrian/bike and emergency access route from Sisemore Street (south of Industrial Way) west to Bond Street, gated at Bond Street, aligning with Mill View Way (north end of Lot B frontage).</u></p>	<p><u>Prior to occupancy of the first building in Lot B</u></p>
<p><u>Korpine Street – Dedication of 60-foot ROW and completion of $\frac{3}{4}$ local street improvements from Industrial Way to Scalehouse Loop – full asphalt street bound by curb on both sides and sidewalk.</u></p>	<p><u>Prior to occupancy of Phase 1 (Lots C, D, E, F and/or G)</u></p>

<u>Mitigation Requirement</u>	<u>Trigger</u>
<p><u>Dedication of public access easement for multi-modal route from Sisemore Street east to Aune Street per COB std R-48 (Primary Trail). Construction of the path will be required with each individual lot's development or as determined during site plan review.</u></p> <p><u>If Lots are to be divided into individual lots, access and utility easements must be recorded across property lines for access between properties and parking lots to remove/reduce access to the ROW.</u></p> <p><u>If at any time during the development of Phase 1 the fire department requires access to the easterly property line of the site (Industrial Way), it will be required that a fire access road, compliant with Fire Code, and dedication of either an emergency access easement or ROW dedication be provided south from the Aune Street roundabout to either a fire access gate to Industrial Way or to an approved turnaround.</u></p>	
<p><u>Scalehouse Loop – ROW dedication required to obtain 30 feet ROW width from ROW centerline to frontage property line and completion of sidewalk along lot frontages. No parking signs must be placed on both sides of the street unless street is widened.</u></p>	<p><u>Prior to occupancy of Lot D</u></p>
<p><u>Scalehouse Loop – ROW dedication required to obtain 30 feet ROW width from ROW centerline to frontage property line and completion of sidewalk along lot frontages. No Parking signs must be placed on both sides of the street unless street is widened.</u></p>	<p><u>Prior to occupancy of Lot F or Lot G</u></p>
<p><u>SW Industrial Way south of the Aune roundabout must dedicate ROW and construct a path with full frontage street improvements to collector street structural standards. A fire gate must be installed at the north side of the Scalehouse Loop intersection. If the fire gate is not installed and public vehicular traffic is permitted onto SW Industrial Way south of Scalehouse Loop, Industrial</u></p>	<p><u>Prior to occupancy of Lot G</u></p>

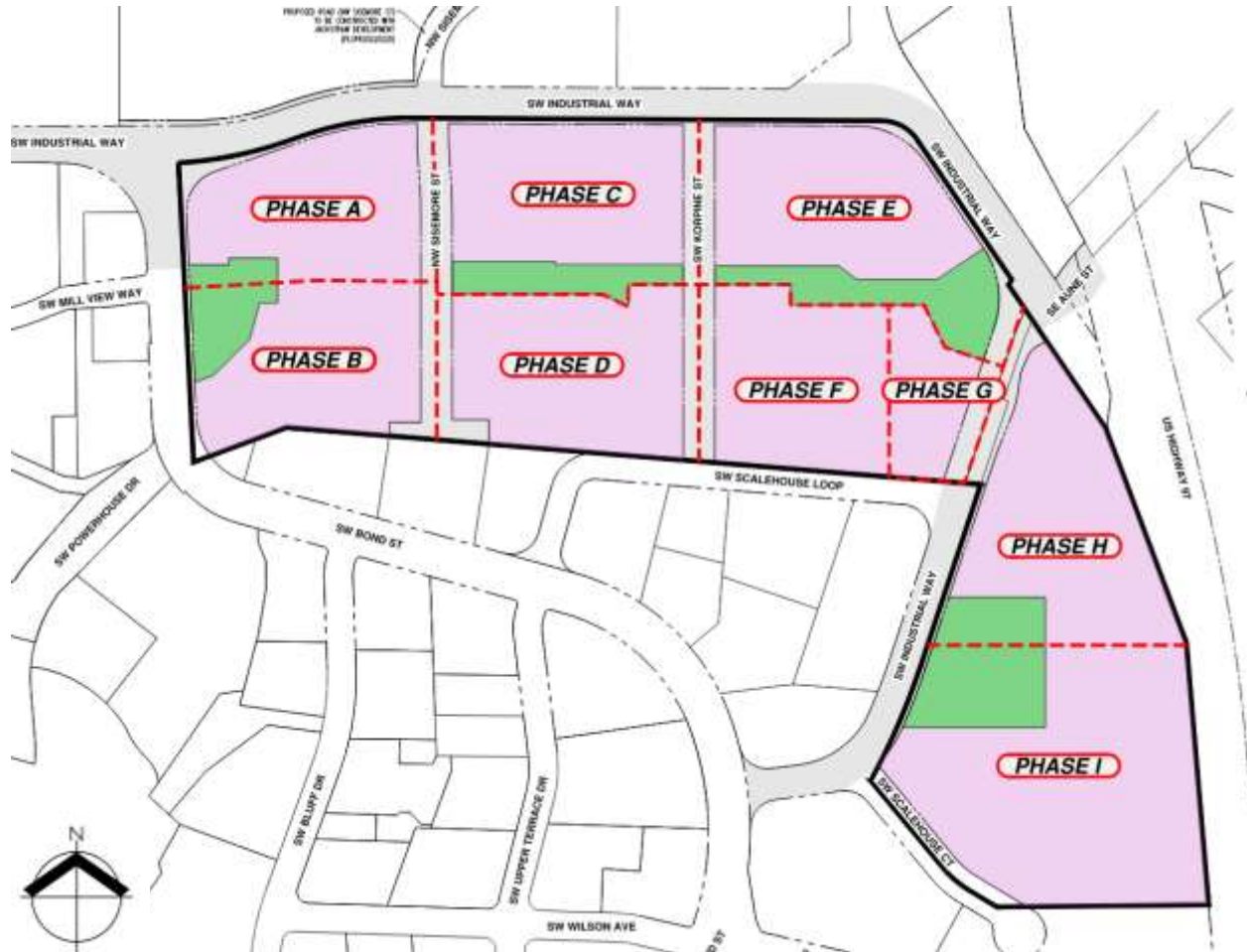
<u>Mitigation Requirement</u>	<u>Trigger</u>
<u>Way to Bond Street will be constructed to Collector street standards and ROW dedicated.</u>	
<u>Industrial Way (from Aune Street roundabout south to Bond Street) is to be dedicated to the City of Bend and reconstructed to City of Bend collector street standards (street reconstruction, multiuse pathway to Bond/Wilson roundabout, and reconstruction of Bond Street/Scalehouse Loop curb radii).</u>	<u>Prior to occupancy of the first building in Lot A, Lot B, Lot H or Lot I</u>
<p><u>If Lot I anticipates access to Scalehouse Court, either:</u></p> <p><u>1) A public access easement and public utility easement must be recorded over the entirety of the street and sidewalk (if the southern half of the street is still under the ownership of a separate landowner, a maintenance agreement must be created), or</u></p> <p><u>2) Scalehouse Court is to be dedicated to the City of Bend and reconstructed to City of Bend local street standards. If a private street is retained, public access easements must be established within the property boundary.</u></p>	<u>Prior to occupancy of building in Lot I</u>

Figure 2.7.4770. Timber Yards Phasing Plan

(Delete following figure)



(Add following figure)



2.7.4780 Future Capacity Reservation.

The Timber Yards Master Plan reserves infrastructure capacity (sewer, water, and transportation) through and including June 21, 2038, for all site plan review and subdivision applications filed pursuant to the phasing plan through June 21, 2038. Site plan review and subdivision applications submitted after June 21, 2038, will be subject to new utility and transportation analyses.

EXHIBIT B
PLANNING COMMISSION
RECOMMENDATION TO THE CITY COUNCIL



PROJECT NUMBER: PLTEXT20250679 (Text Amendment)

HEARING DATE: April 15, 2026 at 6:00 p.m.
City of Bend Council Chambers (hybrid)
710 NW Wall Street, Bend, OR 97703

REPORT DATE: March 12, 2026

APPLICANT: Kennedy Wilson
503 32nd St, Suite 120
Newport Beach, CA 92663

OWNERS: *Tax lot 181205A000500*
KW Kelton-Bend Owner, LLC
151 S El Camino Drive
Beverly Hills, CA 90212

Tax lot 181205A000800
Scalehouse Loop Investments, LLC
95 SW Scalehouse Loop #100
Bend, OR 97702

Tax lot 181205A000900
McKenzie Creek Development, LLC
95 SW Scalehouse Loop #100
Bend, OR 97702

LOCATION: East of Bond Street, west of Hwy 97 and south of Industrial Way;
175 SW Industrial Way, 95 SW Scalehouse Loop and one
unaddressed property; Tax lots 500, 800 and 900 of Deschutes
County Assessor Map 18-12-05A

REQUEST: Amendment to Bend Development Code Article XXVIII, Timber
Yards Master Planned Development, to remove the east-west
local street in the master plan and relocate proposed open
space tracts, with accompanying amendments to the adopted
street cross-sections and Timber Yards Phasing Plan and

Transportation Mitigation Plan. (Type III Quasi-judicial review with Planning Commission recommendation to City Council)

STAFF:

Karen Swenson, AICP, Senior Planner
(541) 388-5567; kswenson@bendoregon.gov

Chris Henningsen, Principal Engineer
(541) 693-2134, chenningesen@bendoregon.gov

APPLICABLE REVIEW CRITERIA, STANDARDS AND PROCEDURES:

Bend Development Code (*Effective Date October 3, 2025*)

Criteria

Chapter 4.5, Master Plans

Standards

Chapter 2.3, Mixed Use Zoning Districts (MU)

Chapter 3.1, Lot, Parcel and Block Design, Access and Circulation

Chapter 3.4, Public Improvement Standards

Chapter 4.7, Transportation Analysis

Procedures

Chapter 4.1, Development Review and Procedures

Bend Comprehensive Plan

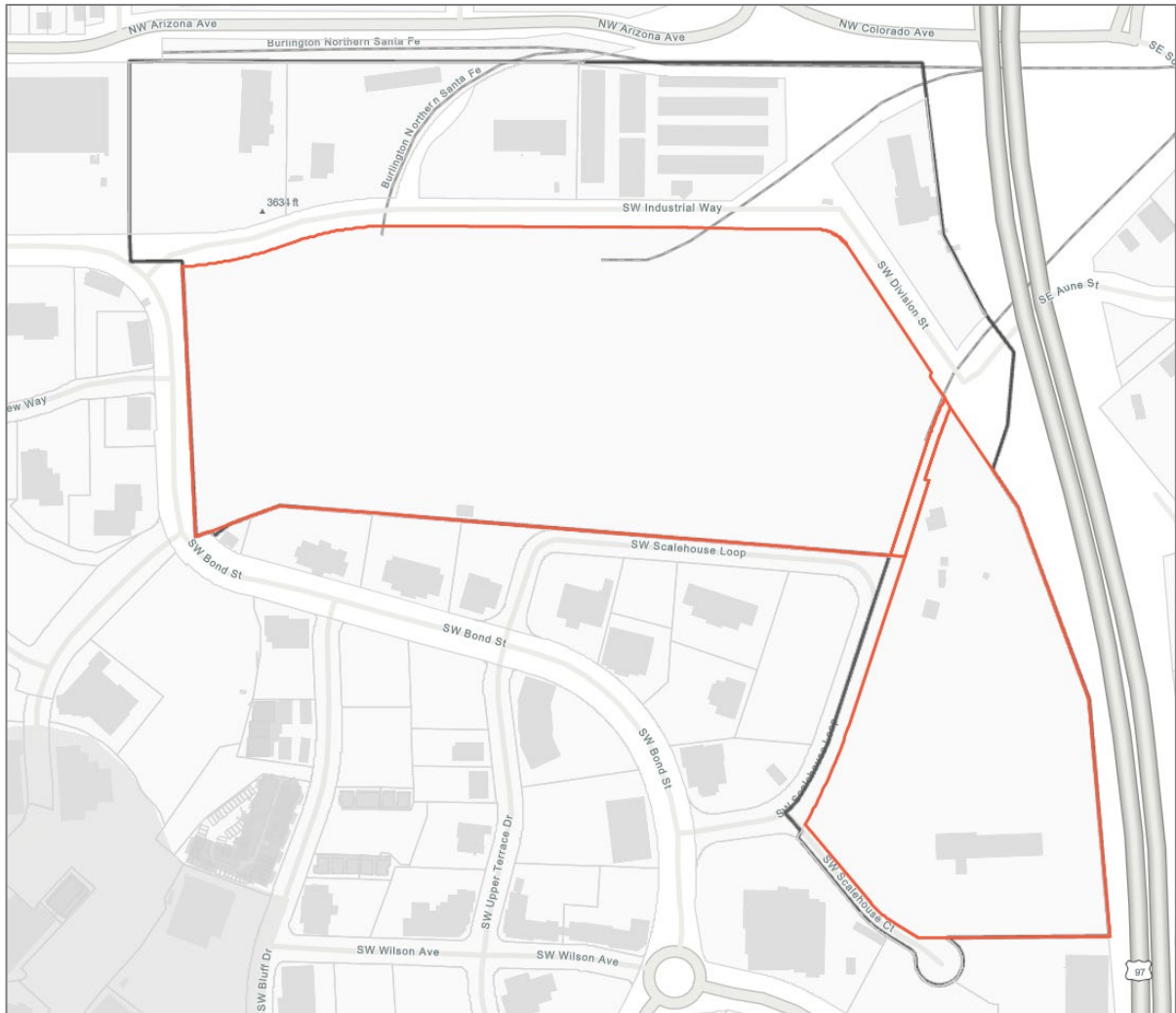
Chapter 11, Growth Management

Oregon Administrative Rules

Chapter 660 Division 15 - Statewide Planning Goals and Guidelines

FINDINGS OF FACT:

- 1. LOCATION:** The Timber Yards Master Plan encompasses 32 acres within the KorPine Opportunity Area. The site is comprised of three tax lots: 500, 800 and 900 of Deschutes County Assessor Map 18-12-05A, with addresses of 175 SW Industrial Way and 95 SW Scalehouse Loop. The property is bounded by SW Industrial Way (collector street) to the north, SW Bond Street (minor arterial) to the west, Bend Parkway (highway) to the east, and SW Scalehouse Loop (private street) to the south.

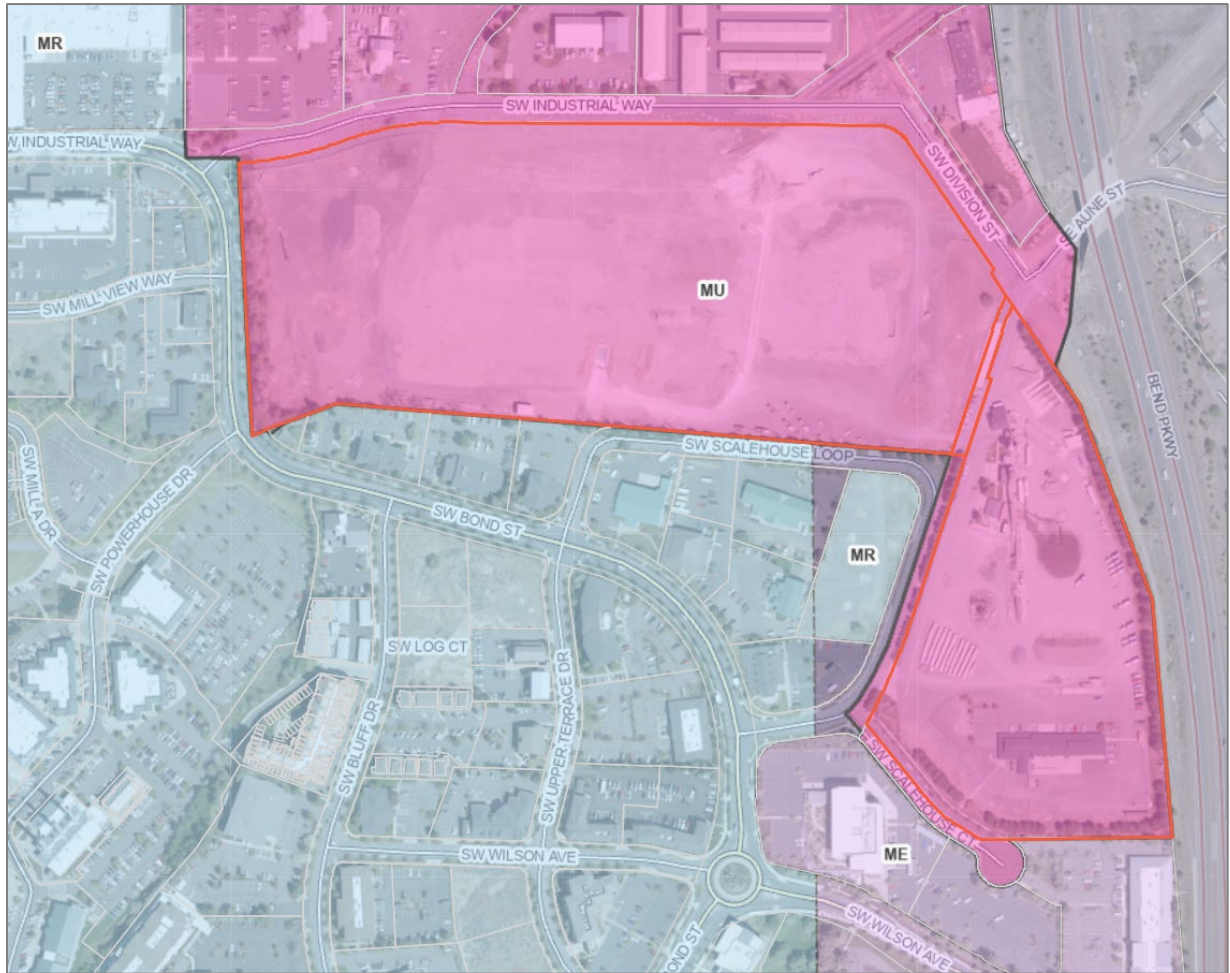


Timber Yard Master Plan subject property outlined in orange; KorPine opportunity area outlined in black

2. **ZONE AND PLAN DESIGNATION:** The Timber Yards Master Plan is located within the KorPine Opportunity Area. The master plan property is designated Mixed Use-Urban (MU) in the Bend Comprehensive Plan and also zoned MU.

4. **SITE DESCRIPTION & SURROUNDING USES:** The northern portion of the Timber Yards Master Plan (Tax Lot 500) is the former location of the KorPine particleboard plant, which operated from 1966 to 2002. In January of 2017, the former KorPine particleboard building collapsed under the weight of heavy snow. The demolished building was removed and that portion of the site has remained vacant since then. Tax Lot 800 to the southeast contains facilities for Hooker Creek, including offices and a concrete batch plant. Tax Lot 900 (approximately 0.2 acres) is a small, paved area between Tax Lots 500 and 800. The topography is relatively level with no significant

vegetation internal to the site; the boundary of the Hooker Creek property is lined with trees.



Surrounding land uses include:

North: North of the property is Industrial Way (collector street). North of Industrial Way is a 7-story mixed-use development (Jackstraw; PLSPR20220228), a landscaping company, and a self-storage facility, which are all zoned MU.

East: Northeast of the property is a brewing facility and brewpub (Crux) zoned MU. East of the property is the Bend Parkway (Highway 97). The properties east of the Parkway are zoned Light Industrial (IL).

South: To the south along SW Bond Street and SW Scalehouse Loop, properties are zoned Mixed-Use Riverfront (MR) and Mixed Employment (ME) and are generally developed with office and retail uses. The property southwest of the intersection of

Scalehouse Loop and Industrial Way is a recently platted Shared Court townhome subdivision named Scalehouse Loop Townhomes (PLLD20211044/PLMOD20240084).

West: To the west, across SW Bond Street, properties are zoned MR and developed with office, retail, and hotel uses.

- 5. BACKGROUND:** The Timber Yards site is located with the Korpine Opportunity Area identified in Figure 11-1 of the Bend Comprehensive Plan as an “opportunity to transform an industrial area into a vibrant urban mixed use district.” The Timber Yards Major Community Master Plan was approved by City Council in 2023. The adopted master plan includes a higher building height limit, design standards, the general location of open space tracts, a street circulation plan with associated street cross-sections, and a Transportation Mitigation Plan.

Due to ongoing challenges in trying to develop the site under the approved Timber Yards Master Plan, the applicant is proposing changes to accommodate a modified version of the original concept. The applicant intends to start development from the east and extending west, rather than the initial development progression from west to east (the alphabetic phasing labels were retained for consistency even though the construction sequence is not proposed to be, nor does it have to be, alphabetical). The 6-story mixed-use building in the northwest corner of the master plan (Phase A, approved under PLSPR20230465), is assumed to remain the same. The Hooker Creek property is still intended as a future phase, after development of the other phases, when Hooker Creek ceases operations on the site.

While the infrastructure analyses (water, sewer and transportation) for the adopted master plan were based on an assumed level and type of development, the specific uses and intensities were not codified as requirements in the Timber Yards Master Plan in Article XXVIII of the Bend Development Code. The land use assumptions in the updated infrastructure analyses (Exhibits G and H of the submittal) include “options” to replace proposed non-residential uses (hotel and/or office uses) with additional dwelling units, dependent on market demand and viability, in two phases (B and I), as shown in the table below.

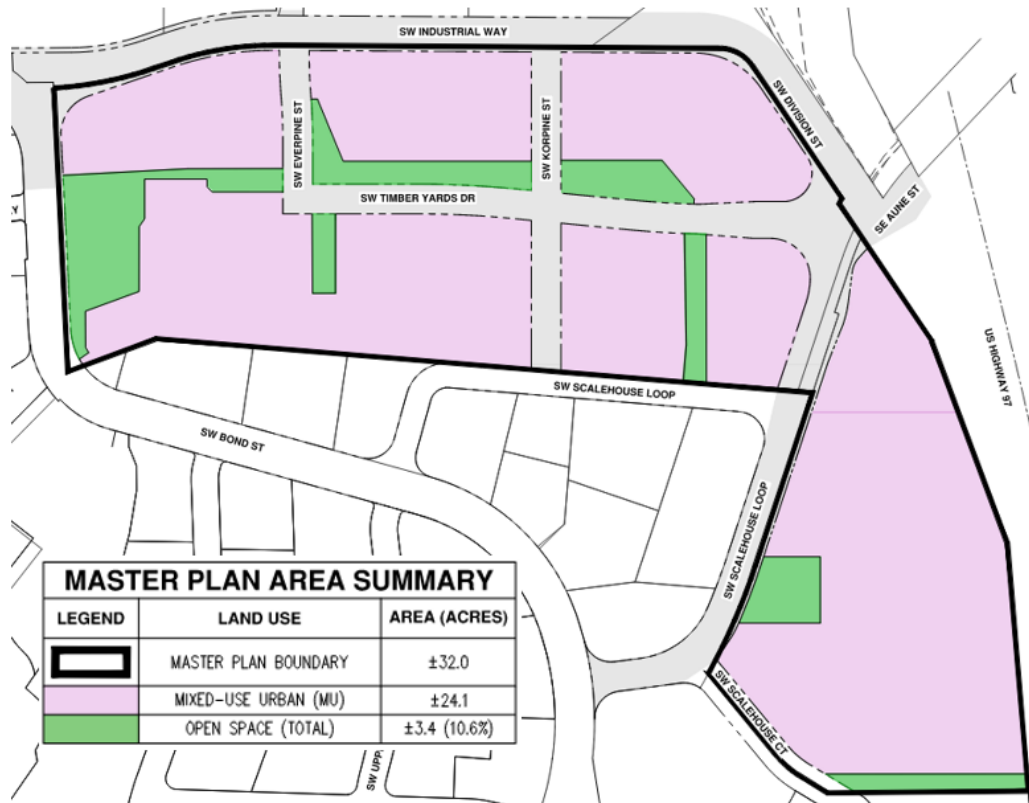
Phase	Original Use Assumptions*	Proposed Use Assumptions**	Optional Uses**
A		250 Dwelling Units 16,000 sf Retail (PLSPR20230465)	
B		176-Room Hotel 5,000 sf Retail	200 Dwelling Units 5,000 sf Retail
C		121 Dwelling Units	
D		121 Dwelling Units	
E		129 Dwelling Units	
F		129 Dwelling Units	
G			
H		250 Dwelling Units	
I		45,000 sf Office 5,000 sf Retail	150 Dwelling Units 5,000 sf Retail
Total	1,600 Dwelling Units 180-Room Hotel 120,000 sf Office 70,000 sf Retail	1,000 Dwelling Units 176-Room Hotel 45,000 sf Office 26,000 sf Retail	1,350 Dwelling Units No hotel 0 sf Office 26,000 sf Retail <i>(if both options utilized)</i>

*October 2022 Traffic Impact Analysis, not separated by phase

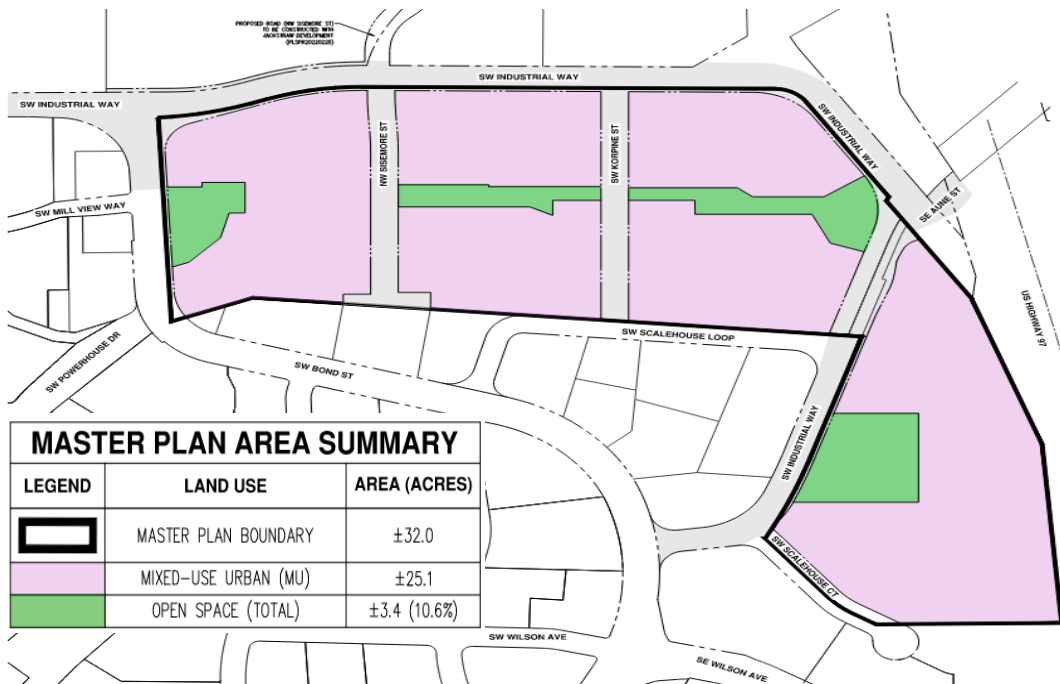
**May 2025 Traffic Analysis Memo

- 6. PROPOSAL:** The applicant proposes a Development Code Text Amendment to the Timber Yards Major Community Master Plan, including amendments to the transportation circulation plan, and amendments to the Transportation Mitigation Plan based on revisions to assumed land uses in subsequent infrastructure analyses.

The proposed text amendments include the replacement of Timber Yards Drive through the center of the master plan with a non-vehicular pedestrian-oriented open space as shown in the proposed master plan figure below. The open space tracts are relocated but the overall open space acreage remains the same. Sisemore Street is proposed to be extended to the southern property line with a permanent turnaround; existing development precludes a street connection to the south.



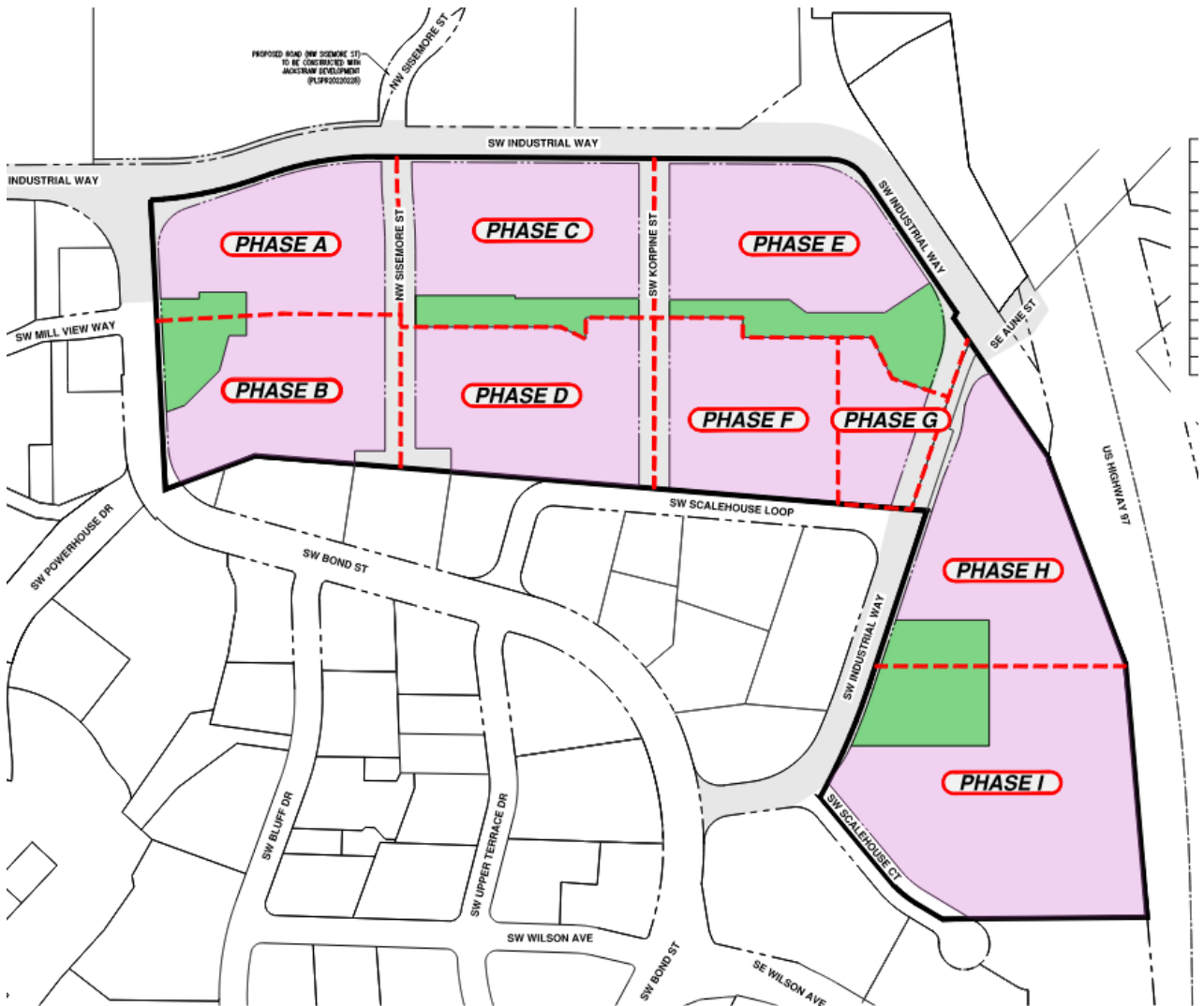
Adopted Master Plan



Proposed Master Plan

The proposed text amendments also modify the phasing plan based on updated land use assumptions and infrastructure analyses as shown in the table above. In general, these assumptions include:

- Reduced density and number of residential units within Phases C-G with four-story buildings and surface parking (see phasing diagram below)
- Reduced supporting retail space
- Reduced potential office space within the Hooker Creek property (Phases H and I)



Phase labels (construction sequence will not be alphabetical)

Finally, the proposal includes amendments to the Transportation Mitigation Plan in BDC 2.7.4770 to:

- Alter the timing of required improvements to reflect the difference in development initiation and direction (east to west);
- Require the completion of Industrial Way from Bond Street, including the Industrial Way/Aune Street roundabout in the first phase rather than staggered by phase;
- Remove the requirement to construct the southbound Hwy 97/Colorado curb returns and ramps;
- Remove the construction of the east-west local street Timber Yards Drive; and
- Change the Industrial Way/Aune Street intersection from a standard single-lane roundabout to a “mini-roundabout”.

7. PUBLIC NOTICE AND COMMENTS: In accordance with BDC 4.1.215, the applicant held a virtual neighborhood meeting on August 14, 2025 via Zoom. Documentation of the public meeting was submitted with this application, including signed verification of compliance forms from the Southern Crossing and Larkspur Neighborhood District representatives, a roster of the Zoom meeting attendees, a list of concerns stated during the virtual meeting, copies of materials provided to neighbors within 500-feet of the Timber Yards site, and copies of emails sent to the land use representatives of the Old Bend, Southern Crossing and Larkspur Neighborhood Districts. Approximately 34 people attended the virtual public meeting, including the Southern Crossing Neighborhood District representative.

On February 11, 2026, the City of Bend Planning Division mailed notice of the Planning Commission public hearing per BDC 4.1.423 and “Proposed Development” signs were posted along the site boundaries per BDC 4.1.425. On March 9, 2026, the City of Bend Planning Commission conducted a public hearing and voted unanimously to recommend approval of the proposal to the City Council.

On March 20, 2026, the Planning Division mailed notice of the Council public hearing to surrounding owners of record as shown on the most recent property tax assessment roll and to the addresses of property (based on the City’s current addressing record) within 500 feet of the site, and to the designated representatives of the Old Bend, Southern Crossing and Larkspur Neighborhood Districts. Five “Proposed Development” signs were also posted along the site boundaries visible from adjacent rights-of-way on March 20, 2026 per BDC 4.1.425.

Two written comments were received supporting the proposal in response to the public notices at the time this recommendation was written. Various agencies were also sent notice; no outside agencies submitted written comments. The required notice to the Department of Land Conservation and Development (DLCD) was

provided on January 12, 2026, more than the required 35 days prior to the first public hearing.

- 8. APPLICATION ACCEPTANCE DATE:** This Type III Text Amendment application and associated submittal materials were submitted on December 11, 2025. The application was deemed incomplete on December 16, 2025, pending the submittal of Draft Development Code amendments and a written narrative, which were uploaded on December 17, 2025. The application was deemed complete on December 17, 2026. In accordance with BDC 4.1.413, this Text Amendment to the Bend Development Code, which is a Post-Acknowledgement Comprehensive Plan Amendment, is exempt from the 120-day review time limitation for final decision.
- 9. RECORD:** The documents in CityView for PLTEXT20250679 are made part of the record and are placed before the City Council for consideration during the proceedings on the amendments. The documents are available for review and can be viewed in the Online Permit Center Portal at <https://cityview.ci.bend.or.us/Portal/Planning/Locator> on the City of Bend website by entering the project number PLTEXT20250679 in the search bar to find the project.

APPLICATION OF THE CRITERIA:

Bend Development Code

Chapter 4.6.300, Quasi-Judicial Amendments

- A. Applicability, Procedure and Authority. Quasi-judicial amendments generally refer to plan or zone change affecting a single or limited group of properties and that involves the application of existing policy to a specific factual setting. Quasi-judicial amendments follow the Type III procedure, as governed by BDC Chapter 4.1, Development Review and Procedures, using the standards of approval in subsections (B) or (C) of this section, as applicable. Based on the applicant's ability to satisfy the approval criteria, the application may be approved, approved with conditions, or denied.**

FINDING: The application is for a Development Code text amendment to the Timber Yards Master Plan in BDC Chapter 2.7, affecting a limited group of properties that involves the application of existing policy to a specific factual setting. Therefore, this request will follow the Type III quasi-judicial procedures. Since the Text Amendment to the master plan does not include amendments to the Bend Comprehensive Plan Map nor includes a zone change, the procedures and approval criteria of BDC 4.6.300 are superseded by BDC Chapter 4.5, Master Plans. Therefore, the quasi-judicial amendment to Article XXVIII,

Timber Yards Master Planned Development, is being reviewed under BDC 4.5.200, Community Master Plan.

4.5.200 Community Master Plan

D. Community Master Plan Approval Criteria.

1. Minor or Major Community Master Plan. The City may approve, approve with conditions, or deny a proposed minor or major community master plan application based on meeting all of the following criteria:

a. The community master plan complies with subsection (E) of this section, Standards and Regulations.

FINDING: Compliance with subsection (E) is addressed below on page 27.

b. Existing water and sewer facilities have adequate capacity to serve the proposed development in compliance with the Collection Systems Master Plan and the Water System Master Plan, latest editions, or adequate facilities will be installed prior to occupancy or use.

FINDING: The Utility Availability Memo (PRSWA202503369) prepared by the City's Private Development Engineering Division shows that adequate capacity exists in the water and collection systems, as further described in findings on page 16 and 17 below. The proposed development must construct the required mitigation measures outlined in the Utility Availability Memo, which will be required under a subsequent land division application and/or Site Plan Review application. This criterion is met.

c. The community master plan complies with BDC Chapter 4.7, Transportation Analysis.

FINDING: The Transportation Analysis Memo (PRTFR202503539) prepared by the City's Private Development Engineering Division outlines compliance with Chapter 4.7. Compared to the adopted master plan, the applicant is identifying a reduction in proposed trips by approximately one third. The safety and operational issues previously identified in the original master plan remain unchanged, but the level of impact specifically attributed to Timber Yards is reduced with the modified proposal. The "options" for various uses generate the same or fewer trips than the proposed uses assumed in the updated transportation analysis; therefore, the mitigation measures required in the Transportation Mitigation Table, BDC 2.7.4770, can support either of the market-driven land use scenarios.

3. Major Community Master Plan. In addition to the approval criteria in subsection (D)(1) of this section the City may approve, approve with conditions, or deny a proposed major community master plan application based on meeting all of the following criteria:

- a. **The community master plan land uses and densities must be consistent with the Bend Comprehensive Plan Map designations. If rearranging the plan designation locations and/or zoning is proposed as part of the master plan application, the master plan must retain the same total area of all plan designations on the subject site or within one percent of the same total acreage and maintain the density/housing numbers consistent with the allocations prescribed by the existing plan designations except as provided in subsection (E)(3) of this section. Any other changes to the plan designations and density/housing numbers, or other changes to the Comprehensive Plan Map designations, require a Comprehensive Plan and Zoning Map amendment to be processed concurrently in accordance with BDC Chapter 4.6, Land Use District Map and Text Amendments.**

FINDING: The Timber Yards site is currently designated MU on the City's Comprehensive Plan Map and is zoned MU. No changes to plan designation or zoning are necessary or planned. The criterion is met.

- b. **The applicant has demonstrated that the standards and zoning district requirements contained in BDC Title 2, Land Use Districts, and BDC Title 3, Design Standards, are capable of being met during site plan or land division review, except as proposed to be modified by the applicant as part of a major community master plan. Where the applicant has proposed deviations to the above standards and/or zoning district requirements, the applicant has demonstrated:...**

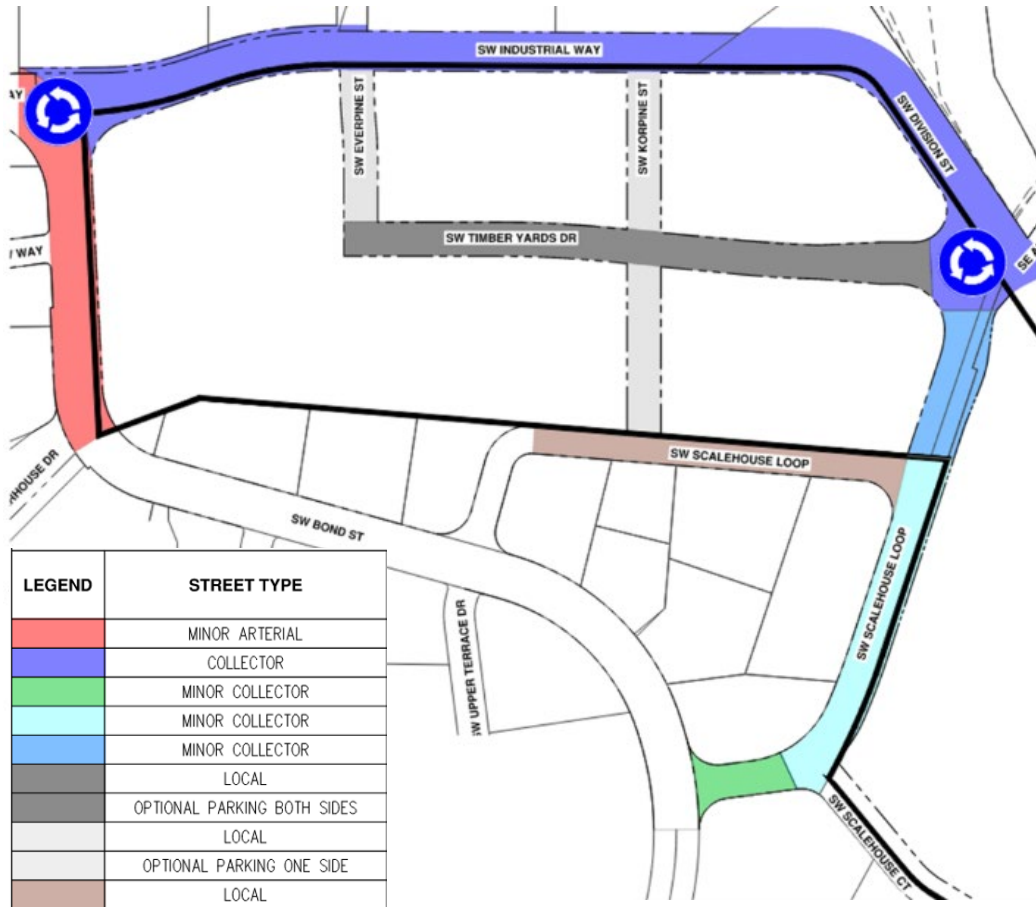
FINDING: The original master plan included approved deviations to the underlying code standards as codified in Article XXVIII of the Bend Development Code for building height, architectural design standards, loading standards and special street standards. No new deviations are included in the proposed text amendments to modify the master plan figure, the circulation plan figure, and the Transportation Mitigation Plan.

Subsequent Site Plan Review applications for future buildings will provide the required details demonstrating compliance with the applicable provisions of BDC Title 2 (permitted uses, development and design standards, etc.) The Timber Yards Major Community Master Plan (Exhibit B of the application) shows that applicable requirements of Title 3 are capable of being met, except as already modified in BDC Chapter 2.7 BDC Article XXVIII, as summarized below.

Chapter 3.1 – Lot, Parcel and Block Design, Access and Circulation

As shown in the adopted Figure 2.7.4760, Timber Yards Street and Circulation Plan, and as stated in the adopted Section 2.7.4760.B of the Timber Yards Master Plan code, the block

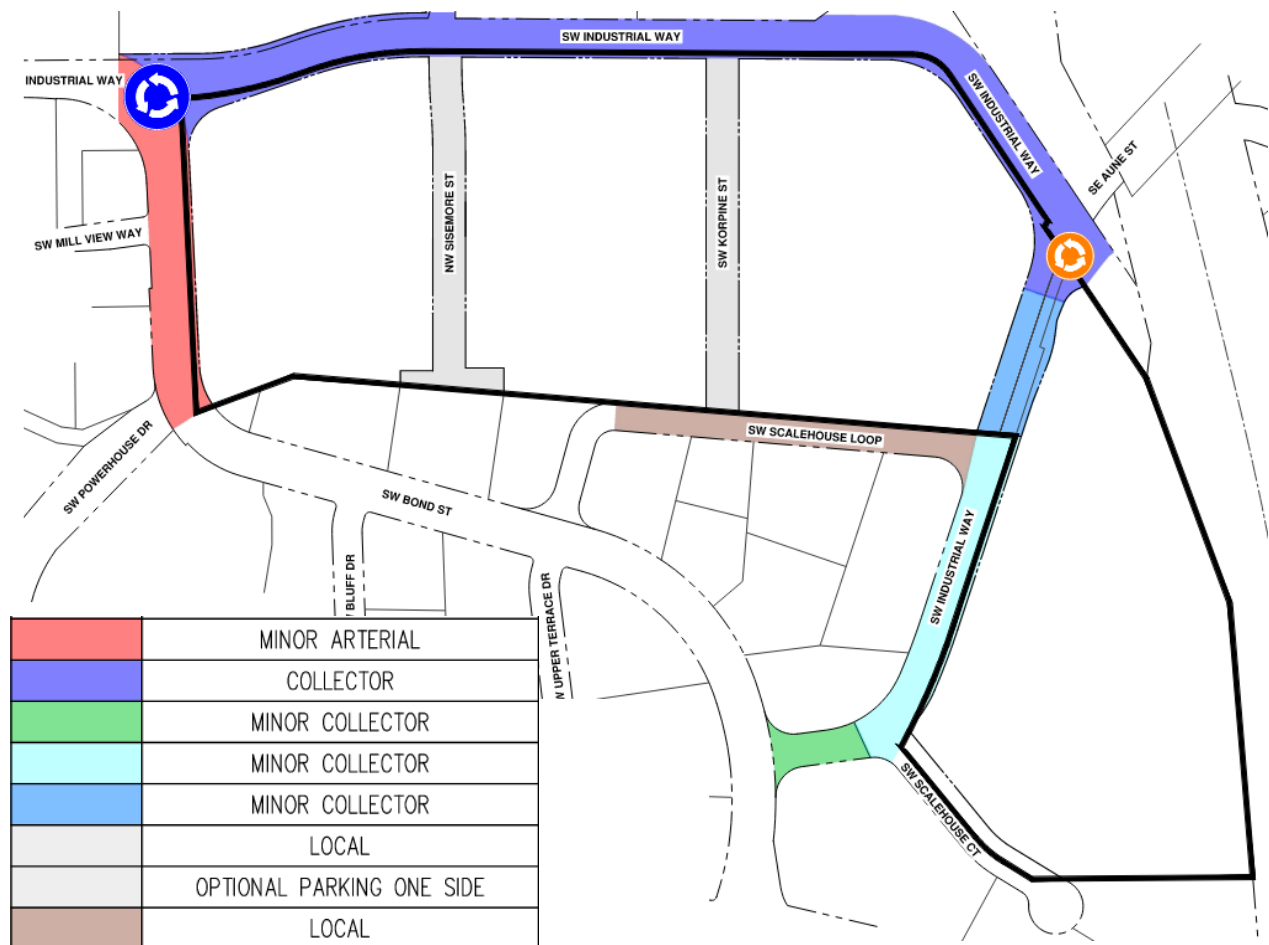
length and perimeter maximums in BDC Chapter 3.1 do not apply if the future streets align with that figure.



Adopted Street Circulation Plan

The proposed Preliminary Street Circulation Plan (Updated BDC Figure 2.7.4760 and Sheet P06 of Exhibit B of the application) shows the proposed street and block configuration and Timber Yards' general circulation scheme. The proposed text amendment application removes the central east-west local street, Timber Yards Drive, from Sisemore Street to Aune Street/Industrial Way. The adopted master plan included an open space tract abutting a portion of the northern edge of Timber Yards Drive; this amendment removes

Timber Yards Drive as a vehicular street and extends the open space tract to the east rather than the south (see adopted and proposed BDC Figure 2.7.4720).



Proposed Street Circulation Plan

Since the proposed text amendment removes the east-west local street, Timber Yards Drive, the proposed block length and block perimeter distances will change. BDC 3.1.200.D.2.c sets a maximum block length of 660 feet and maximum block perimeter of 2,640 feet for the MU zone district. The existing Timber Yards Master Plan included special street standards that superseded the typical BDC standards for block length and block perimeter.

The proposed block perimeter of the eastern block bounded by Korpine Street, Scalehouse Loop and SW Industrial Way doubles in length to approximately 2,450 lineal feet with the removal of Timber Yards Drive; however, it would remain less than the maximum block perimeter of 2,640 feet in BDC 3.1.200.D.2.c. The block that was

previously bounded by Sisemore Street, SW Industrial Way, SW Korpine Street and Timber Yards Drive, a distance of approximately 1,750 lineal feet, no longer exists with the removal of Timber Yards Drive. The remaining block perimeter bounded by SW Industrial Way, SW Korpine Street, Scalehouse Loop and Bond Street is approximately 3,485 lineal feet, which is the same length in the approved master plan. The proposed block lengths and perimeters remain similar to those in the approved master plan and further deviations are not required.

The proposed linear open space tract between Sisemore Street and SW Korpine Street and a public access easement (to be required during subsequent land division review) connecting that linear open space tract with the plaza open space tract near Bond Street will provide an access corridor as allowed under a Discretionary Track Review under BDC 3.1.200.D.2.d. As proposed, BDC 3.1 can be met and will be reviewed in detail during future land division review.

Chapter 3.2 – Landscaping, Tree Preservation, Fences and Walls

The Preliminary Open Space Plan (Sheet P04 of Exhibit B of the application) shows the urban plaza near Bond Street, the proposed central east-west open space corridor that replaces Timber Yards Drive, and the proposed open space tract straddling Phases H and I east of Industrial Way. Cumulatively, these open space tracts comprise at least 10 percent of gross acreage of the subject properties.

Future land use applications will address tree preservation in detail, as required in Chapter 3.2. The Existing Conditions Plan (Sheet P02 of Exhibit B of the application) shows the subject properties are devoid of vegetation. Street trees exist along some portions of the property boundaries within public rights-of-way or public access easements on private streets, including a row of trees on the east side of Bond Street, on the east side of Industrial Way between Scalehouse Loop and Scalehouse Court, and on the north side of Scalehouse Loop. Preservation of these offsite trees will be reviewed with future land division applications and right-of-way permits.

Conceptual street tree locations are also shown on Sheet P02 but are dependent on utility placement with future land use applications and right-of-way permits. Fences and walls will be addressed in future land use applications, if applicable. In addition to new street tree plantings that will be required with future land division and site plan review approvals, on-site landscaping for future open space will incorporate additional trees and landscaping. These standards can be met.

Chapter 3.3 – Vehicle Parking, Loading and Bicycle Parking

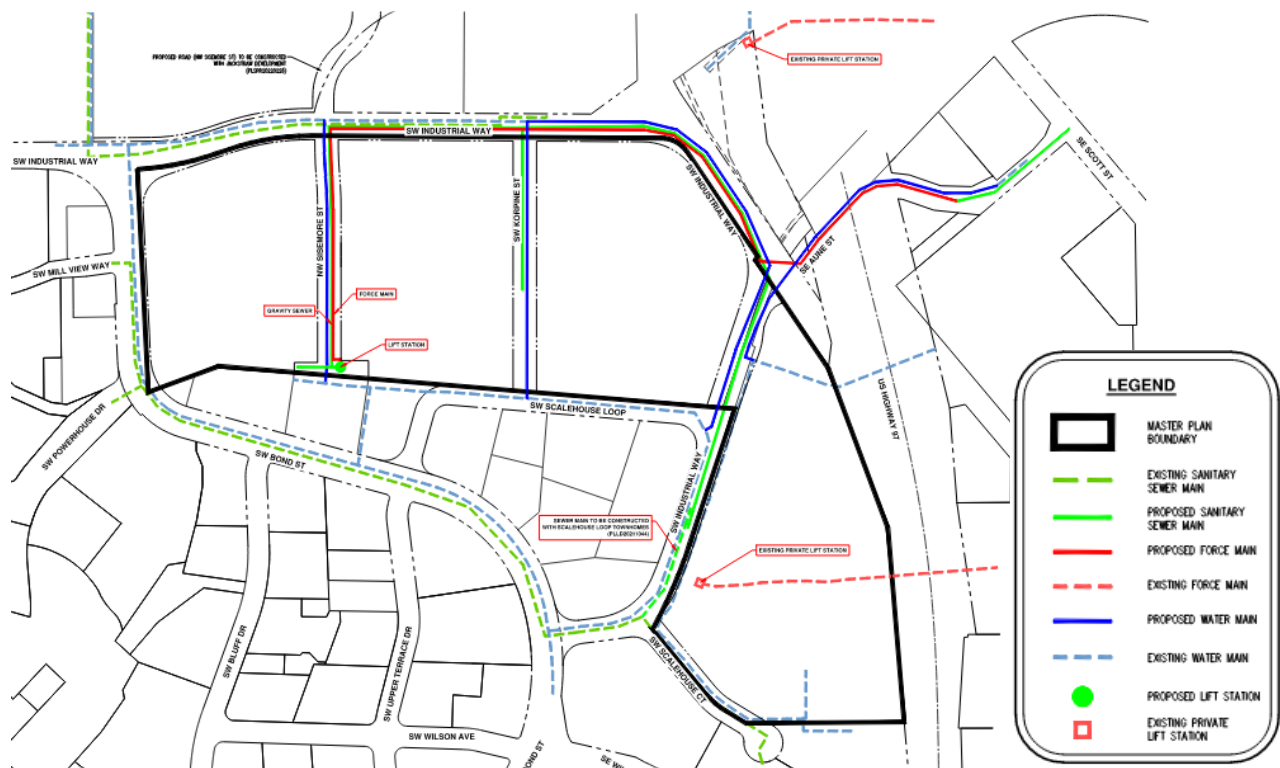
Compliance with Chapter 3.3 will be reviewed during future Site Plan Review for multi-unit residential, mixed-use, and commercial development. Compliance with this section can be

met through site design and preserving adequate areas to meet applicable vehicle parking, loading, and bicycle parking requirements. The adopted master plan code (BDC 2.7.4740.B) does not require on-site loading areas.

Chapter 3.4 – Public Improvement Standards

Infrastructure necessary to serve the variety of planned uses within the Timber Yards Master Plan as they are built out over the next decade and beyond has been identified. Sheet P10, Preliminary Utility Plan, of Exhibit B of the application, shows the proposed alignment of sanitary sewer and potable water mains as well as a proposed sewer lift station at the southwest edge of the master plan. More detailed review of public infrastructure requirements will occur as part of future land use applications.

The sewer analysis indicates the Drake lift station and the Old Mill lift station are at capacity; thus no flows are permitted into the SW Industrial Way or Bond Street gravity mains. A sewer lift station is required in the southwest portion of the site with a force main that ultimately discharges to the gravity main at SE Scott Street/SE 2nd Street. The existing 8-inch gravity main in Aune Street must be replaced with a 10-inch gravity main to accommodate the discharge from the force main. All gravity mains within the master plan site will drain to the new sewer lift station.



The water capacity analysis indicates that the development extends over two pressure zones: the subject property west of Scalehouse Loop/Division Street (Tax Lot 500) is in Pressure Zone 5 and the property east of Scalehouse Loop (Tax Lot 800) and Aune Street is in Pressure Zone 4. The mitigation measures for the proposed development require looping the water mains separately within each pressure zone. Within Pressure Zone 5, the development must extend a 10-inch water main within Industrial Way to SE Aune Street and then extended south from the SE Aune Street roundabout to connect to the existing main in SW Scalehouse Loop. An 8-inch water main must be constructed within each of the two new north-south local streets. Within Pressure Zone 4, the development must extend a 10-inch main from the existing dead end main within Aune Street under the Hwy 97 underpass to the Aune Street roundabout and then south to connect to the existing 12-inch main near the northern boundary of the Hooker Creek site.

The Utility Availability Memo (PRSWA202503369) and its associated mitigation measures demonstrate that water and sewer facilities will be adequate once facilities are installed with construction and prior to occupancy or use.

Sheet P07, Preliminary Street Circulation Plan, of Exhibit B of the application, shows the transportation facilities that will serve the proposed development. The Transportation Analysis Memo (PRTFR202503539) includes measures that must be constructed to mitigate the transportation impacts of the proposed development, which are included in the proposed Transportation Mitigation Plan in the amended Article XXVIII of the Bend Development Code.

Future land use applications will address public improvement standards in detail, as required in Chapter 3.4.

c. In lieu of the approval criteria in BDC 4.6.300, Quasi-Judicial Amendments, major community master plan applications that do not propose a Bend Comprehensive Plan amendment must demonstrate compliance with the following:

i. Approval of the request is consistent with the relevant Statewide planning goals that are designated by the Planning Director or designee; and

Goal 1, Citizen Involvement

To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.

FINDING: The City's acknowledged citizen involvement program for quasi-judicial amendments is codified in BDC Chapter 4.1. The first step for citizen involvement is the public meeting required by BDC 4.1.215. The applicant held a neighborhood public meeting on August 14, 2025. Notice was provided to property owners located within

500 feet of the subject property, and the Old Bend, Southern Crossing and Larkspur Neighborhood Districts, in compliance with BDC 4.1.215. The required Verification of Compliance Public Meeting form, and documentation of the mailing of notices, were included in the submittal. Type III applications are also noticed by the City pursuant to BDC 4.1.423 and the applicant posted five Proposed Development signs on the site visible from adjacent street rights-of-way, which ensures that members of the public are informed in multiple ways of the opportunity to participate in a public hearing. The requirements of this goal have been met.

Conformance with Goal 1 is further achieved through compliance with Title 4 of the Bend Development Code, and Chapter 4.1, Development Review and Procedures. Section 4.6.300 of the Development Code establishes that quasi-judicial amendments must follow a Type III procedure as governed by Chapter 4.1.

An initial public hearing before the Planning Commission will precede a second public hearing before the City Council. The public involvement procedures identified in the Development Code are being followed, which will ensure compliance with Statewide Planning Goal 1.

Goal 2, Land Use Planning

To establish a land use planning process and policy framework as a basis for all decisions and actions related to use of land and to assure an adequate factual base for such decisions and actions.

FINDING: The City will review and process this application consistent with the procedures detailed in BDC Chapter 4.1, including consideration of any agency and public comments received regarding the application and findings of fact throughout this staff report. Therefore, consistency with this Statewide Planning Goal is established.

Goal 3, Agricultural Lands

To preserve and maintain agricultural lands.

Goal 4, Forest Lands

To conserve forest lands by maintaining the forest land base and to protect the state's forest economy by making possible economically efficient forest practices that assure the continuous growing and harvesting of forest tree species as the leading use on forest land consistent with sound management of soil, air, water, and fish and wildlife resources and to provide for recreational opportunities and agriculture.

FINDING: There are no designated agricultural or forest lands within the project area. Therefore, Goals 3 and 4 do not apply.

Goal 5, Natural Resources, Scenic and Historic Areas, and Open Spaces

To protect natural resources and conserve scenic and historic areas and open spaces.

FINDING: The City's adopted inventories and maps do not show any Goal 5 significant natural resources, scenic and historic areas, and open spaces on the subject property. Therefore, Goal 5 is not applicable.

Goal 6, Air, Water and Land Resources Quality

To maintain and improve the quality of the air, water and land resources of the state.

FINDING: Air and water quality are regulated by the Oregon Department of Environmental Quality. For areas within the Bend Urban Growth Boundary (UGB), the Bend Development Code includes regulations for the Waterway Overlay Zone (WOZ) and Areas of Special Interest (ASI), which has been acknowledged by the Department of Land Conservation and Development. The subject properties are not located within the WOZ nor do they contain an ASI. Maintaining or improving the quality of the community's air, water and land resources will be assured through enforcement of state and local regulations. Noise levels will not exceed DEQ noise regulations. For the above reasons, the requirements of Goal 6 are met.

Goal 7, Areas Subject to Natural Hazards

To protect people and property from natural hazards.

FINDING: No 100-year floodplains or mapped landslide areas or any inventoried or mapped Goal 7 natural hazards are located within the subject property. Therefore, Goal 7 is not applicable.

Goal 8, Recreational Needs

To satisfy the recreational needs of citizens of the state and visitors and, where appropriate, to provide for the siting of necessary recreational facilities including destination resorts.

FINDING: The City adopted the Bend Comprehensive Plan and the Department of Land Conservation and Development has acknowledged that Plan. Chapter 2 of the Bend Comprehensive Plan establishes Goals and Policies which address Natural Features and Open Space needs within the City of Bend and prescribes responsibilities of both the City and the Bend Park and Recreation District (BPRD), via Policy 2-2.

Specific to this site, the Timber Yards Master Plan includes 3.4 acres (10.6 percent) of urban open space. The applicant envisions a plaza fronting SW Bond Street that will provide the pedestrian gateway into the neighborhood. The plaza will then connect through Phase A/B within a public access easement to Sisemore Street. East of Sisemore Street a linear open space is planned to be developed with paths and trails that connect the future buildings, smaller pockets of open space, and the larger network of multiuse paths. Specific details about the uses and design of the open space areas will be determined through future land division and Site Plan Review applications. Therefore, Goal 8 is satisfied.

Goal 9, Economic Development

To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens.

FINDING: The KorPine Opportunity Area (within which the Timber Yards master plan is located) is summarized in BCP Chapter 11 as an “opportunity to transform an industrial area into a vibrant urban mixed use district.” The subject properties have been identified in the City’s Comprehensive Plan as appropriate for a mix of residential, commercial, and/or public and institutional uses with a Mixed-Use Urban (MU) land use designation. In addition to capacity for 1,000 residential units, the assumed uses in the infrastructure analyses include 26,000 square feet of retail uses on the 32-acre site. A hotel and 45,000 square feet of office uses may also be developed in Phases B and I, if not developed with 200 and 150 residential units, respectively. Although the specific uses and intensity of uses will be determined later with subsequent Site Plan Review applications, the land use designation and overall size of the master plan area can accommodate a variety of economic activities. Therefore, compliance with Goal 9 is achieved.

Goal 10, Housing

To provide for the housing needs of the citizens of the state.

FINDING: Bend’s housing needs are changing and key demographic changes are occurring in Bend and across the nation. According to the City’s Housing Needs Analysis (HNA), Baby Boomers may need affordable housing or may choose to downsize their housing, resulting in greater demand for small single-unit dwellings, cottages, accessory dwelling units, townhomes, apartments, and condominiums and growth in Millennial households will increase the need for affordable housing for renters and homeowners such as small single-unit dwellings, cottages, accessory dwelling units, duplexes, townhomes, garden apartments, and apartments.

Table 20 of the City’s most recent 2016 HNA (shown below) indicates the needed types of housing units in the City of Bend between 2014 and 2028. Per the City’s online Housing Data Hub (Bend Housing Needs Dashboard), based on the building permits submitted since 2014, a need for 1,955 single-unit detached units, 539 single-unit attached (townhome) units and 144 multi-family units remains until the end of 2028. At the current annual production schedule (based on a 5-year average), approximately 1,438 single-unit dwellings, 343 townhomes and 1,249 multi-family units are anticipated to be permitted by the end of 2028, leaving a deficit of 517 single-unit dwellings and 196 townhome dwellings while the multi-family unit needs will be met by 2028. The future housing units constructed within the Timber Yards master plan will be occupied after 2028, and thus do not contribute to meeting the housing needs outlined in the current Housing Needs Analysis. Technical work has begun in 2026 on a new Housing Needs Analysis for the next time horizon, likely 2029-2049, which will provide an updated breakdown of housing need by unit type.

Table 20. Summary of All New Housing Units by Type and Category, Bend, 2014-2028

	2014-2028 Needed Housing Units		2014-2028 Needed Group Quarter Units	2014-2028 Second Homes	2014-2028 Total New Housing Units	
	Units	Mix	Units	Units	Units	% of Total Units
Single-family detached (including mobile homes)	7,574	55%		1,652	9,225	54%
Single-family attached	1,377	10%		300	1,677	10%
Multi-family	4,819	35%	461	1,051	6,331	37%
Total	13,770	100%	461	3,003	17,234	100%

Source: ECONorthwest

The proposed text amendment to the Timber Yards Master Plan provides capacity for approximately 1,000-1,350 residential units in multi-unit or mixed-use developments near transit and services. Given a typical buildout rate of master plans of this size, the proposed units within the Timber Yards Master Plan will likely be permitted in the time horizon of the new HNA currently underway.

Goal 11, Public Facilities and Services

To plan and develop a timely, orderly and efficient arrangement of public facilities and services to serve as a framework for urban and rural development.

FINDING: The applicant, City staff, and utility service providers have worked to identify the infrastructure necessary to serve the variety of planned uses within the Timber Yards Master Plan, and how its construction can be coordinated with other planned public improvement projects in the area. The Preliminary Utility Plan (Sheet

P10 in Exhibit B of the application) shows how sanitary sewer and potable water are available and planned throughout the project site. The proposed revisions to the Transportation Mitigation Plan show how impacts to the transportation system will be mitigated over time, as the master plan is built out. Goal 11 is satisfied.

Goal 12, Transportation

To provide and encourage a safe, convenient and economic transportation system.

FINDING: The Transportation Planning Rule (TPR) (OAR 660-012-0000) implements Goal 12 and states the purpose is “to provide and encourage a safe, convenient and economic transportation system.” The TPR also supports mobility and accessibility, the availability of multimodal choices, efficient flow of freight, protection of existing and planned transportation facilities, and coordination among service providers.

Under OAR 660-012-0060(1), when a local government amends a land use regulation it must determine whether the amendment would “significantly affect an existing or planned transportation facility.” The adoption of the original Timber Yards Master Plan included findings that compliance with the Transportation Planning Rule (TPR) is achieved. Because the Timber Yards Master Plan was adopted as a “special planned district” under BDC Chapter 2.7, the proposed text amendment constitutes an amendment to a land use regulation.

BDC Figure 2.7.4760, Timber Yards Street and Circulation Plan, shows the transportation facilities that will serve the proposed development. The Transportation Analysis Memo (PRTFR202503539) includes measures that must be constructed to mitigate the transportation impacts of the proposed development, which are included in the proposed Transportation Mitigation Plan in the amended Article XXVIII of the Bend Development Code.

As noted in findings above, compared to the adopted master plan, the applicant is identifying a reduction in proposed vehicle trips by approximately one third. The safety and operational issues previously identified in the original master plan remain unchanged, but the level of impact specifically attributed to Timber Yards is reduced with the modified proposal. The “options” for various uses generate the same or fewer trips than the proposed uses assumed in the updated transportation analysis; therefore, the mitigation measures required in the Transportation Mitigation Table, BDC 2.7.4770, can support either of the market-driven land use scenarios.

Therefore, compliance with the Transportation Planning Rule (TPR) is achieved. As such, Goal 12 is satisfied.

Goal 13, Energy Conservation

Land and uses developed on the land shall be managed and controlled so as to maximize the conservation of all forms of energy, based upon sound economic principles.

FINDING: Goal 13 generally imposes obligations on local governments to develop plans and implementation measures that conserve energy. On a per-acre basis, the assumed mix and density of land uses within the proposed Timber Yards Master Plan, with the development standards of the MU Zone, and an increased maximum building height of 75 feet, can result in one of the more efficiently utilized development sites in the City, helping the City meet its housing needs in a manner that conserves energy, in part, by also minimizing transportation energy use.

The Preliminary Bicycle and Pedestrian Plan (Sheet P05 of Exhibit B of the application) shows planned multimodal improvements that can support transit and encourage a pedestrian-friendly environment. A network of multiuse paths will provide additional pedestrian and bicycle facilities around and through the neighborhood, including an off-site extension to connect to the City's Wilson Avenue corridor improvements. A mobility hub serving transit and multiple modes of transportation is planned on the western edge of the master plan and, per the Transportation Mitigation Plan, must be constructed prior to occupancy of the first building in Lot A/Phase A.

Therefore, Goal 13 is satisfied.

Goal 14, Urbanization

To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities.

FINDING: Timber Yards is located within the KorPine Opportunity Area. As part of the 2016 Urban Growth Boundary (UGB) expansion, the State of Oregon tasked the City with increasing the efficient use of land within the existing City Limits. This resulted in 1) the establishment of two new mixed-use zones intended to accommodate a range of residential and commercial uses in pedestrian-oriented mixed-use centers and corridors; and 2) identification of "Opportunity Areas" within the existing City Limits that have significant development (or redevelopment) potential. Consequently, the City approved the KorPine Opportunity Area, changed the plan designation and zoning on the subject property from General Industrial (IG) to MU, and then subsequently approved the Timber Yards Master Plan. The subject properties have been identified in the City's Comprehensive Plan as appropriate for a mix of

residential, commercial, and/or public and institutional uses. Uses and development intensity contemplated by the Timber Yards Master Plan are consistent with the MU zone. Therefore, Goal 14 is satisfied.

Goal 15, Willamette River Greenway

Goal 16, Estuarine Resources

Goal 17, Coastal Shorelands

Goal 18, Beaches and Dunes

Goal 19, Ocean Resources

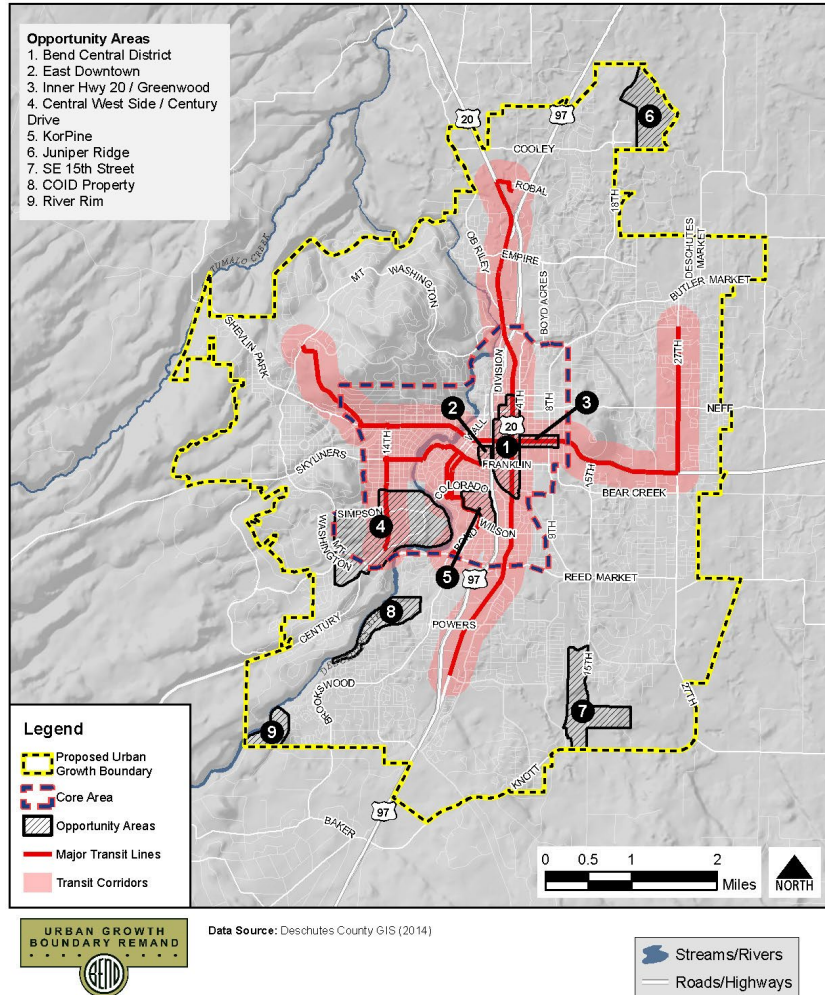
FINDING: Goals 15 through 19 are not applicable to the proposed amendments because the subject properties do not include any of the noted features and are not located within the coastal or Willamette Valley regions.

ii. Approval of the request is consistent with only the relevant policies of the Bend Comprehensive Plan Chapter 11, Growth Management, that are designated by the Planning Director or designee;

FINDING: The KorPine Opportunity Area (within which Timber Yards is located) is summarized in BCP Chapter 11 as an “opportunity to transform an industrial area into a vibrant urban mixed use district.” BDC 4.5.200(D)(3)(c) requires compliance only with the relevant policies of BCP Chapter 11, Growth Management. These are addressed below. Bend Comprehensive Plan Figure 11-1 below identifies the Korpine Opportunity Area as Opportunity Area 5.

Figure 11-1: Core Area, Transit Corridors, and Opportunity Areas

Bend UGB
Opportunity Areas
 July 18, 2016



Chapter 11: Growth Management

General Growth Management Policies

11-1 *The City will encourage compact development and the integration of land uses within the Urban Growth Boundary to reduce trips, vehicle miles traveled, and facilitate non-automobile travel.*

FINDING: The subject properties have been identified in the City's Comprehensive Plan as appropriate for a mix of residential, commercial, and/or public and institutional uses. Uses and development intensity contemplated and allowed by the Timber Yards Master Plan are consistent with the MU zone, except that building heights of 75 feet are allowed in Timber Yards rather than the 65 feet allowed in other MU-zoned areas,

providing more opportunity for compact development. The MU Zone requires a minimum residential density of 7.3 units per gross acre (same as the Medium Density Residential (RM) Zone) and there is no maximum residential density. The allowed mix of uses, as well as locating housing in close proximity to established commercial uses, will reduce vehicle miles traveled. The Preliminary Bicycle and Pedestrian Plan (Sheet P05 of Exhibit B of the application) shows a pedestrian and bicycle transportation network can be implemented that facilitates and encourages non-automobile travel throughout the Timber Yards site and with adjacent areas. This policy is met.

11-2 The City will encourage infill and redevelopment of appropriate areas within Bend's Central Core, Opportunity Areas and transit corridors (shown on Figure 11-1).

FINDING: The subject properties are located within the KorPine Opportunity Area and have been identified in the City's Comprehensive Plan as appropriate for a mix of residential, commercial, and/or public and institutional uses. Uses and development intensity contemplated and allowed by the Timber Yards Master Plan are consistent with the MU zone. The policy is met.

11-3 The City will ensure that development of large blocks of vacant land makes efficient use of land, meets the city's housing and employment needs, and enhances the community.

FINDING: The Timber Yards Master Plan consists of 32 acres of land zoned MU. The specific mix and intensity of uses will be refined through additional design and market analysis and ultimately specified through future Site Plan Review applications. However, for purposes of infrastructure analysis and mitigation, Timber Yards has been analyzed to accommodate 1,000 dwelling units, a 176-room hotel (or 200 additional dwelling units), 45,000 square feet of office space (or 150 additional dwelling units), and 26,000 square feet of retail space. The mix and density of land uses within Timber Yards will help the City meet its housing and employment needs.

The Preliminary Bicycle and Pedestrian Plan (Sheet P05 of Exhibit B of the application) shows planned multimodal improvements that can support transit use and encourage a pedestrian-friendly environment. A network of multiuse paths will provide additional pedestrian and bicycle facilities around and through the neighborhood, including an off-site extension to connect to the City's Wilson Avenue corridor improvements. The policy is met.

11-4 Streets in the Centers and Corridors, Employment Districts, Neighborhoods, and Opportunity Sites will have the appropriate types of pedestrian, biking, and transit scale amenities to ensure safety, access, and mobility.

FINDING: The Preliminary Bicycle and Pedestrian Plan (Sheet P05 of Exhibit B of the application) shows planned multimodal improvements that can support transit use and encourage a pedestrian-friendly environment. A mobility hub on SW Bond Street serving transit and multiple modes of transportation is planned on the western edge of the master plan and, per the Transportation Mitigation Plan, must be constructed prior to occupancy of the first building in Lot A/Phase A. A network of multiuse paths will provide additional pedestrian and bicycle facilities around and through the neighborhood, including an off-site extension to connect to the City's Wilson Avenue corridor improvements. The policy is met.

Policies for Centers and Corridors

11-24 The City will encourage vertical mixed use development in commercial and mixed use zones, especially where those occur within the Central Core, Opportunity Areas and along transit corridors.

FINDING: Timber Yards is located within the KorPine Opportunity Area, which has been identified in the City's Comprehensive Plan as appropriate for a mix of residential, commercial, and/or public and institutional uses. The Timber Yards Master Plan allows for vertical mixed-use development and building heights up to 75 feet rather than the 65 feet allowed in other MU-zoned areas, providing more opportunity for compact development. For purposes of infrastructure analysis and mitigation, Timber Yards has been analyzed to accommodate 1,000 dwelling units, a 176-room hotel (or 200 additional dwelling units), 45,000 square feet of office space (or 150 additional dwelling units), and 26,000 square feet of retail space. Phase A has been approved for a vertical mixed-use development (PLSPR20230465), including 246 residential units and approximately 16,000 square-feet of commercial space, which is included in this assumed level of development. Phases B and I include options for development (shown in parentheses above) which will be refined through additional design and market analysis and ultimately specified through future Site Plan Review applications. The policy is met.

4.5.200.D.3. Major Community Master Plan. (Continued from page 22)

- d. If the major community master plan proposal contains a zone change request to bring the zoning into compliance with the Bend Comprehensive Plan designation, the zone change is subject to the approval criteria of BDC 4.6.300(C).**

FINDING: The proposal does not include a zone change. This criterion does not apply and the application is not subject to BDC 4.6.300(C).

- e. **If the major community master plan proposal contains a proposed amendment to the Bend Comprehensive Plan Map or text, the amendment is subject to the approval criteria of BDC 4.6.300(B).**

FINDING: The proposal does not contain an amendment to the Bend Comprehensive Plan Map or text amendment. Therefore, the approval criteria of BDC 4.6.300(B) do not apply.

E. Standards and Regulations. Minor and major community master plans must comply with the following standards:

1. **Access to Commercial Goods and Services. Access to commercial goods and services must be provided in compliance with the following standards:**

- a. **The community master plan must have access to commercial goods and services by walking or biking a distance not greater than a one-half mile radius measured from all points along the perimeter of the master plan boundary to any land planned, zoned or developed for one or more such services. Such commercial uses may be provided within nearby neighborhoods or nonresidential districts as long as the minimum distance standard is met. In satisfying such distance standard, commercial goods and services that are not accessible by walking or biking because of physical or geographic barriers (e.g., rivers, Bend Parkway, canals, and railways) may not be used. Except for minor community master plans that are proposing needed housing as defined by state statutes, the Review Authority may find that this provision is met when the commercial uses are located further away than one-half mile but the purpose and intent of providing reasonable access to the commercial uses has been met.**

FINDING: The subject property is zoned MU, which allows commercial uses. Therefore, the entirety of the master plan boundary is within 0.5 miles of land planned or zoned for commercial goods and services. Retail uses are assumed in the infrastructure analyses in Phases A, B and I, which are within 0.5 miles of the entire master plan boundary. In addition, multiple types of commercial uses exist within 0.5-mile surrounding the site. This standard is met.

2. **Multimodal Connections. Multimodal connections must be provided on site in compliance with the City of Bend Transportation System Plan (TSP) and the Bend Parks and Recreation District Parks, Recreation, and Green Spaces**

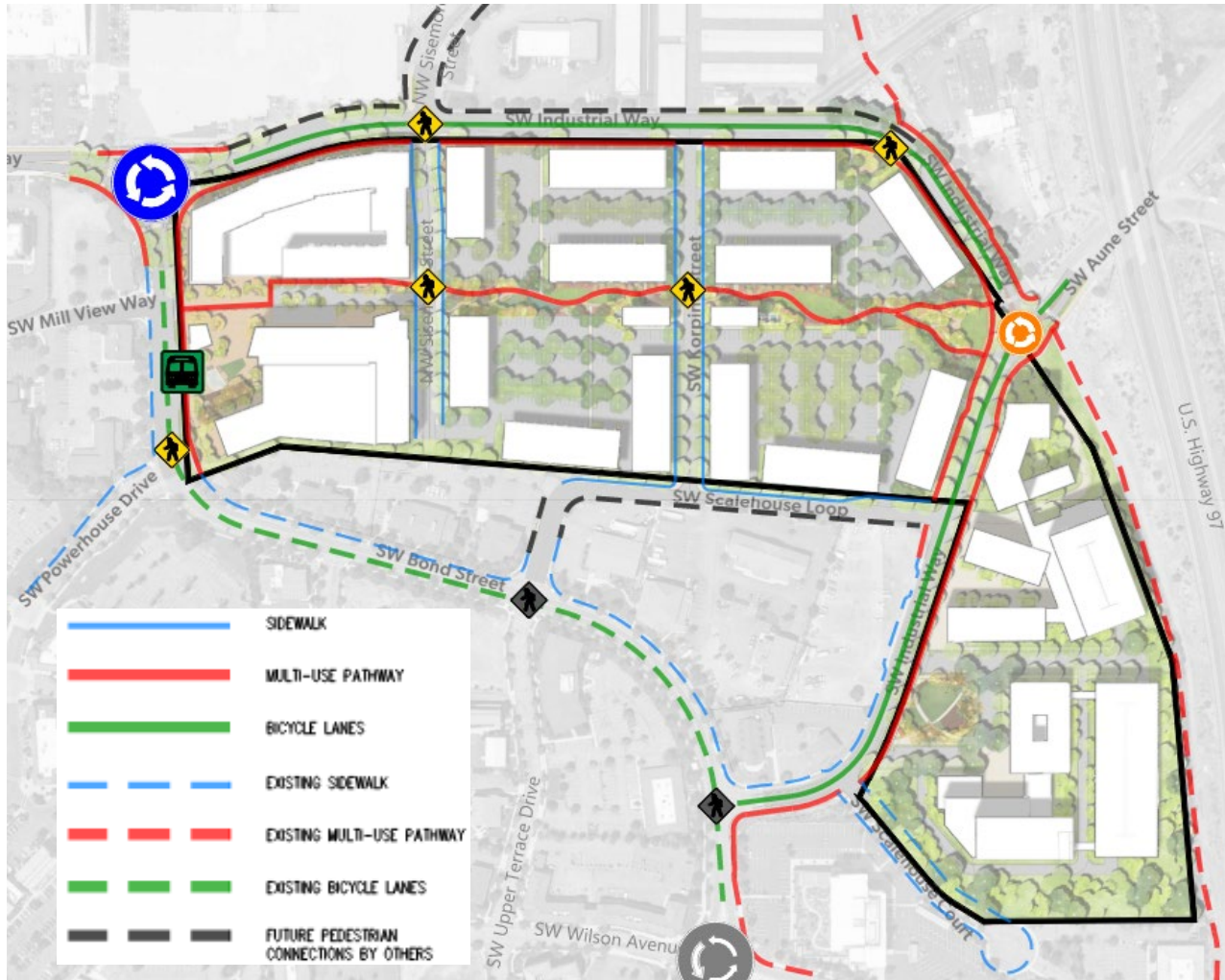
Comprehensive Plan, latest editions, and the existing and planned trail systems adjacent to the community master plan must be continued through the entire community master plan.

FINDING: The Preliminary Bicycle and Pedestrian Plan (Sheet P05 of Exhibit B of the application) shows planned multimodal improvements that can support transit use and encourage a pedestrian-friendly environment. A mobility hub on SW Bond Street serving transit and multiple modes of transportation is planned on the western edge of the master plan and, per the Transportation Mitigation Plan, must be constructed prior to occupancy of the first building in Lot A/Phase A. A network of multiuse paths will provide additional pedestrian and bicycle facilities around and through the neighborhood, including a small off-site extension on SW Industrial Way to connect to the City's Wilson Avenue corridor improvements.

The TSP identifies the US Highway 97 pathway system, the SW Bond Street frontage, the SE Aune Street undercrossing, and the southern leg of Industrial Way as part of the City's "low stress" cycling system (shown in green below). The submitted Preliminary Bicycle and Pedestrian Plan (Sheet P05 of Exhibit B of the application, shown below) shows the location of existing and proposed multi-use paths and bicycle lanes as well as sidewalks. The Street Circulation Plan (BDC Figure 2.7.4760) and the Street Cross-Sections (Sheets P06 and Sheets P08 - P09, respectively, of Exhibit B of the application) show how Timber Yards complies with the low-stress network in the TSP, primarily with wide, separated multi-use paths along the abutting arterial and collector streets. The multi-use path on the north-south segment of SW Industrial Way will connect to the Rails with Trails Connector path that extends to Highway 97 to the south and to the intersection of Arizona and Colorado Avenues to the north. The east-west SW Industrial Way Collector segment is not an identified low-stress route but is expected to provide a supplemental role in the east-west cycling system as shown in the specific cross-section for that street. The 2024 BPRD Comprehensive Plan Update does not show planned BPRD facilities through the site. The applicable standards are met.



Low Stress Network



Proposed Conceptual Bicycle and Pedestrian Plan

3. **Housing Density and Mix. Community master plans 20 acres or larger must provide a mix of housing types and achieve minimum housing densities in conformance with the standards of subsections (E)(3)(a) and (b) of this section. To the extent that the Bend Comprehensive Plan Chapter 11, Growth Management, proposes a different mix of housing and/or density standards in the specific expansion area policies, then those policies apply.**

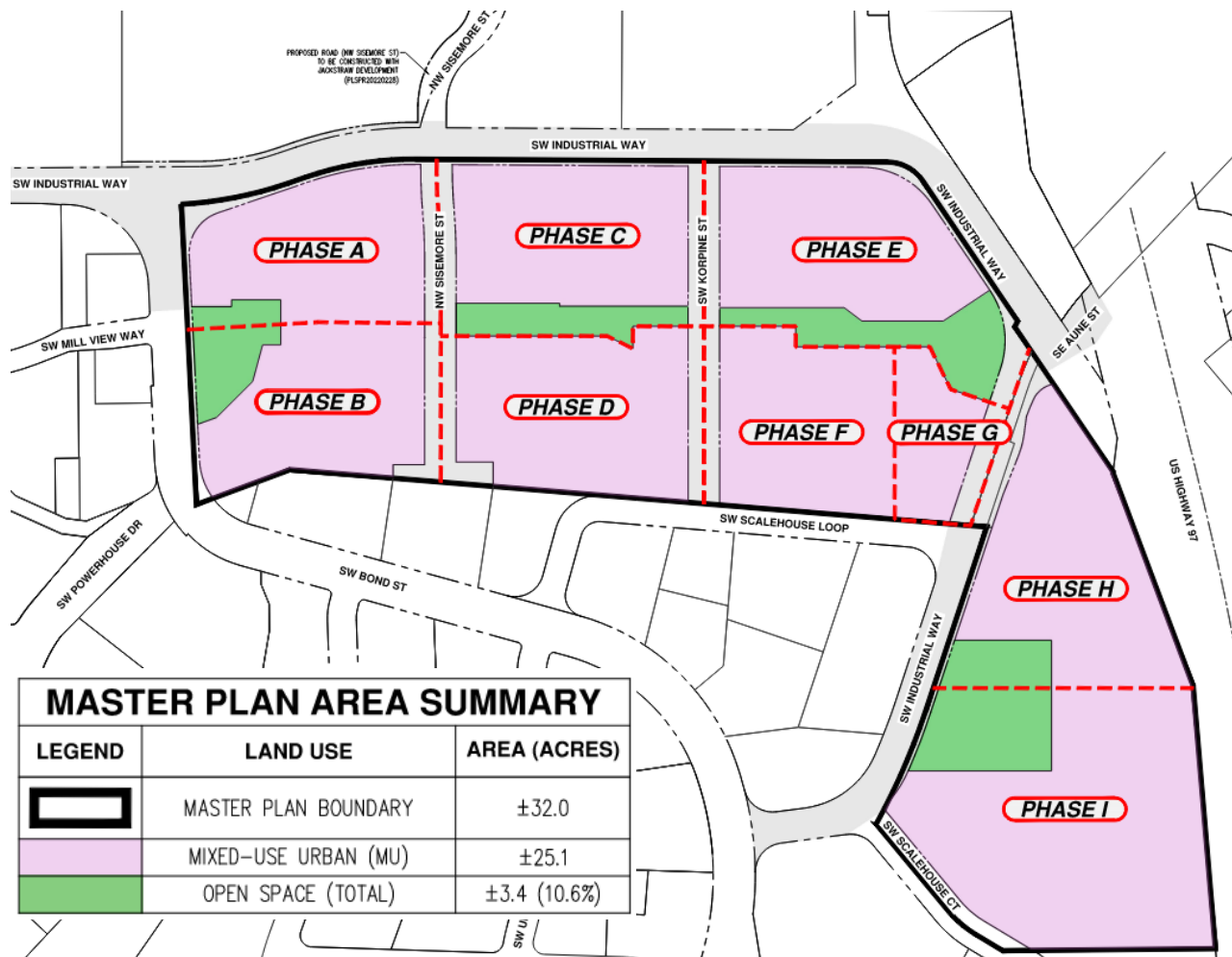
FINDING: The subject property is zoned MU. The housing density and mix standards in BDC 4.5.200(E)(3)(a) and (b) apply explicitly to residential zones and do not apply to the MU zone. BDC 2.3.300 establishes density standards for the MU zone, where there is no maximum density and the minimum density standards of the RM zone apply (7.3 units per gross acre). There is no minimum density for vertical mixed-use. Based on a studied buildout of 1,000-1,350 housing units over the 32-acre master plan area, an overall density

of 35-47 housing units per acre can be achieved (excluding open space tracts). Detached single-unit homes are not permitted in the MU zone, so all permitted residential uses will likely provide housing in a mix of building types, as contemplated by BCP Chapter 11 and BDC 4.5.200(E)(3). The applicable residential density standards are, or can be, met.

- 4. The community master plan must contain a minimum of 10 percent of the gross area as public or private open space such as parks, pavilions, squares and plazas, multi-use paths within a minimum 20-foot wide corridor, areas of special interest, tree preservation areas, or public and private recreational facilities and must comply with the following:**
 - a. The open space area must be shown on the conceptual site plan and recorded with the final plat or separate instrument.**
 - b. The open space must be conveyed in accordance with one of the following methods:**
 - i. By dedication to the Park District or City as publicly owned and maintained open space. Open space proposed for dedication to the Park District or City must be acceptable with regard to the size, shape, location, improvement, environmental condition, and budgetary and maintenance abilities; or**
 - ii. By leasing or conveying title (including beneficial ownership) to a corporation, owners association or other legal entity. The terms of such lease or other instrument of conveyance must include provisions (e.g., maintenance, property tax payment, etc.) acceptable to the City. Private open space must be located in a tract and include an open space easement.**
 - c. Adequate guarantee must be provided to ensure permanent retention of common open space and recreation areas which may be required as conditions of approval.**
 - d. The open space must be open to the public and must not be fenced-off unless it is related to a park or approved public or private recreational facility including, but not limited to, tennis courts, swimming pools, driving ranges and ball fields.**

FINDING: The submitted Preliminary Master Plan shows 3.4 acres of open space (10.6 percent), as shown on Sheet P03 of Exhibit B of the application and shown below. The applicant has also provided a conceptual open space plan (Sheet P04, shown below) to show how these open spaces could be developed and vegetated. An open space tract with

a plaza fronting SW Bond Street will provide the pedestrian gateway into the master plan area, which will then connect through Phase A/B within a public access easement to a linear open space tract east of Sisemore Street, that continues to connect to Industrial Way to the east. This linear open space tract replaces the original Timber Yards Drive in this location. The open space tract straddling Phases H and I east of Industrial Way is increased in size compared to the originally planned open space tract in this location; the open space tract at the southern boundary of Phase I is removed. All open spaces will be located in tracts with an open view space easement. Open space is planned to be owned and maintained by a corporation, owners association, or other legal entity in compliance with BDC 4.5.200(E)(4). The standards are met.



Proposed Master Plan with Phasing



Proposed/Amended Conceptual Open Space Plan

F. Duration of Approval.

- 1. An approved community master plan will remain valid indefinitely unless withdrawn by all owner(s) of property within the community master plan. The City may deny withdrawal when a switch to otherwise applicable standards would not be in the public interest because of sufficient development under the community master plan. Standards and regulations identified in the approved community master plan will control all subsequent site development as long as the approved community master plan is valid. If alternative standards and regulations are not specifically identified in the approved community master plan, the applicable City standard at the time any development application is submitted will apply.**

- 2. The duration of approval for a community master plan must coincide with the timeline outlined in the approved phasing plan and in accordance with the time frames studied in the transportation analysis and water and sewer capacity analysis for the community master plan. Site plan review or land division applications submitted consistent with or earlier than as provided in an approved phasing plan will not require an updated transportation analysis and water and sewer capacity analysis as part of the development application. Infrastructure capacity may be reserved for the community master plan site for up to 15 years or as specified in an approved phasing plan.**
- 3. The time period set forth in this subsection (F) will be tolled upon filing of an appeal to LUBA and must not begin to run until the date that the appellate body has issued a final order.**

FINDING: The timeframe studied in the original transportation analysis extended to 2040; and the Utility Availability Memo assumed an open-ended timeline to achieve full build out. This timeline is not modified with this application. Thus, the infrastructure capacity is reserved for 15 years from the date of adoption of the original master plan, until June 2038.

IV. PLANNING COMMISSION RECOMMENDATION: Based on the application materials submitted by the applicant, and the findings in this report which are based on the applicant's narrative addressing the relevant criteria for approval, the Planning Commission recommends that the Bend City Council adopt an ordinance to amend Bend Development Code Chapter 2.7, Special Planned Districts, Refinement Plans, Area Plans and Master Plans to amend the Timber Yards Master Planned Development as proposed by the applicant and amended by staff.